Town of Arlington, MA
Redevelopment Board

Agenda & Meeting Notice
October 25, 2021

This meeting is being held remotely in accordance with the Governor's March 12, 2020 Order Suspending Certain Provisions of the Open Meeting Law G.L. c. 30A, Section 20. Public comments will be accepted during the public comment periods designated in the agenda. Per Board Rules and Regulations, public comments will be accepted during the public comment periods designated on the agenda. Written comments may be provided by email to jraitt@town.arlington.ma.us by October 25, 2021 at 4:00 p.m. The Board requests that correspondence that includes visual information should be provided by October 22, 2021 at 12:00 p.m.

The Arlington Redevelopment Board will meet Monday, October 25, 2021 at 7:30 PM in the
Join via Zoom at https://town-arlington-ma-us.zoom.us/j/81649252781 Meeting ID: 816 4925 2781.or dial 1-646-876-9923, 816 4925 2781 then #.

1. Public Hearings

7:30 p.m. **Continued Public Hearing**
Docket #3665, 645 Massachusetts Avenue
***Please be advised that the Applicant for Docket #3665 has requested that their application be withdrawn without prejudice.**
Board will continue hearing Special Permit Docket #3665 in accordance with the provisions of MGL Chapter 40A § 11, and the Town of Arlington Zoning Bylaw Section 3.4, Environmental Design Review. The applicant proposes to establish a Chase Bank location on the premises at 645 Massachusetts Avenue, Arlington, MA in the B5 Business District. The continued hearing provides for additional Board review and public comment on the project under Section 3.4, Environmental Design Review and Section 6.2, Signs.

Continued Public Hearing
Docket #3673, 455-457 Massachusetts Avenue
Notice is herewith given that an application has been filed on August 30, 2021 by 2-14 Medford Street, LLC, 455 Massachusetts Avenue, Suite 1, Arlington, MA, to open Special Permit Docket #3673 in accordance with the provisions of MGL Chapter 40A § 11, and the Town of Arlington Zoning Bylaw Section 3.4, Environmental Design Review. The applicant proposes to renovate existing building spaces and construct a mixed-use building containing retail, restaurants, and 13 residential units, including two affordable units, at 455-457 Massachusetts Avenue and 2-14 Medford Street in the B3 Village Business District. The opening of the Special Permit is to allow the Board to review and approve the development under Section 3.4, Environmental Design Review. This hearing is advertised to begin at 8:00 p.m.

Continued Public Hearing
Docket #3348, 833 Massachusetts Avenue
Board will continue hearing Special Permit Docket #3348 in accordance with the provisions of M.G.L. Chapter 40A § 11, and the Town of Arlington Zoning Bylaw Section 3.4, Environmental Design Review, in order to review compliance with special condition 5 of the Special Permit Decision, dated April 13, 2009, and in order to hear from the property owner regarding such compliance. Special condition 5 of the Special Permit Decision refers to the future redevelopment of the Atwood House at 851 Mass Ave.

For the second public hearing: The applicant will be provided 15 minutes for an update on materials requested by the Board.
- DPCD staff will discuss any updates per staff project review.
-Members of the public will be provided time to comment.
-Board members will discuss each docket and may vote.

For the third public hearing: The Board will receive an update from the property owner on the status of the property and review compliance with the EDR Special Permit conditions

2. Meeting Minutes

8:30 p.m. *Board will review and approve meeting minutes*

3. Open Forum

8:35 p.m. *Except in unusual circumstances, any matter presented for consideration of the Board shall neither be acted upon, nor a decision made the night of the presentation. There is a three-minute time limit to present a concern or request.*

4. Adjourn

8:55 p.m. Estimated time of adjournment

5. Correspondence Received

Correspondence received from:
S. Mraz 9/8/2021
S. Tuttle 9/12/2021
M. Guyton 9/14/2021
D. Seltzer 9/21/2021
J. Spiller 9/23/2021
D. Seltzer 10/23/21
T. Kahmann 10/25/2021

Town of Arlington, Massachusetts

Public Hearings

Summary:

7:30 p.m.

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ATTACHMENTS:

Type	File Name	Description
Reference Material	Request_for_Docket_3665_to_be_withdrawn_without_prejudice_received_from_R._Annese_10122021.pdf	Request for Docket 3665 to be withdrawn without Prejudice from R. Annese 10122021 9-27-2021 Continuance

Reference Material	Continuation_Request__Docket_3665__645_Massachusetts_Avenue_received_9-27-2021.pdf	request Docket #3665 to 10/25/2021
Reference Material	Agenda_Item_1_-_Memo_from_Town_Counsel_re_EDR_Process.pdf	Memo from Town Counsel re EDR Process 09232021
Reference Material	Updated_Memo_to_ARB_re_Docket_3665_09-22-21.pdf	Updated Memo to ARB re Docket 3665 09222021
Reference Material	20210922_Memo_Banks_in_Arlington_Center.pdf	09222021 Memo Banks in Arlington Center
Reference Material	Chase_Arlington_MA_-_Response_to_ARB_Meeting_Review_Comments.pdf	9-18-2021 Response to ARB Meeting Review Comments
Reference Material	Docket#_3665_Request_to_be_continued_to_9-27-21_meeting.pdf	09-07-2021 Request for Docket #3665 to be continued to 9/27/21 meeting
Reference Material	Agenda_Item_1B_-_EDR_Public_Hearing_Memo_#3665_645_Mass_Ave.pdf	8-24-2021 EDR Public Hearing Memo #3665
Reference Material	Agenda_Item_1B_-_645_Mass_Ave_Combined_Application.pdf	8-11-2021 645 Mass Ave Combined Application
Reference Material	EDR_Public_Hearing_Memo_Docket_3673_455-457_Mass_Ave_Update.pdf	EDR Public Hearing Memo Docket 3673 455-457 Mass Ave. Update 10-21-2021
Reference Material	ARB_Comments.pdf	New - 10/19/2021 ARB Comments
Reference Material	2020034_455_Mass_Ave_SD_101921.pdf	New -10-19-2021 455 Mass Ave Site Plans
Reference Material	2021-10-18_-_Supplemental_Plans.pdf	New - 10-18-2021 455 Mass Ave. Supplemental Plans
Reference Material	New_-__455_Mass_Ave._bike_racks_and_lighting_intent_update.pdf	New - 455 Mass Ave Bike Rack and lighting update 9-27-2021

Reference Material	EDR_Public_Hearing_Memo_Docket_3673_455-457_Mass_Ave.pdf	EDR Public Hearing Memo docket 3673 455-457 Mass Ave. 09232021
Reference Material	Docket_3673_455-457_Mass_Ave_Application_Materials_-_time_stamped.pdf	Docket 3673 455-457 Application Materials 08302021
Reference Material	FAR_calculation_by_applicant_via_email_083021.pdf	8-3-2021 FAR calculation by applicant via email
Reference Material	New_-_10-21-2021_Conceptual_Design_Schemes-_Atwood_House.pdf	New - Conceptual Design Schemes - Atwood House 10212021
Reference Material	New_-_10212021_Conceptual_design_scheme_email_from_R._Annese.pdf	New- email from R. Annese re Conceptual design Scheme 10-21-2021
Reference Material	NEW_Notification_of_Bylaw_Violations_821_Mass._Ave._to_G._Noyes_7-21-21.pdf	7-21-2021 Notification of Bylaw Violations 821 Mass Ave. to G. Noyes
Reference Material	Letter_from_Annese_re_Atwood_House.pdf	9-23-2020 Letter from Annese re Atwood House
Reference Material	ARB_vote_regarding_821_Mass_Ave_050420.pdf	5-4-2020 ARB Vote regarding 820 Mass Ave
Reference Material	833_Mass_Ave_Book_and_Page.pdf	12-18-2019 833 Mass Ave. Registry Book and Page recording
Reference Material	Decision_Docket_3348_833_Mass_Ave.pdf	12-9-2019 Decision Docket 3348 833 Mass Ave
Reference Material	Letter_to_Noyes_re_Atwood_House_081519.pdf	8-15-2019 Letter to Noyes re Atwood House
Reference Material	EDR_Public_Hearing_Memo_833_Mass_Ave.pdf	8-6-2019 EDR Public Hearing Memo 833

Reference Material	CVS_Arlington_MA_Special_Permit_6.27.19_reduced.pdf	Mass Ave 6-27-2019 CVS Arlington MA Special Permit
Reference Material	Letter_to_Noyes_re_special_permit_#3348_12-16-19_public_hearing_11182019.pdf	11-18-2016 Letter to Noyes re Special Permit #3348 12-16-19 Public Hearing

From: "Robert Annese" <law@robertannese.com>
To: "Jennifer Raitt" <JRaitt@town.arlington.ma.us>
Cc: "'Kelly Lynema'" <KLynema@town.arlington.ma.us>, "'Ian Heanue'" <iheanue@peconsultingcorp.com>, "'Marc Sides'" <msides@core-states.com>, <richard.ramsey@wtphelan.com>, <brendan@noonanrealestate.com>, "'Cabrera, Brooke C'" <brooke.c.cabrera@chase.com>, "'Buscemi, Dave'" <dave.buscemi@jpmchase.com>, "'Steve Tomasello'" <stomasello@atlanticretail.com>, "'Coral Silsbe'" <Csilsbe@peconsultingcorp.com>, "'McCool, Matthew J'" <matthew.j.mccool@chase.com>
Date: Tue, 12 Oct 2021 16:54:24 -0400
Subject: Application Under Environmental Design Review for 645 Massachusetts Avenue i.e., Chase Bank Application

Hi Jenny:

I have been retained by Chase Bank to represent the Bank with respect to the continued hearing scheduled for October 25, 2021.

I would be requesting that the current application i.e., Docket #3665 be withdrawn without prejudice as I would want to meet with my clients' representatives for the purpose of filing a new application with the ARB.

Would you please let me know whether this request will be granted.

Thank you.

Bob

Robert J. Annese, Esquire
1171 Massachusetts Avenue
Arlington, MA 02476
Telephone: 781-646-4911
Facsimile: 781-646-4910
law@robertannese.com

From: Ian Heanue <iheanue@peconsultingcorp.com>
To: Jenny Raitt <JRaitt@town.arlington.ma.us>, Mary Muszynski <MMuszynski@town.arlington.ma.us>, Kelly Lynema <KLynema@town.arlington.ma.us>
Cc: "Marc Sides <msides@core-states.com>" <msides@core-states.com>, "Cabrera, Brooke C" <brooke.c.cabrera@chase.com>, "Steve Tomasello <stomasello@atlanticretail.com>" <stomasello@atlanticretail.com>, "'brendan@noonanrealestate.com'" <brendan@noonanrealestate.com>, "'richard.ramsey@wtphelan.com'" <richard.ramsey@wtphelan.com>
Date: Mon, 27 Sep 2021 19:45:46 +0000
Subject: Docket 3665, 645 Massachusetts Avenue

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CAUTION: This email originated from outside of the Town of Arlington's email system. Do not click links or open attachments unless you recognize the REAL sender (whose email address in the From: line in "< >" brackets) and you know the content is safe.

Good Afternoon,

Please accept this email as an official request to have our special permit application (Docket #3665) for JPMorgan Chase NA Bank, at 645 Massachusetts Avenue, Arlington MA, continued to the October 25th, 2021 board hearing.

This continuance will allow the team time to prepare our response in greater detail and have Bank representatives present.

If you have any questions or require any additional documentation please feel free to reach out.

Thank You,

Ian Heanue

Project Expediters Consulting Corp., *WBE, WBENC Certified*
Project Expediters Inc, *WBE, WBENC Certified*
Faneuil Hall Marketplace
4 South Market Street
Suite S-4035, Third Floor
Boston, MA 02109
Office: 617-227-0159
Cell: 908-907-0484
Fax: 617-227-0501

Town of Arlington
Legal Department

Douglas W. Heim
Town Counsel

50 Pleasant Street
Arlington, MA 02476
Phone: 781.316.3150
Fax: 781.316.3159
E-mail: dheim@town.arlington.ma.us
Website: www.arlingtonma.gov

To: Arlington Redevelopment Board;
Jennifer Raitt, Director of Planning and Community Development

From: Douglas W. Heim, Town Counsel

Date: September 23, 2021

Re: Special Permit Process for EDR Applications

Members of the Arlington Redevelopment Board ("ARB" or "Board"), you inquired as to the appropriate process and standards for hearing special permit applications subject to Environmental Design Review ("EDR") under your purview; specifically, whether or not the Board should evaluate EDR standards under §3.4.4 if and when the Board (or some of its members) believes that a permit application should be denied for failure to satisfy the more general special permit criteria of §3.3.3. Articulated another way, should the Board essentially bifurcate its "regular" special permit criteria from its EDR standards to implement a rounded or "phased" permitting process where it anticipates a denial? As set forth fully below, this Office does not recommend adopting a phased special permit process for EDR-qualified applications at this time. If the Board were inclined to consider such an approach, it is recommended that your regulations and application materials be updated to more clearly reflect the purpose, parameters, and timing of phased or rounded special permit hearings.

Background and Context

To this Office's understanding, the specific context of the Board's inquiry is a pending special permit application before it subject to EDR which may or may not present an undesirable or excessive use under special permit criteria 3.3.3(B) and (G). Board members inquired whether an initial determination that the proposed use was undesirable and/or excessive might foreclose further review at hearing of EDR standards and form the sufficient basis for a denial. The Director of Planning and Community Development responded that the Board's current practice under the Zoning Bylaw and ARB is not to bifurcate your consideration and decision making, and that a denial should incorporate the Board's full consideration of EDR standards as well as baseline special permit criteria of § 3.3.3. The Board, through its Chair sought further review of the issue and the opinion of this Office.

The Bylaw, Rules & Regulations, & EDR

As noted in prior memos to the Board, you are a body of limited, but special jurisdiction, functioning as a Redevelopment Authority, Planning Board, and Special Permit Granting Authority (SPGA) through the lens of Environmental Design Review ("EDR") as codified in the Zoning Bylaw. Accordingly, approximately 10 percent of the Town's special permit applications are submitted to you, each involving commercial, industrial, larger scale residential, or mixed uses "which have a substantial impact on the character of the town and on traffic, utilities, and property values, thereby affecting the public health, safety and general welfare."

In order to accomplish your goals and realize the ARB's purpose as set forth in the Bylaw, the Board utilizes the more rigorous, but also more flexible and subjective toolkit of the EDR process, which adds to special permitting standards and processes established for predominantly (though not exclusively) residential uses currently governed by the Zoning Board Appeals ("ZBA"). Indeed, §3.4.3 of the Zoning Bylaw outlines a specific procedure for EDR projects which does not apply to "regular" special permits as follows:

> Procedures
>
> A. Application. Applicants shall submit an application for Environmental Design Review in accordance with the Arlington Redevelopment Board's ("Board") rules and regulations.
>
> B. The Board shall hold a public hearing in *accordance with Section 3.3 of this Bylaw* and G.L. c. 40A, §§ 9 and 11.
>
> C. The Board shall refer the application to the Department of Planning and Community Development ("Department"), which shall prepare and submit written reports with recommendations to the Board before or at the public hearing. The Board shall not take final action on the special permit application until it has received the Department's report or until 35 days have elapsed after submittal of the proposal to the Department. Failure of the Department to submit

> written reports or to give an oral report at the public hearing shall not invalidate action by the Board.
>
> D. A favorable decision by the Board shall require the votes of at least four members.
>
> E. *The Board shall not deny a special permit under this Section 3.4 unless it finds that the proposed use does not comply with the Environmental Design Review* Standards listed below to such a degree that such use would result in a substantial adverse impact upon the character of the neighborhood or the town, and upon traffic, utilities, and public or private investments, thereby conflicting with the purposes of this Bylaw.

(emphasis added).

Reading §§ 3.4.3 (B) and (E) harmoniously suggests that EDR special permitting reflects a holistic approach whereby the ARB requires information fully responsive to both the Special Permit Criteria set forth in §3.3.3 and the twelve (12) EDR criteria set forth in §3.4.4. These requirements are echoed in Rule 14 of your Rules and Regulations, and further clarified by Rule 15, which states:

> RULE 15 : BOARD DECISIONS
>
> The ARB shall review the plans and may grant a special permit subject to the conditions and safeguards listed in the Arlington Zoning Bylaw *Section 3.3 and 3.3.4.* For stated reasons the ARB may deny approval of a special permit or may approve a special permit without a finding of hardship. As required by M.G.L. c. 40A, §9, a positive vote of at least four members of the Redevelopment Board is needed to issue a special permit. Upon the Board's approval, the Secretary Ex-Officio may sign decisions following a vote of the Board and file decisions per requirements of M.G.L. c. 40A. The final decision shall be emailed and may receive administrative corrections following the Board's votes.

(emphasis added).

Based on the Bylaw, ARB Rules and Regulations, your application requirements, as well as a review of a collection of past decisions of the Board (and c. 40A), it is clear that in any application for an EDR permit, the applicant must address all the Special Permit Criteria set forth in §3.3.3 *and* the twelve (12) EDR criteria set forth in §3.4.4; and further, any approval decision by the ARB must at least assess and address same.[1] The only question therefore is whether or not a denial (or anticipated denial) would be excused from the same process and standard if

[1] It may well be, and indeed several of your decisions reflect, that one or even several specific EDR standards are not central to the ARB's thinking on any given application, and/or that conditions on a permit focus on some EDR standards more than others.

rooted in a failure to satisfy §3.3.3. For the reasons discussed below, this Office concurs with the Director of Planning and Community Development's assessment that your best practice is to utilize the same process in any instance where denial is possible, but not objectively certain on the face of the application.

Analysis

As an initial matter, the nuance of denials under c. 40A should be noted. On one hand, c. 40A §9 provides that a Special Permit Granting Authority "shall cause to be made a detailed record of its proceedings, indicating the vote of each member upon each question, or if absent or failing to vote, indicating such fact, and setting forth clearly the reason for its decision and of its official actions..." Further, a decision granting a special permit must include any findings required by the municipal ordinance or bylaw, as well as the findings required by the applicable provisions of the Zoning Act. *Sheehan v. Zoning Bd. of Appeals of Plymouth*, 65 Mass. App. Ct. 52, 56 (2005).

On the other, favorable actions require more vigorous support articulating the basis for the grant of a special permit than a denial of same. *Gamache v. Town of Acushnet*, 14 Mass. App. Ct. 215 (1982) (denial of variance based on town's policy against trailer parks is sufficient absent a clear record to the contrary); *Board of Aldermen of Newton v. Maniace*, 429 Mass. 726 (1999) (even failure to obtain requisite affirmative votes for a draft decision constitutes a sufficient basis for denial). Nonetheless, as a general rule of practice it should be rare that a process is determined by a potential (or even likely) outcome absent a very clear roadmap for fast-tracking a decision or failure to meet entirely objective threshold criteria. In other words, the difficulty in making a decision solely on the basis of §3.3.3 criteria is standardizing the Board's level of certainty that a vote on a subject criteria – the desirability or concentration of a particular use – preempts all further development of the record.

There are examples where boards and bodies adopt a "phased" or "rounded" process" whereby only some facets of an application are considered in different stages. Under such processes, applications essentially pass or fail (typically on a more narrow set of criteria) before proceeding on to the next stage of analysis with the specific goals identified and served by evaluating only portions of an application. In most of those cases however, there is no prejudicial impact of a denial akin to the two (2) year prohibition on repeat applications found in c. 40A §16. The ARB theoretically could implement such a process. However, at present the ARB's Rules and Regulations and application materials do not provide a clear roadmap for the goals, timing, or tools necessary of bifurcating EDR special permit applications into §3.3.3 analysis and *then* §3.4.4 analysis at some later phase.

For example, it is not clear when and how the Board would assess an application and take a vote to make a threshold determination on the baseline Special Permit criteria under §3.3 of the Zoning Bylaw. Would an unsuccessful motion to deny based solely on §3.3.3 criteria preclude later denial on the same grounds after application of EDR standards? If a member of the Board

has further bases for denial that have not yet been addressed on the record under EDR, may those concerns be articulated in the decision as well? Are they sufficiently supported in the record?

It is similarly unclear when and to what extent an applicant's response to EDR standards under §3.4.4 can help or hinder the Board's assessment of §3.3 criteria under a bifurcated review. Both your Bylaw and your Rules and Regulations imply interplay between these criteria. There may be circumstances where after the more robust application of EDR, a member of the ARB is persuaded or dissuaded that a given project is more or less responsive to being "essential or desirable to the public convenience or welfare" – one of the baseline §3.3.4 criteria. Similarly, the Board might be deterred by or impressed with an applicants' proposal with regard to EDR criteria "J" ("[w]ith respect to Arlington's heritage, removal or disruption of historic, traditional or significant uses, structures, or architectural elements shall be minimized insofar as practicable, whether these exist on the site or on adjacent properties") in such a manner as to inform the desirability of the use under §3.3.3(B).

Moreover, while the standards for denials of special permit applications are more modest than approvals, in order to best defend its decisions and convey the basis for denials to future applicants, the Board may articulate any and all reasons for denial in the most comprehensive manner practicable. Alternately stated, if the Board is denying a permit for *both* causing an excess of use detrimental to the character of a neighborhood (3.3.3(G)) *and* negative findings with respect to relation of buildings to the environment (3.4.4(B)), such denial is all the better supported. Without engaging in EDR, it begs the question of why EDR supporting materials were required in the first instance and could undermine an otherwise valid denial with respect to any basis from the desirability of use (3.3.3(B)) to unduly impairing pedestrian safety (3.3.3(C)).

The foregoing should not be read to imply that the Board may not deny a special permit under EDR for one of the reasons set forth in §3.3.3 such as an excessive use, or that only EDR standards ought to form the basis for approval or denial to the exclusion of §3.3.3. Such a determination however typically involves some subjective, qualitative judgment, which may be informed positively or negatively by application of full EDR standards and process. There may also be rare instances where an application cannot reasonably proceed because the use requested is not permitted in a district and an applicant has submitted despite efforts to persuade them otherwise. This Office is sensitive to the demands upon the ARB's time and attention. It remains however in the Board's interest to fully examine EDR applications under a full EDR process unless objective or procedural denials are merited, and/or Rules and Regulations harmonious to the Zoning Bylaw are developed to support a bifurcated or staged application review.

Conclusion

For the reasons set forth herein, this Office agrees with the Director of Planning and Community Development's recommendation to assess Special Permit Applications before you with both "Special Permit" Criteria under §3.3.3 and EDR Standards under §3.4.4 before voting upon your decisions unless and until the Board commits to a more detailed bifurcated or phased process in your Rules and Regulations.

Town of Arlington, Massachusetts
Department of Planning & Community Development
730 Massachusetts Avenue, Arlington, Massachusetts 02476

Public Hearing Memorandum - Update

The purpose of this memorandum is to provide the Arlington Redevelopment Board and public with technical information and a planning analysis to assist with the regulatory decision-making process.

To: Arlington Redevelopment Board

From: Jennifer Raitt, Secretary Ex Officio

Subject: Environmental Design Review, Docket #3665
645 Massachusetts Avenue

Date: September 22, 2021

This memo is provided as an update to the previous memo provided on August 30, 2021. The materials provided for the continued public hearing are not responsive to the ARB's comments. The Applicant provided the following materials:

- Cover letter to the Redevelopment Board, dated September 18, 2021; and
- Sign Package, including elevations and renderings, prepared by Philadelphia Sign, revised September 9, 2021.

The Board requested that the Applicant install clear-glazed windows along the streetfront façade that align with the commercial district and use; move the proposed blade sign; maintain the existing sidewalk seating and planters from the prior tenant; address the rear access point for accessibility; and incorporate the required bike parking. The Board also asked the Applicant to provide a better description of their services and how they differ from similar business uses in Arlington Center.

Regarding the windows along the Massachusetts Avenue and David Lamson Way facades, the renderings and elevations in the sign package show that the gridded windows will be replaced with commercial windows. It appears from the renderings that the glazing has also been addressed. Specifications on window type and glazing have not been provided.

With regard to signage, the Board can find that the request to move the proposed blade has been addressed.

Regarding sidewalk seating and planters, the applicant has revised the renderings to show seating and planters along the Massachusetts Avenue façade. The applicant will need to provide updated site and/or floor plans indicating the locations of these items.

Regarding bicycle parking, the applicant states that submission materials have been updated to show short term bicycle parking in front of the site. A bicycle rack is shown on the rendering on DRC Page 8 of the updated sign package. The applicant should update site and/or floor plans accordingly, indicate the amount of short-term bicycle parking provided. While the applicant indicates that long-term parking will be integrated into the project, the applicant will need to provide updated floor plans identifying the location, quantity, and type of long-term bicycle parking. Bike rack specifications for short- and long-term parking are needed to determine compliance with Section 6.1.12.E.

Regarding providing an accessible entrance at the rear of the building, the applicant concluded that the existing conditions prohibit their ability to improve accessibility through the rear entrance. The closest HP placard parking space to the main entry and ATM is in front of the Coldwell Banker property at 635 Massachusetts Avenue.

The applicant should provide a more detailed update regarding any of the above items at the continued hearing.

Representatives from Chase Bank will be in attendance at the September 27, 2021 hearing to present information on the services they intend to provide at this location, as well as to answer questions regarding any market studies that were completed as part of the bank's decision to establish a branch at this location.

Finally, staff were asked to calculate the total linear feet of banks within Arlington Center. For economic development purposes, the Town defines the Arlington Center as 375 to 1056 Massachusetts Avenue; 283 to 327 Broadway; Medford, Mill, Mystic Streets; Schouler Court; Summer Street. Within these limits are a total of 264 commercial units[1].

There are a total of nine banks or ATMs in Arlington Center, or 3.4% of total commercial units. The table below provides the bank name, type, address, parcel frontage, and building width. Parcels on corner lots are indicated with an asterisk; for corner lots, the total parcel and building frontage along both streets are listed first, followed by the frontage specific to Massachusetts Avenue in parentheses.

[1] According to Arlington Town Assessor Data. This figure does not include the 180 individual rental units and commercial condos within commercial properties like 22 Mill, 29 Mill, etc.

Bank name	Type	Address	Parcel frontage (ft)	Street-facing building width (ft)
Chase Bank	ATM	323 Broadway	n/a[2]	23
Leader Bank	Bank Branch	449 Mass Ave*	n/a[3]	70 (26)
Cambridge Savings Bank	Bank Branch and Drive-up	626 Mass Ave*	285 (115)	187 (133)
Bank of America	Bank Branch	655 Mass Ave	51	48
Citizens Bank	Bank Branch and Drive-up	699 Mass Ave*	278 (151)	139 (82)
Brookline Bank	Bank Branch and Drive-up	856 Mass Ave*	204 (91)	135 (67)
Leader Bank	Residential Lending	864 Mass Ave*	218 (107)	154 (103)
TD Bank North	Bank Branch and Drive-up	880 Mass Ave*	408 (179)	182 (80)
Watertown Savings Bank	Bank Branch and Drive-up	980 Mass Ave*	268 (110)	155 (69)
		TOTAL	**1,712 (804)**	**1,093 (631)**
* Corner lot				

Linear calculations have been estimated using the Town's GIS data. Should precise numbers be required, individual site plans would need to be requested from each property.

[2] Tenant in a structure with multiple units on one lot
[3] Tenant in a structure with multiple units on one lot

TOWN OF ARLINGTON
DEPARTMENT OF PLANNING and
COMMUNITY DEVELOPMENT

TOWN HALL, 730 MASSACHUSETTS AVENUE
ARLINGTON, MASSACHUSETTS 02476
TELEPHONE 781-316-3090

MEMORANDUM

To: Arlington Redevelopment Board
From: Jennifer Raitt, Director, Department of Planning and Community Development/kl/ac
Date: September 22, 2021
RE: Linear Feet of Commercial Banks in Arlington Center

During the Redevelopment Board's August 30, 2021 hearing on Docket #3665 to establish a Chase Bank location within the storefront at 645 Massachusetts Avenue, staff were asked to calculate the total linear feet of banks within Arlington Center.

For economic development purposes, the Town defines the Arlington Center as 375 to 1056 Massachusetts Avenue; 283 to 327 Broadway; Medford, Mill, Mystic Streets; Schouler Court; Summer Street. Within these limits are a total of 264 commercial units[1].

There are a total of nine banks or ATMs in Arlington Center, or 3.4% of total commercial units. The table below provides the bank name, type, address, parcel frontage, and building width. Parcels on corner lots are indicated with an asterisk; for corner lots, the total parcel and building frontage along both streets are listed first, followed by the frontage specific to Massachusetts Avenue in parentheses.

Bank name	Type	Address	Parcel frontage (ft)	Street-facing building width (ft)
Chase Bank	ATM	323 Broadway	n/a[2]	23
Leader Bank	Bank Branch	449 Mass Ave*	n/a[3]	70 (26)
Cambridge Savings Bank	Bank Branch and Drive-up	626 Mass Ave*	285 (115)	187 (133)
Bank of America	Bank Branch	655 Mass Ave	51	48
Citizens Bank	Bank Branch and Drive-up	699 Mass Ave*	278 (151)	139 (82)
Brookline Bank	Bank Branch and Drive-up	856 Mass Ave*	204 (91)	135 (67)
Leader Bank	Residential Lending	864 Mass Ave*	218 (107)	154 (103)
TD Bank North	Bank Branch and Drive-up	880 Mass Ave*	408 (179)	182 (80)
Watertown Savings Bank	Bank Branch and Drive-up	980 Mass Ave*	268 (110)	155 (69)
		TOTAL	**1,712 (804)**	**1,093 (631)**
* Corner lot				

Linear calculations have been estimated using the Town's GIS data. Should precise numbers be required, individual site plans would need to be requested from each property.

[1] According to Arlington Town Assessor Data. This figure does not include the 180 individual rental units and commercial condos within commercial properties like 22 Mill, 29 Mill, etc.
[2] Tenant in a structure with multiple units on one lot
[3] Tenant in a structure with multiple units on one lot

CORE STATES GROUP

September 18, 2021

Town of Arlington
Redevelopment Board
730 Mass Ave. Annex
Arlington, MA 02476

Re: Application for Special Permit Review, 645 Massachusetts Avenue – Second Review

The whom it may concern;
We would like to thank the board for their time during the August 30, 20201 Arlington Redevelopment Board meeting. The input that was provided during this meeting was taken into consideration in preparation for our second appearance on the topic of the special permits requested for this site.

To respond to some of the comments issued, we offer the following, in conjunction with the attached revised documents:

Automobile Parking Relief: It appeared, from the response from the board, that they were in favor of granting this relief, on the grounds of the large municipal lot behind the proposed site and the presence of on-street parking in front. NO RESPONSE REQUIRED

Bicycle Parking Relief: Per the board, no relief would be granted on the bicycle parking requirement. THE SUBMISSION MATERIALS HAVE BEEN REVISED TO REFLECT SHORT TERM BICYCLE PARKING IN FRONT OF THE PROPOSED SITE. LONG TERM PARKING WILL BE INTEGRATED INTO THE PROJECT AS WELL TO MEET THE REQUIREMENT.

Special Permit for the Installation of a Bank:
Change of Use: Chase bank is seeking a special permit to allow for a bank use greater than 2,000 sf within the B5 District, as required by the zoning by-laws. Additionally, the previous use was a restaurant, requiring a change of use to the proposed bank (business) use. THE BOARD WAS SPLIT ON THE TOPIC. CHASE BANK REQUESTED A CONTINUATION IN ORDER TO ASSEMBLE ADDITIONAL PRESENTATION MATERIALS AND HAVE REPRESENTATIVES FROM CHASE BANK PRESENT TO DISCUSS THE PROJECT.
Façade: The board recommended changing the existing gridded windows to match the proposed new windows. THE RENDERINGS HAVE BEEN REVISED TO INCORPORATE THIS CHANGE
Sidewalk Amenities: The previously proposed plan reflected the removal of the existing benches and planters. The board recommended retaining this component of the exterior. THE RENDERINGS HAVE BEEN REVISED TO INCORPORATE THIS CHANGE

9 Galen Street | Suite 117
Watertown, MA 02472
617.576.0007
www.core-states.com

Accessibility: The board questioned why the vestibule at the rear of the building was not being made accessible and requested studying the addition of a ramp or a wheelchair lift. THERE ARE MULTIPLE EXISTING CONSTRAINTS THAT WILL LIMIT THE ABILITY TO MAKE THE REAR DOOR FULLY ACCESSIBLE, INCLUDING IMPACTS TO THE UPPER-LEVEL TENANT ENTRANCE, EXISTING (MUNICIPAL) PARKING LOT, AND SIDEWALK ALONG DAVID LAMSON WAY. IN SPITE OF THESE CONSTRAINTS, CHASE WILL CONTINUE TO ENDEAVOR TO STUDY THESE CONDITIONS AND EVALUATE THE VIABILITY OF AN ACCESSIBLE REAR ENTRANCE. PLEASE NOTE THAT THERE IS RESERVED ACCESSIBLE STREET PARKING ON MASS AVE NEAR THE FRONT ENTRANCE, WHICH WOULD PROVIDE ACCESS TO THE BANK AND ATM VESTIBULE.
Signage: The board was not in favor of the proposed blade sign. They believed the sign was not in keeping with other signage in the Central Business District as asked if we could consider moving the sign on the gray band along the alley side of the building. THE RENDERINGS HAVE BEEN REVISED TO INCORPORATE THIS CHANGE

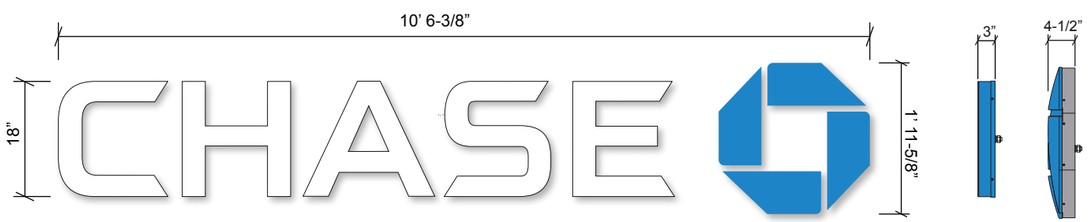
We look forward to the opportunity to meet with the board again to review this project. Please let us know if anything additional is required.

Sincerely,

James Lalli
Director of Architecture - Financial
908.462.9949 | jlalli@core-states.com

South Elevation - Massachusetts Ave

SIGN E10 NOT SEEN FROM THIS ANGLE



LIF-WBO-18-LED

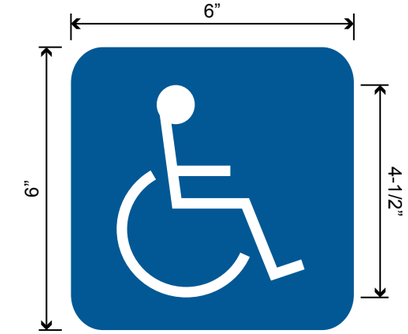
White w/ Blue Octagon Illum Channel Letters - 20.7 SF

Scale: NTS

NMP-XX

Dark Nickel Metal Pan - Dimensions TBD

Scale: NTS



ADA-EP

Handicapped Entrance Plaque

Scale: NTS

SIGN E10 MOUNTS BACK-TO-BACK WITH SIGN I01.

CUST-VIN

Matching Blue Vinyl Backer

Scale: NTS

CHASE

CHS.NB.961 - Arlington Mass Avenue

645 Massachusetts Avenue
Arlington, MA 02476

DESIGNER - JM

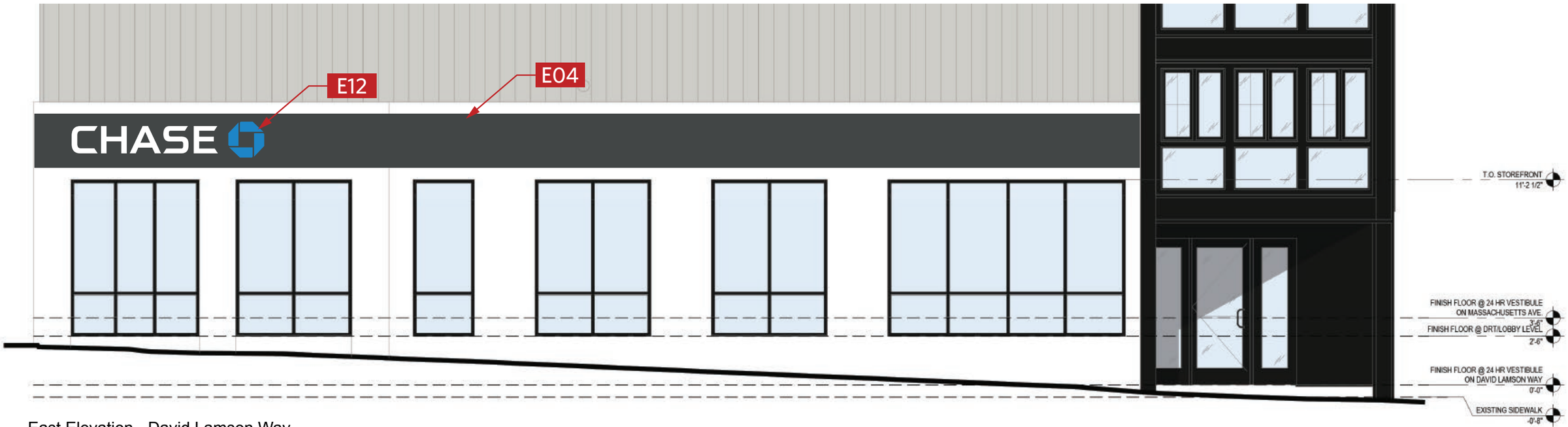
CREATED - 06.17.21

DRAWING - B95122

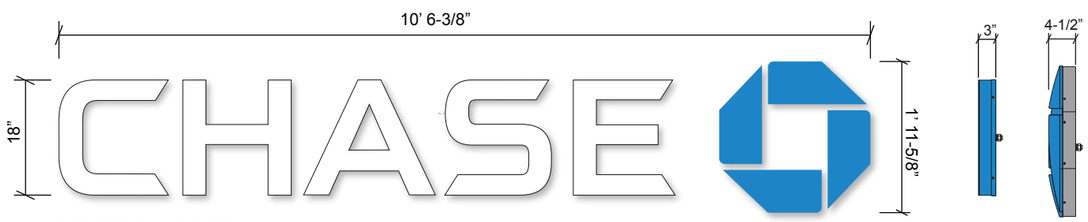
PHILADELPHIASIGN
BRINGING THE WORLD'S BRANDS TO LIFE
707 WEST SPRING GARDEN ST • PALMYRA, NJ • 08065
P: 856-829-1460 • F: 856-829-8549 • WEB: http://www.philadelphiasign.com

E04 - NMP-XX

E12 - LIF-WBO-18-LED



East Elevation - David Lamson Way



LIF-WBO-18-LED

White w/ Blue Octagon Illum Channel Letters - 20.7 SF

Scale: NTS

NMP-XX

Dark Nickel Metal Pan - Dimensions TBD

Scale: NTS

CHASE

CHS.NB.961 - Arlington Mass Avenue

645 Massachusetts Avenue
Arlington, MA 02476

DESIGNER - JM

CREATED - 06.17.21

DRAWING - B95122

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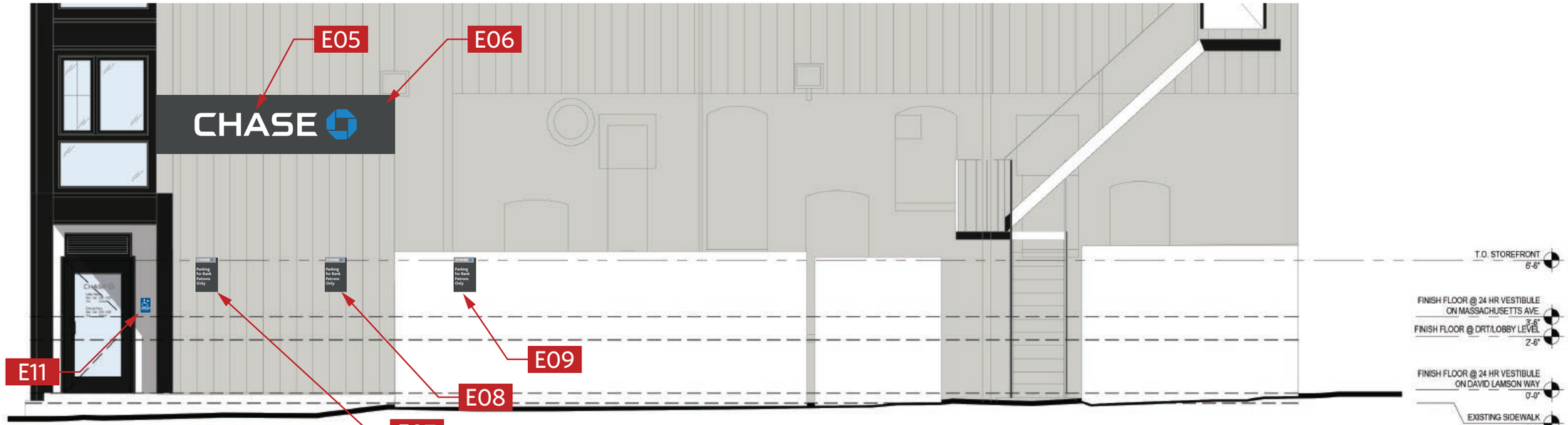
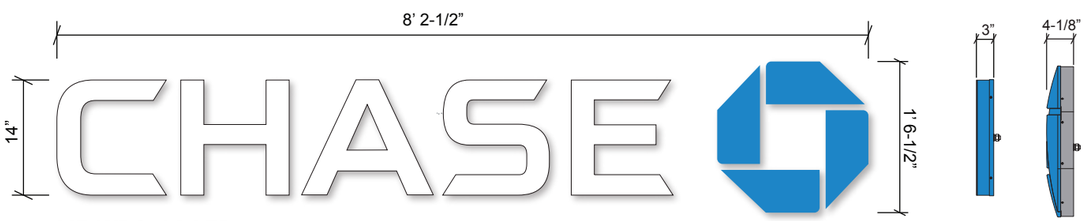


North Elevation - Shared Parking



LIF-WBO-14-LED

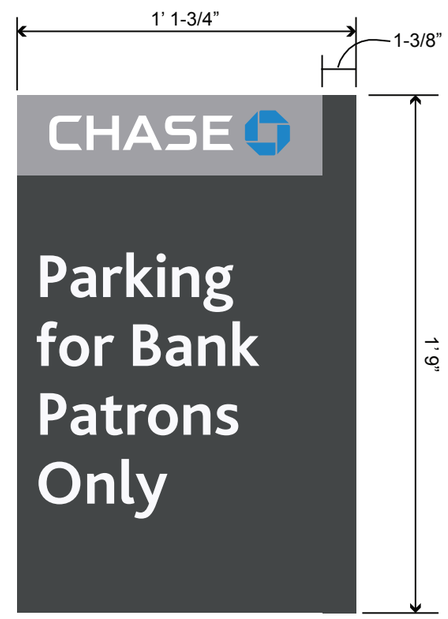
White w/ Blue Octagon Illum Channel Letters - 12.7 SF

Scale: NTS

NMP-XX

Dark Nickel Metal Pan - Dimensions TBD

Scale: NTS



TC-W-A-RE

Wall Mounted Regulatory Sign (Re-engineered) - 2sf

Scale: NTS

ADA-EP-NA

Handicapped Entrance Plaque - Not Accessible

Scale: NTS

CHASE

CHS.NB.961 - Arlington Mass Avenue

645 Massachusetts Avenue
Arlington, MA 02476

DESIGNER - JM

CREATED - 06.17.21

DRAWING - B95122

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ARCHITECTURAL RENDERING - CORNER ELEVATION

CHASE

CHS.NB.961 - Arlington Mass Avenue

645 Massachusetts Avenue
Arlington, MA 02476

DESIGNER - JM

CREATED - 06.17.21

DRAWING - B95122

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ARCHITECTURAL ELEVATION - MASSACHUSETTS AVENUE

CHASE

CHS.NB.961 - Arlington Mass Avenue

645 Massachusetts Avenue
Arlington, MA 02476

DESIGNER - JM

CREATED - 06.17.21

DRAWING - B95122

PHILADELPHIASIGN
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ARCHITECTURAL ELEVATION - DAVID LAMSON WAY ELEVATION

CHASE

CHS.NB.961 - Arlington Mass Avenue

645 Massachusetts Avenue
Arlington, MA 02476

DESIGNER - JM

CREATED - 06.17.21

DRAWING - B95122

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Survey Photos

CHASE

CHS.NB.961 - Arlington Mass Avenue

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Arlington, MA 02476

DESIGNER - JM

CREATED - 06.17.21

DRAWING - B95122

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Approach Photos

CHASE

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DESIGNER - JM

CREATED - 06.17.21

DRAWING - B95122

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From: "Jenny Raitt" <JRaitt@town.arlington.ma.us>
To: "Mary Muszynski" <MMuszynski@town.arlington.ma.us>
Cc: "Kelly Lynema" <KLynema@town.arlington.ma.us>
Date: 09/07/2021 11:18 AM
Subject: Fwd: Chase Bank - 645 Massachusetts Avenue, Arlington MA -Special permit

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Please post to the ARB page.

Jennifer Raitt
Director, Department of Planning and Community Development
Town of Arlington

Arlington values equity, diversity, and inclusion. We are committed to building a community where everyone is heard, respected, and protected.

From: Ian Heanue <iheanue@peconsultingcorp.com>
To: Jenny Raitt <JRaitt@town.arlington.ma.us>, Mary Muszynski <MMuszynski@town.arlington.ma.us>, Kelly Lynema <KLynema@town.arlington.ma.us>
Cc: Marc Sides <msides@core-states.com>, Kaitlyn Flynn <kflynn@core-states.com>
Date: Tue, 7 Sep 2021 14:42:38 +0000
Subject: Chase Bank - 645 Massachusetts Avenue, Arlington MA -Special permit

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Good Morning,

Please accept this email as an official request to have our special permit application (Docket #3665) for JPMorgan Chas NA Bank, at 645 Massachusetts Avenue, Arlington MA, continued to the next board meeting schedule on 9/27/2021.

We will work to have all updated documents submitted to your office by 9/22/2021 in order to meet the deadline for the 9/27 meeting.

If you have any questions or require any additional documentation please feel free to reach out.

Thank You,

Ian Heanue

Project Expediters Consulting Corp., *WBE, WBENC Certified*
Project Expediters Inc, *WBE, WBENC Certified*
Faneuil Hall Marketplace
4 South Market Street
Suite S-4035, Third Floor
Boston, MA 02109
Office: 617-227-0159
Cell: 908-907-0484
Fax: 617-227-0501

Town of Arlington, Massachusetts

Department of Planning & Community Development

730 Massachusetts Avenue, Arlington, Massachusetts 02476

Public Hearing Memorandum

The purpose of this memorandum is to provide the Arlington Redevelopment Board and public with technical information and a planning analysis to assist with the regulatory decision-making process.

To: Arlington Redevelopment Board

From: Jennifer Raitt, Secretary Ex Officio

Subject: Environmental Design Review, 645 Massachusetts Ave, Arlington, MA
Docket #3665

Date: August 24, 2021

I. Docket Summary

This is an application by JP Morgan Chase Bank NA, 1111 Polaris Parkway, Columbus, OH to operate a bank and replace the signage at 645 Massachusetts Avenue property owned by Key West Realty LLC, 63 Trapelo Road, Belmont, MA 02478. The opening of the Special Permit is to allow the Board to review and approve the use as a commercial bank branch with greater than 2,000 square feet in the B5 Central Business District, renovations to the building, and alterations to the façade under Section 3.4 Environmental Design Review (EDR). The prior use was a restaurant, a use that has been at this location since at least the 1970s.

Materials submitted for consideration of this application:

- Application for EDR Special Permit dated August 4, 2021;
- Project Narrative by Core States Group, dated July 28, 2021;
- Existing and Proposed Floor Plans and Photographs, prepared by KMS, dated June 9, 2021;
- Dimensional and Parking Information Sheet;
- Parking Plan;
- Sign Submittal Package, prepared by Philadelphia Sign, dated June 17, 2021; and
- Quitclaim Deed dated September 7, 2007.

II. Application of Special Permit Criteria (Arlington Zoning Bylaw, Section 3.3)

1. **Section 3.3.3.A.**
   The use requested is listed as a Special Permit in the use regulations for the applicable district or is so designated elsewhere in this Bylaw.

   A commercial bank of 2,000 square feet or more is allowed in the B5 Central Business District with a Special Permit under the jurisdiction of the ARB due to its location on Massachusetts Avenue. The Board can find that this condition is met.

2. **Section 3.3.3.B.**
   The requested use is essential or desirable to the public convenience or welfare.

   The use and the upgrades to the building and site are in the public's interest. However, banking uses can be found throughout the immediate area in Arlington Center, including four banks/ lending institutions and a number of stand-alone ATMs.

3. **Section 3.3.3.C.**
   The requested use will not create undue traffic congestion or unduly impair pedestrian safety.

   The proposed location is in the heart of Arlington Center. Many customers are likely to access this location by foot, bicycle, or use on-street parking along Massachusetts Avenue, or park in the public municipal lot behind the structure. The use will not impair pedestrian safety any more than the prior restaurant use. The Board can find that this condition is met.

4. **Section 3.3.3.D.**
   The requested use will not overload any public water, drainage or sewer system or any other municipal system to such an extent that the requested use or any developed use in the immediate area or in any other area of the Town will be unduly subjected to hazards affecting health, safety, or the general welfare.

   The bank does not have a high demand need for water or sewer. The Board can find that this condition is met.

5. **Section 3.3.3.E.**
   Any special regulations for the use as may be provided in the Bylaw are fulfilled.

   All such regulations are fulfilled.

6. **Section 3.3.3.F.**
   The requested use will not impair the integrity or character of the district or adjoining districts, nor be detrimental to the health or welfare.

Multiple other banks are located within the vicinity and have had no detrimental impact on the integrity or character of the neighborhood, district, or adjoining districts, or on the health and welfare of the community. The Board can find that this condition is met.

7. **Section 3.3.3.G.**
The requested use will not, by its addition to a neighborhood, cause an excess of the use that could be detrimental to the character of said neighborhood.

At present, there are four different bank branches and additional ATMs in Arlington Center. The site of this proposed use is directly adjacent to another bank branch location. The prior restaurant use contributed to an active, lively streetscape in the afternoon and evening and provided meals tax income to the Town, neither of which can be said of the proposed use.

III. Environmental Design Review Standards (Arlington Zoning Bylaw, Section 3.4)

1. **EDR-1 Preservation of Landscape**
The landscape shall be preserved in its natural state, insofar as practicable, by minimizing tree and soil removal, and any grade changes shall be in keeping with the general appearance of neighboring developed areas.

The project site is developed, contains a multi-tenant building, and is entirely impervious. The landscaping around the perimeter of the site will remain in its current state. The Board can find that this condition is met.

2. **EDR-2 Relation of the Building to the Environment**
Proposed development shall be related harmoniously to the terrain and to the use, scale, and architecture of the existing buildings in the vicinity that have functional or visible relationship to the proposed buildings. The Arlington Redevelopment Board may require a modification in massing so as to reduce the effect of shadows on the abutting property in an R0, R1 or R2 district or on public open space.

The existing storefront and entry will be renovated, and a second rear entry will be introduced. Additional windows will be introduced to the façade facing David Lamson Way, increasing the transparency of the ground floor. New signage will be introduced. These updates will rehabilitate the existing storefront consistent with the style of the building and neighborhood.

The applicant proposes to eliminate the awning over the Mass Ave and David Lamson Way façades of the building which was added by the prior tenant. The presence of awnings along Mass Ave is desirable, as they provide shelter for pedestrians in

inclement weather, are within the design vernacular of other storefronts along the corridor and mediate the stark façade of the structure.

The applicant should confirm that the "proposed new storefront" callout along David Lamson Way on floor plans on DRC pages 7 and 8 refer to the new commercial windows and not operable service windows.

3. **EDR-3 Open Space**
All open space (landscaped and usable) shall be so designed as to add to the visual amenities of the vicinity by maximizing its visibility for persons passing by the site or overlooking it from nearby properties. The location and configuration of usable open space shall be so designed as to encourage social interaction, maximize its utility and facilitate maintenance.

There are no changes proposed to the existing building site and currently no open space on the project site. The Board can find that this condition is met.

4. **EDR-4 Circulation**
With respect to vehicular and pedestrian and bicycle circulation, including entrances, ramps, walkways, drives, and parking, special attention shall be given to location and number of access points to the public streets (especially in relation to existing traffic controls and mass transit facilities), width of interior drives and access points, general interior circulation, separation of pedestrian and vehicular traffic, access to community facilities, and arrangement of vehicle parking and bicycle parking areas, including bicycle parking spaces required by Section 6.1.12 that are safe and convenient and, insofar as practicable, do not detract from the use and enjoyment of proposed buildings and structures and the neighboring properties.

The number of parking spaces will increase from zero to three. The new use requires eight spaces, therefore the applicant requests relief from the Zoning Bylaw parking requirement. The site is adjacent to available on-street parking along Massachusetts Avenue and the public parking in the Railroad lot. The ARB has jurisdiction to reduce the number of parking spaces required by the Zoning Bylaw.

The applicant does not propose to add handicap parking, nor is the proposed rear entry to the building ADA accessible. Under this proposal, accessible access to the building would require parking along Mass Ave and travelling in the right of way until reaching a curb cut, or parking in the Railroad lot and circumnavigating the entire east and south sides of the building before reaching the accessible entry and designate one of the three parking spaces as HC accessible.

The circulation on the site will change with the addition of the second entry to the building off the Railroad lot. The application materials indicate an assumption that 50% of customers will use this entrance. The applicant is strongly encouraged to make the rear entry ADA accessible for the reasons described above.

Related to bicycle parking, the "office, business, or professional use" requires two long term and two short term bicycle parking spaces. The applicant has requested an exemption from the bike parking bylaw, citing bike racks along Massachusetts Avenue and in the Railroad lot as reasonable provision of parking. No long-term bicycle parking is indicated in the application.

The applicant should provide both indoor long-term bicycle parking for employees and short-term parking for visitors. The proximity of the business to the Minuteman Bikeway increases the demand for bicycle parking. For short term parking, the addition of a bike rack to the Massachusetts Avenue or the rear side of the building is recommended, which would support bicycle access to Arlington Center overall. Long term bicycle parking should be provided in an enclosed or covered area or inside the building. Further, indoor bicycle storage shall be included on the plan.

5. **<u>EDR-5 Surface Water Drainage</u>**

Special attention shall be given to proper site surface drainage so that removal of surface waters will not adversely affect neighboring properties or the public storm drainage system. Available Best Management Practices for the site should be employed, and include site planning to minimize impervious surface and reduce clearing and re-grading. Best Management Practices may include erosion control and stormwater treatment by means of swales, filters, plantings, roof gardens, native vegetation, and leaching catch basins. Stormwater should be treated at least minimally on the development site; that which cannot be handled on site shall be removed from all roofs, canopies, paved and pooling areas and carried away in an underground drainage system. Surface water in all paved areas shall be collected in intervals so that it will not obstruct the flow of vehicular or pedestrian traffic and will not create puddles in the paved areas.

In accordance with Section 3.3.4., the Board may require from any applicant, after consultation with the Director of Public Works, security satisfactory to the Board to insure the maintenance of all stormwater facilities such as catch basins, leaching catch basins, detention basins, swales, etc. within the site. The Board may use funds provided by such security to conduct maintenance that the applicant fails to do.

The Board may adjust in its sole discretion the amount and type of financial security such that it is satisfied that the amount is sufficient to provide for any future maintenance needs.

There will be no changes to the exterior of the building or surface water run-off as a result of this proposal. The Board can find that this condition is met.

6. **<u>EDR-6 Utilities Service</u>**

Electric, telephone, cable TV, and other such lines of equipment shall be underground. The proposed method of sanitary sewage disposal and solid waste disposal from all buildings shall be indicated.

There will be no changes to the utility service as a result of this proposal, which will affect only the interior of the building. The Board can find that this condition is met.

7. **EDR-7 Advertising Features**
The size, location, design, color, texture, lighting and materials of all permanent signs and outdoor advertising structures or features shall not detract from the use and enjoyment of proposed buildings and structures and the surrounding properties.

The project site is in the Business Sign District. The proposal exceeds the total number of signs allowable by right. The applicant submitted a sign package with a number of primary and incidental signs proposed:

- One wall sign with illuminated channel lettering located over the main entrance measuring 20.7 square feet;
- One wall sign with illuminated channel lettering located over the rear entrance measuring 12.7 square feet;
- One illuminated projecting sign measuring 8.17 square feet on each side;
- Three (3) wall mounted regulatory signs in parking area measuring 2 square feet each;
- One ADA entrance plaque measuring 0.25 square feet; and
- One ADA directional sign measuring 0.45 square feet.

The existing awning will be removed and the sign band across the Mass Ave and David Lamson Way façades will be covered with a dark nickel metal panel three feet in height.

While the narrative description addresses the two proposed wall signs, it does not describe the projecting sign, incidental signage, or other façade elements such as the new windows on David Lamson way and the dark nickel metal panels on all three facades.

Per Section 6.2.2(C), the ARB may grant a Special Permit to allow more than the number of signs allowed, "provided the architecture of the building, the location of the building relative to the street, or the nature of the use being made of the building is such that an additional sign or signs of a larger size should be allowed in the public interest."

In terms of sign area, the two wall signs and the projecting sign comply with Section 6.2.5 of the Zoning Bylaw if the dark metal panels (E02, E03, and E06 in the sign package), for which dimensions are not provided, are not counted toward the total square footage of allowed signage. Dimensional information for the elements on all

three façades, including the mounting height of the projecting sign, are required to determine compliance with Sections 6.2.5(D)(8) and 6.2.5(D)(10).

Overall, the aggregate square footage of incidental signage exceeds what is allowed per Section 6.2.1(E)(3) by 0.076 square feet. The applicant has not proposed any window signage, however any intended signage including hours of operation and logos on entryways needs to be included in the sign package. Any additional window signage or incidental signage would add to the excess of the six feet of allowed incidental signage.

8. **EDR-8 Special Features**

Exposed storage areas, exposed machinery installations, service areas, truck loading areas, utility buildings and structures, and similar accessory areas and structures shall be subject to such setbacks, screen plantings or other screening methods as shall reasonably be required to prevent their being incongruous with the existing or contemplated environment and the surrounding properties.

There will be no adverse impacts on light, air and water resources, or on noise and temperature levels. The Board can find that this condition is met.

9. **EDR-9 Safety**

With respect to personal safety, all open and enclosed spaces shall be designed to facilitate building evacuation and maximize accessibility by fire, police and other emergency personnel and equipment. Insofar as practicable, all exterior spaces and interior public and semi-public spaces shall be so designed to minimize the fear and probability of personal harm or injury by increasing the potential surveillance by neighboring residents and passersby of any accident or attempted criminal act.

The renovation of the space at 645 Massachusetts Avenue will conform to code requirements for safety and accessibility by emergency personnel and equipment. The Board can find that this condition is met.

10. **EDR-10 Heritage**

With respect to Arlington's heritage, removal or disruption of historic, traditional or significant uses, structures or architectural elements shall be minimized insofar as practical whether these exist on the site or on adjacent properties.

The building containing 645 Massachusetts Avenue is listed on the *Inventory of Historically or Architecturally Significant Properties in the Town of Arlington* and is under the jurisdiction of the Arlington Historical Commission. The Historical Commission will review the proposal and this permit shall be conditioned on their approval.

11. **EDR-11 Microclimate**

With respect to the localized climatic characteristics of a given area, any development which proposes new structures, new hard surface, ground coverage or

the installation of machinery which emits heat, vapor or fumes shall endeavor to minimize insofar as practicable, any adverse impacts on light, air and water resources or on noise and temperature levels of the immediate environment.

There are no proposed changes (new structures, hard surface, ground coverage, or machinery) that will impact the microclimate. The Board can find that this condition is met.

12. <u>EDR-12 Sustainable Building and Site Design</u>
Projects are encouraged to incorporate best practices related to sustainable sites, water efficiency, energy and atmosphere, materials and resources, and indoor environmental quality. Applicants must submit a current Green Building Council Leadership in Energy and Environmental Design (LEED) checklist, appropriate to the type of development, annotated with narrative description that indicates how the LEED performance objectives will be incorporated into the project.

The construction will be primarily indoors, and will prioritize energy efficient HVAC systems, plumbing fixtures, LED fixtures, and products incorporating low/no VOCs and recycled content. The Board can find that this condition is met.

IV. Conditions

1. The final sign, exterior material, and lighting plans shall be administratively approved by the Department of Planning and Community Development. Any substantial or material deviation during construction from the approved plans and specifications is subject to the written approval of the Arlington Redevelopment Board

2. Any substantial or material deviation during construction from the approved plans and specifications is subject to the approval of the Arlington Redevelopment Board.

3. The Board maintains continuing jurisdiction over this permit and may, after a duly advertised public hearing, attach other conditions or modify these conditions as it deems appropriate in order to protect the public interest and welfare.

4. Snow removal from all parts of the site, as well as from any abutting public sidewalks, shall be the responsibility of the owner and shall be accomplished in accordance with Town Bylaws.

5. Trash shall be picked up only on Monday through Friday between the hours of 7:00 am and 6:00 pm. All exterior trash and storage areas on the property, if any, shall be properly screened and maintained in accordance with the Town Bylaws.

6. Upon the issuance of the building permit the Applicant shall file with the Inspectional Services Department and the Police Department the names and telephone numbers of contact personnel who may be reached 24 hours each day during the construction period.

PLANNING & COMMUNITY DEVELOPMENT

2021 AUG 4 P 2:18

TOWN OF ARLINGTON
REDEVELOPMENT BOARD
Application for Special Permit In Accordance with Environmental Design Review Procedures (Section 3.4 of the Zoning Bylaw)

Docket No. 3665

1. Property Address 645 Massachusetts Avenue, Arlington MA 02476
   Name of Record Owner(s) Key West Realty LLC Phone
   Address of Owner 63 Trapelo Road, (Street), Belmont MA 02478 (City, State, Zip)

2. Name of Applicant(s) (if different than above) JPMorgan Chase Bank NA
   Address 1111 Polaris Parkway Columbus OH 43240 Phone c/o 617.874.0131
   Status Relative to Property (occupant, purchaser, etc.) Occupant

3. Location of Property 645 Massachusetts Avenue, Arlington MA 02476 05-07-7
   Assessor's Block Plan, Block, Lot No.

4. Deed recorded in the Registry of deeds, Book 50073, Page 393;
   -or- registered in Land Registration Office, Cert. No. _____, in Book _____, Page _____.

5. Present Use of Property (include # of dwelling units, if any) Presently a restaurant

6. Proposed Use of Property (include # of dwelling units, if any) Proposed Commercial Bank Branch

7. Permit applied for in accordance with the following Zoning Bylaw section(s)

section(s)	title(s)
3.4	Environmental design review
6.1.4	Table of Off-Street Parking Regulations
6.1.12	Bicycle Parking

RECEIVED 2021 AUG -6 AM 9:59 TOWN CLERK'S OFFICE ARLINGTON, MA 02174

8. Please attach a statement that describes your project and provide any additional information that may aid the ARB in understanding the permits you request. Include any reasons that you feel you should be granted the requested permission.

(In the statement below, strike out the words that do not apply)

The applicant states that _____ is the owner -or- occupant -or- purchaser under agreement of the property in Arlington located at 645 Massachusetts Avenue, Arlington MA 02476 which is the subject of this application; and that unfavorable action -or- no unfavorable action has been taken by the Zoning Board of Appeals on a similar application regarding this property within the last two years. The applicant expressly agrees to comply with any and all conditions and qualifications imposed upon this permission, either by the Zoning Bylaw or by the Redevelopment Board, should the permit be granted.

Matthew McCool - Vice President
JPMorganChase Bank, N.A

Signature of Applicant(s)

100 International Dr, Floor 21
Baltimore, MD, 21202-4673, United States

Address

410-949-2150

Phone

Town of Arlington Redevelopment Board
Application for Special Permit in accordance with
Environmental Design Review (Section 3.4)

Required Submittals Checklist

Two full sets of materials and one electronic copy are required. A model may be requested. Review the ARB's Rules and Regulations, which can be found at arlingtonma.gov/arb, for the full list of required submittals.

_X_ Dimensional and Parking Information Form (see attached)

_X_ Site plan of proposal

_NA_ Model, if required

_X_ Drawing of existing conditions

_X_ Drawing of proposed structure

_NA_ Proposed landscaping. May be incorporated into site plan

_X_ Photographs

_X_ Impact statement

_X_ Application and plans for sign permits

_NA_ Stormwater management plan (for stormwater management during construction for projects with new construction

FOR OFFICE USE ONLY

_____ Special Permit Granted Date: _____

_____ Received evidence of filing with Registry of Deeds Date: _____

_____ Notified Building Inspector of Special Permit filing Date: _____

TOWN OF ARLINGTON
Dimensional and Parking Information
for Application to
The Arlington Redevelopment Board

Docket No. _____

Property Location Corner of Mass Ave/David Lamson Way

Zoning District B-5

Owner: Key West Realty LLC

Address: 645 Massachusetts Ave

Present Use/Occupancy: No. of Dwelling Units:
Restaurant/Bar

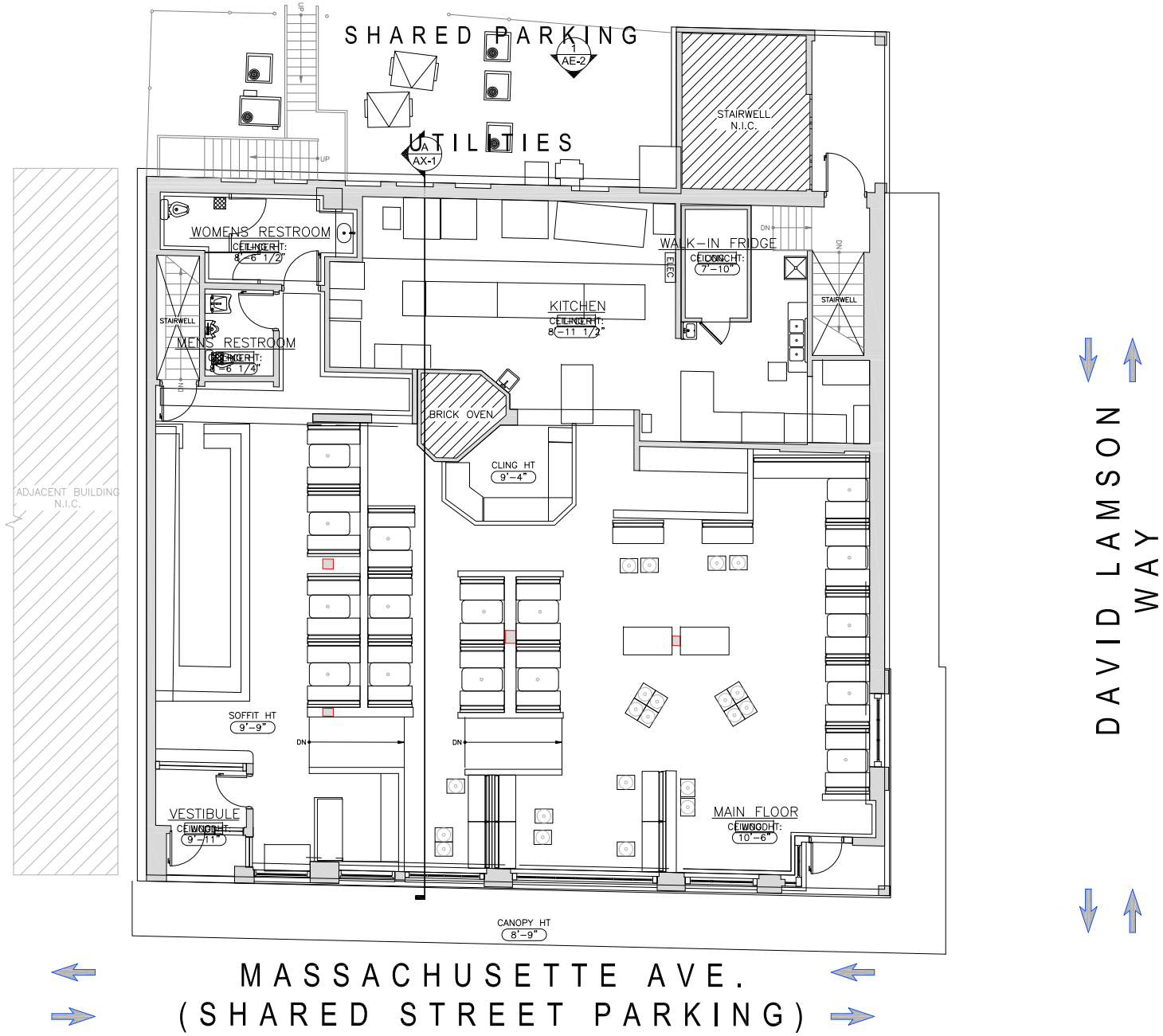
Uses and their gross square feet:
6400 sf

Proposed Use/Occupancy: No. of Dwelling Units:
Financial Center > 2000sf

Uses and their gross square feet:
3826 sf (first floor only)

	Present Conditions	Proposed Conditions	Min. or Max. Required by Zoning for Proposed Use
Lot Size	6400sf	3826sf	min. >2,000 sf (with special permit)
Frontage	63'-5"	63'-5"	min.
Floor Area Ratio			max.
Lot Coverage (%), where applicable	N/A	N/A	max.
Lot Area per Dwelling Unit (square feet)	N/A	N/A	min.
Front Yard Depth (feet)	N/A	N/A	min.
Side Yard Width (feet) right side	N/A	N/A	min.
left side	N/A	N/A	min.
Rear Yard Depth (feet)	N/A	N/A	min.
Height			min.
Stories	2	2	stories Existing
Feet	31'-6"	31'-6"	feet Existing
Open Space (% of G.F.A.)			min.
Landscaped (square feet)	N/A	N/A	(s.f.)
Usable (square feet)	N/A	N/A	(s.f.)
Parking Spaces (No.)	0	3	min. 8 (based on 1 per 500sf)
Parking Area Setbacks (feet), where applicable	N/A	N/A	min.
Loading Spaces (No.)	0	0	min.
Type of Construction	IIIA		
Distance to Nearest Building	0-Adjacent	0-Adjacent	min. Existing


Proposed (3) parking spaces dedicated to Chase Bank
Existing Bike Racks
Water St
Massachusetts Ave
David Lawson Way
Peg Spengler Way
Mystic St
Winslow St



CHASE

EXISTING FLOOR PLAN

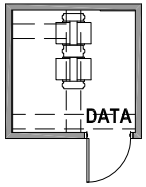
ARLINGTON MASS AVE

645 Mass Ave - Arlington, MA 02476

DATE	DESIGNER	AREA	SCALE
09 JUN 21	KMS	AREA sq. ft.	NONE

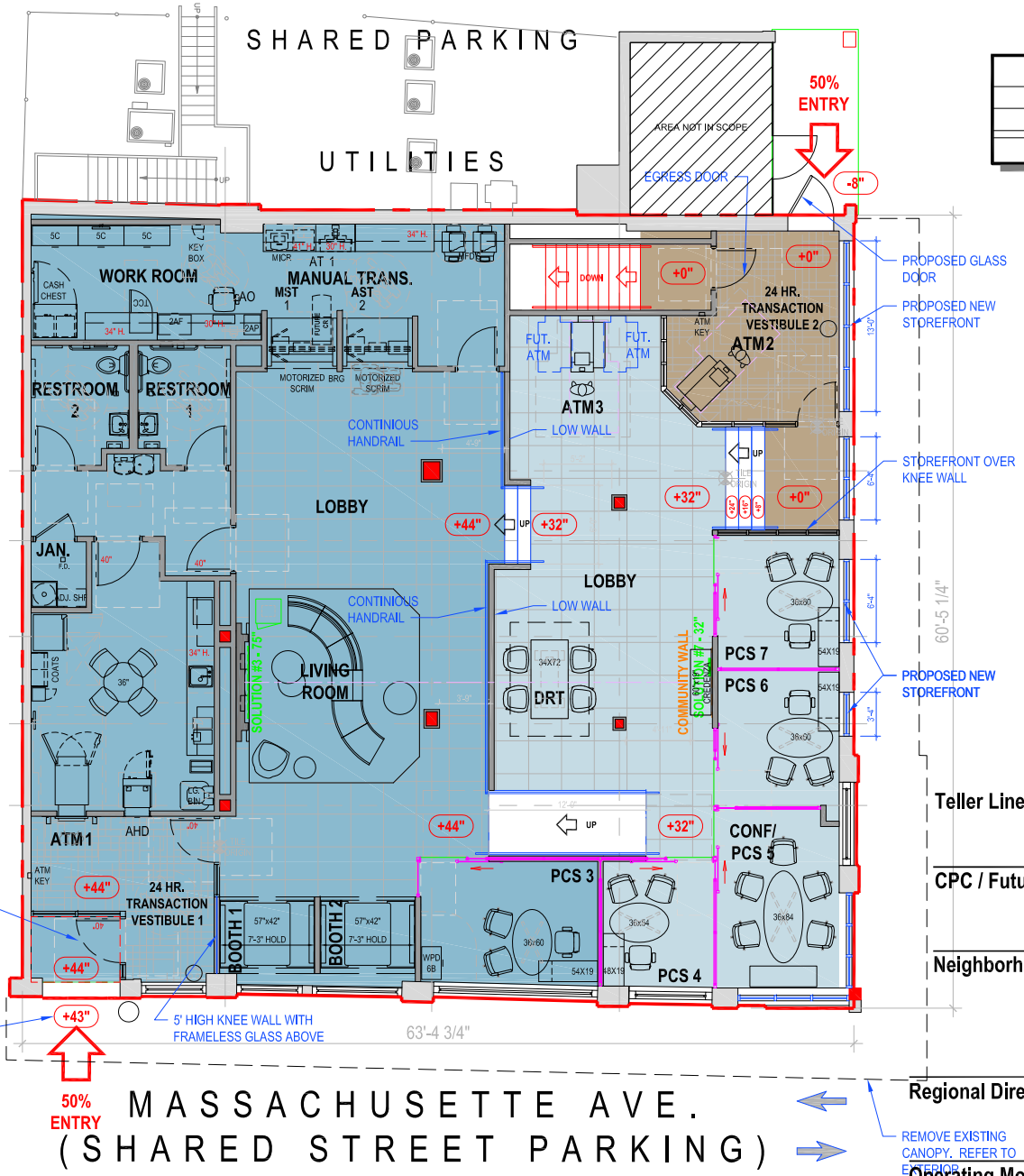
INFORMATION ONLY

REPRESENTED FLOOR PLAN PRECEDES AOR'S FINAL DD / CODE COMPLIANCE VALIDATION. FINAL CONFIGURATION MAY VARY.
NOTE TO AOR; DO NOT PROCEED WITH CONSTRUCTION DOCUMENTS IF THIS STAMP IS PRESENT.



DATA AND ELECTRICAL ROOM IN LOWER LEVEL

ELEC. PNL.



AREA CALCULATIONS (S.F.)	
RETAIL BANK - MAIN LEVEL	3826
LOWER LEVEL	3862
TOTAL (S.F.)	7688

DESIGN STANDARDS
20.3

HARDINESS ZONE	7A
OVERHEAD DOOR ACCEPTABLE *	

* HARDINESS ZONES >/= 7A

Teller Line BRG YES ☐ NO ☐

CPC / Future CPC Office

Neighborhood Name

Regional Director (RD) DATE

Operating Model Lead (OML) DATE

CHASE

PROPOSED FLOOR PLAN

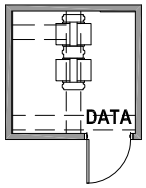
ARLINGTON MASS AVE

645 Mass Ave - Arlington, MA 02476

DATE	DESIGNER	AREA	SCALE
09 JUN 21	KMS	SEE ABOVE	NONE

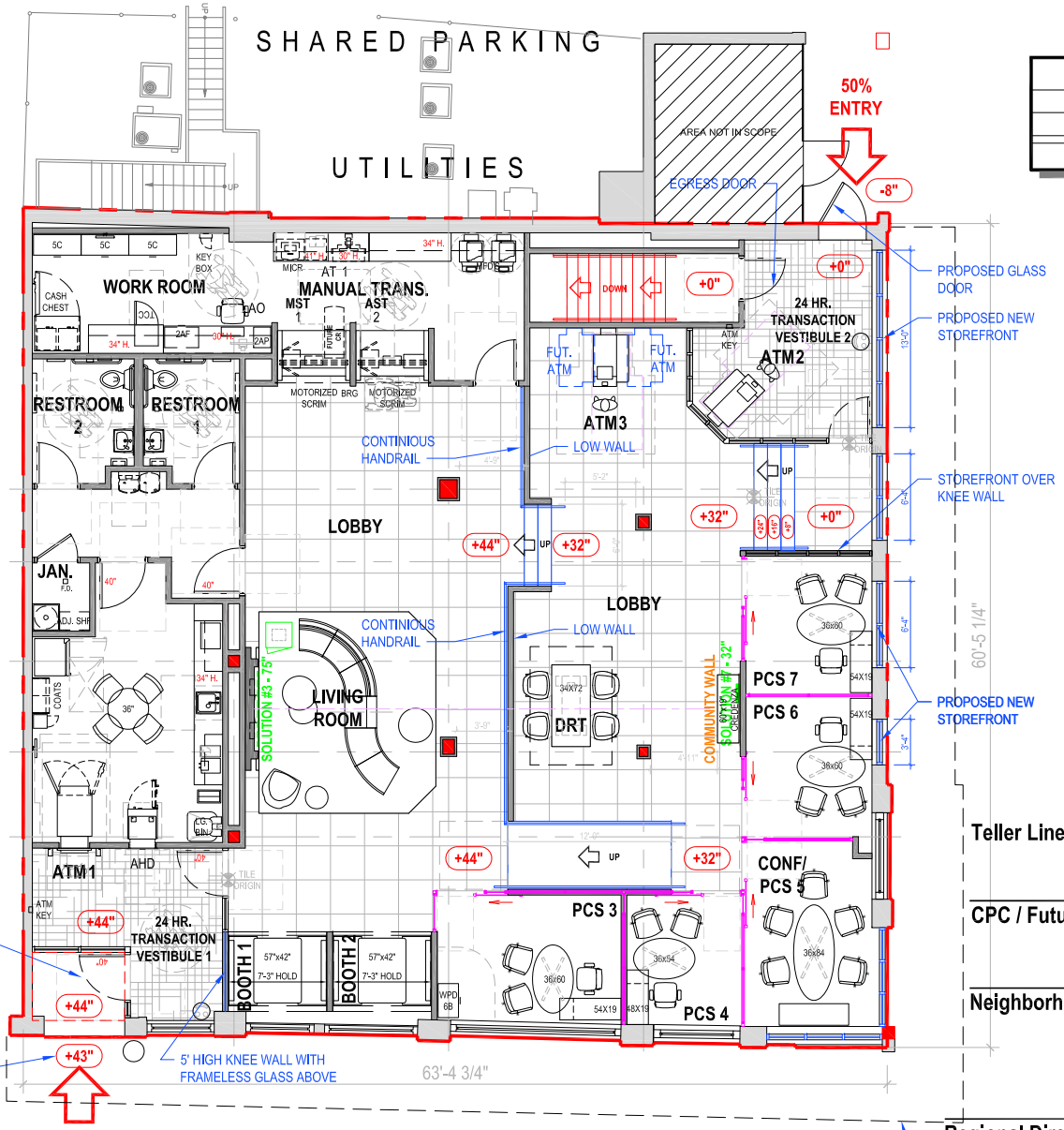
INFORMATION ONLY

REPRESENTED FLOOR PLAN PRECEDES AOR'S FINAL DD / CODE COMPLIANCE VALIDATION. FINAL CONFIGURATION MAY VARY.
NOTE TO AOR; DO NOT PROCEED WITH CONSTRUCTION DOCUMENTS IF THIS STAMP IS PRESENT.



DATA AND ELECTRICAL ROOM IN LOWER LEVEL

ELEC. PNL.



AREA CALCULATIONS (S.F.)	
RETAIL BANK - MAIN LEVEL	3826
LOWER LEVEL	3862
TOTAL (S.F.)	7688

DESIGN STANDARDS
20.3

HARDINESS ZONE	7A
OVERHEAD DOOR ACCEPTABLE *	

* HARDINESS ZONES >/= 7A

Teller Line BRG YES ☐ NO ☐

CPC / Future CPC Office

Neighborhood Name

Regional Director (RD) DATE

Operating Model Lead (OML) DATE

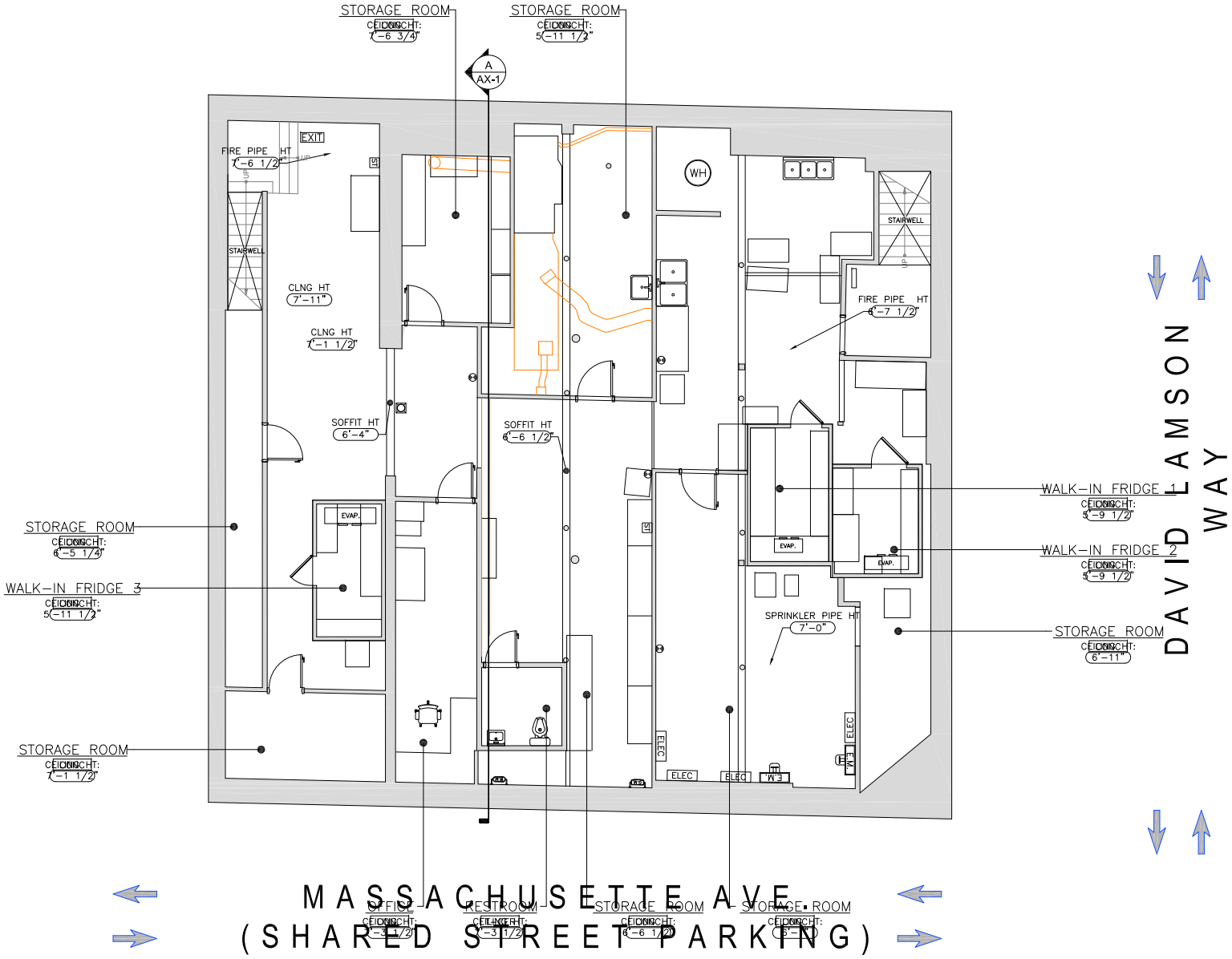
CHASE

PROPOSED FLOOR PLAN

ARLINGTON MASS AVE

645 Mass Ave - Arlington, MA 02476

DATE	DESIGNER	AREA	SCALE
09 JUN 21	KMS	SEE ABOVE	NONE



CHASE

EXISTING LOWER LEVEL FLOOR PLAN

ARLINGTON MASS AVE

645 Mass Ave - Arlington, MA 02476

DATE	DESIGNER	AREA	SCALE
09 JUN 21	KMS	AREA sq. ft.	NONE


COLDWELL BANKER
RESIDEN

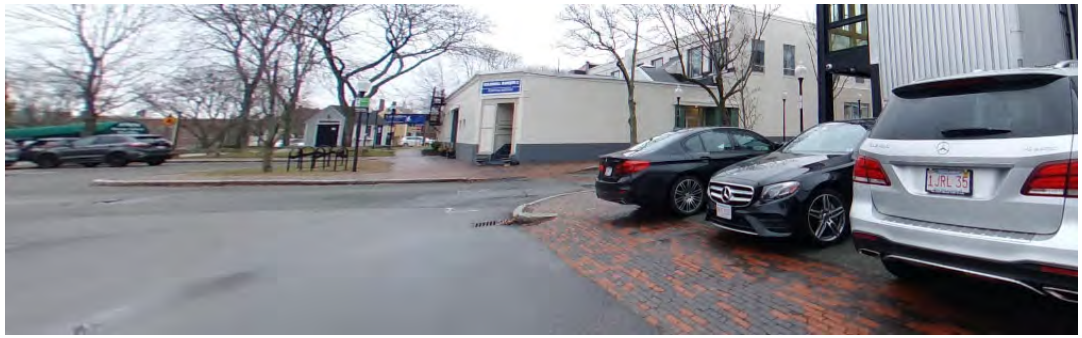

YOUR JOE'S


Rear egress (Not ADA Compliant)
Tenant will have exclusive rights to 3/5 parking spots (TBD)

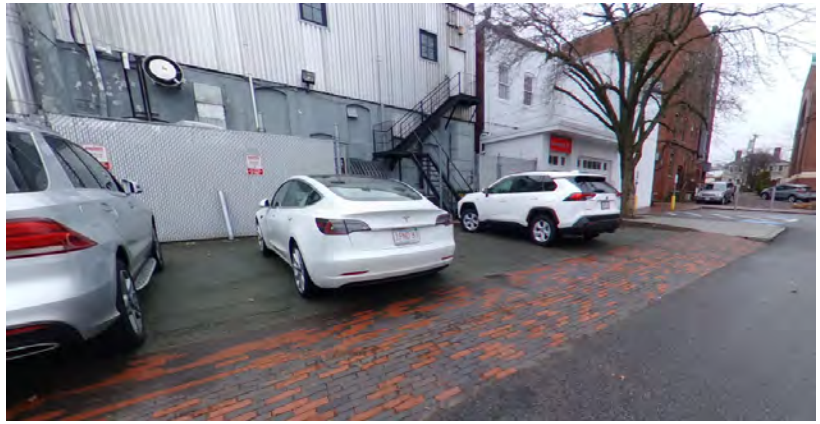
View from Massachusette Ave

Views from David Lamson Way

View from municipal parking



Caldwell Bankers

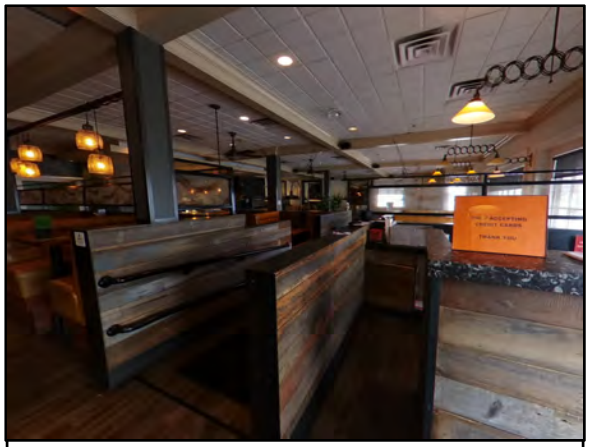


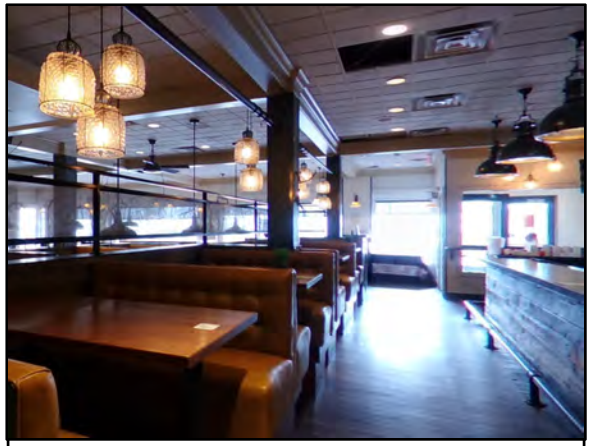
Bank of America

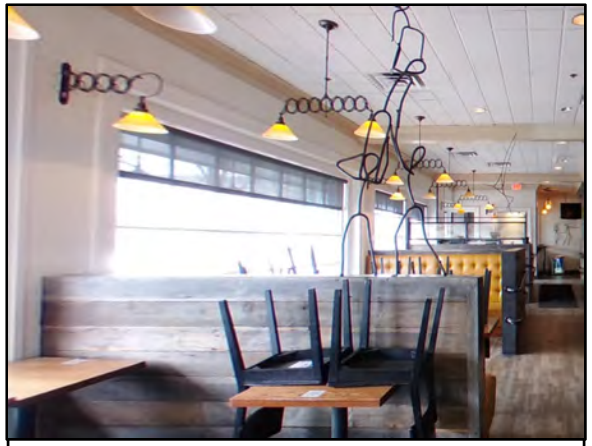
Rear entrance

Arlington Mass Ave

CHASE

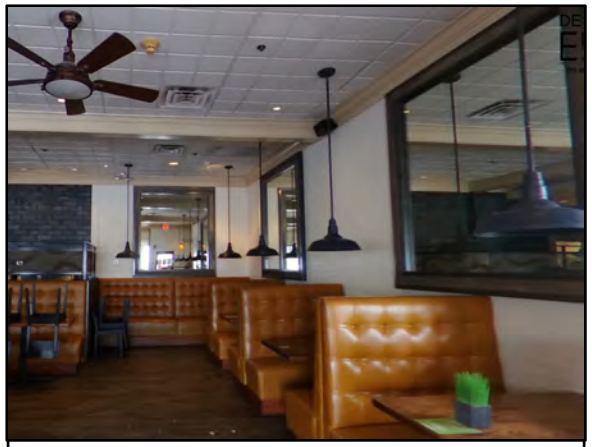






Arlington Mass Ave

CHASE



CORE STATES GROUP

July 28, 2021

Town of Arlington
Redevelopment Board
730 Mass Ave. Annex
Arlington, MA 02476

Re: Application for Special Permit Review, 645 Massachusetts Avenue

The whom it may concern;
The following information is regarding a proposed Chase Bank facility at 645 Massachusetts Avenue, Arlington MA. The project will entail the installation of a new financial center in the location formerly occupied by Not Your Average Joe's restaurant and bar. The project will involve work on the first floor of the building and is an interior renovation, not exceeding the building limits.

Please see below, provided to meet the informational requests of the Petition for Special Permit under Environmental Design Review, per section 3.4 of the Arlington Zoning Bylaw for Applicability.

1. Preservation of Landscape: The proposed Chase Bank will be within the existing building limits. As such, the proposed project will not impact any of the existing landscape.
2. Relation of buildings to environment: The proposed project is largely contained in the first floor of the existing two-storey building, so the relationship of the existing building to the adjacent structures will be maintained. Additionally, exterior materials (largely brick) will be maintained, retaining the current building's character.
3. Open Space: The proposed Chase Bank will be within the existing building limits, largely on the first floor of the existing two-storey building. As such, the proposed project will not alter any existing open space.
4. Circulation: The proposed Chase Bank will be within the existing building limits, largely on the first floor of the existing two-storey building. As such, the proposed project will not alter any existing circulation. The bank will have two entrances, one in the front, one in the rear facing the parking lot.
5. Surface Water Drainage: The proposed Chase Bank will be within the existing building limits, largely on the first floor of the existing two-storey building. As such, the proposed project will not impact any existing surface water drainage, not will it contribute any new load to the drainage systems.
6. Utility Service: Chase Bank intends to reuse the existing utilizes that were feed the prior tenant (Not Your Average Joe's). This includes electrical, gas, water, sewer, and fire sprinkler services.

The bank, in all likelihood, will use significantly less utilities and resources than the prior restaurant use.

7. Advertising Features: The bank is proposing a set of 18" internally illuminated channels over the entrance on Massachusetts Avenue. The bank is also proposing a set of 14" internally illuminated letters at the rear entrance, facing the parking lot. The (2) building letter sets comply with section 6.2.8 of the zoning by-laws. Refer to attached for additional information on the proposed signage package.
8. Special Features: The proposed Chase Bank will be within the existing building limits, largely on the first floor of the existing two-storey building. The bank proposes to remove the existing awning from the front and side of the building as part of the project. While the existing equipment area in the rear of the building is to remain, the bank use will require significantly less equipment, and will remove any of those features made obsolete by the conversion such as the exhaust system, exterior storage, and food waste containment.
9. Safety: The proposed Chase Bank will be within the existing building limits, largely on the first floor of the existing two-storey building. The existing life safety systems, including fire alarm and fire sprinkler, will be modified as needed based on the proposed work to maintain all current and required life safety requirements. The project will not impact the exterior sidewalks, posing no impact to public safety beyond the confines of the building.
10. Heritage: The proposed Chase Bank will be within the existing building limits, largely on the first floor of the existing two-storey building. The building will mostly maintain the existing building materials and character, as not to impact the character of the building or the streetscape.
11. Microclimate: The proposed Chase Bank will be within the existing building limits, largely on the first floor of the existing two-storey building. As such, the proposed project will not pose any new impact on the microclimate of the area.
12. Sustainable building and Site Design: Chase Bank incorporates sustainable measures into their projects. While the building is not a LEED building Chase still takes measures in the form of energy efficient HVAC systems, efficient plumbing fixtures, LED light fixtures, and products incorporating low/no VOCs and recycled content.

In addition to the above, Chase bank is seeking relief on the parking requirements in the Zoning By-Laws. Section 6.1.4 of the By-Laws indicate that a business use would be required to provide 1 space per 500sf of gross floor area. Since the proposed Chase Bank space is 3,826sf on the first floor (customer floor), that would require 8 parking spaces. The landlord is providing the bank with 3 dedicated spaces for their use, which falls short of the 8 space requirement. Since the Chase Bank space has on-street parking directly in front along Massachusetts Avenue and a large municipal parking lot directly behind, Chase Bank is seeking relief from the requirement in the By-Laws.

Chase bank is seeking relief on the bicycle parking requirements in the Zoning By-Laws. Section 6.1.12 of the By-Laws indicate that a business use would be required to provide .3 spaces per 1000sf of gross floor area. . Since the proposed Chase Bank space is 3,826sf on the first floor (customer floor), that would require 1.2 spaces (rounds up to 2). Since the Chase Bank space has a bicycle rack in the sidewalk directly

in front along Massachusetts Avenue and 6 racks in the large municipal parking lot directly behind, as well as pay per ride bicyles, Chase Bank is seeking relief from the requirement in the By-Laws.

Please let us know should any further information be required.

Sincerely,

James Lalli
Director of Architecture - Financial
908.462.9949 | jlalli@core-states.com

Project Details

Project Name:	Arlington Mass Ave P373597	DND#:	0
Branch Name:		Region:	
		Market:	
Document Name:	20210701_Arlington Mass Ave_Brand Book_DRC PENDING.pdf	Address:	
		City:	
DRC Date:	07/06/2021	State:	
OVP#:	38100P373597	Zip:	
Program:	New Build Program		

Project Program:

- [ ] ATM - Offline
- [ ] BBI - Offline
- [ ] Large Cap Retrofit
- [ ] Merch Only – Offline
- [x] NB - Regular or Expansion
- [ ] Path
- [ ] Path of Travel - Offline
- [ ] Relocations
- [ ] Retrofit - BAU - Offline
- [ ] Retrofit - Community - Offline
- [ ] Retrofit - Companion - Offline
- [ ] RSU
- [ ] Signage - Offline

Project Type: New Build Signage/Elevation

Designer: Kanishka Moham Salehi

Status: Approved

Comment	By	On
AV Approved. If AV changes during any phase of the project contact Allison Howard	Allison Miche Howard	7/2/2021

Approver	Approval Status	Responded
Ashlee Jo Kelly	Approved	07/08/2021 15:38
Robert John Placek	Approved	07/06/2021 11:14
Tiffany Anne McLeod	Approved	07/08/2021 08:24

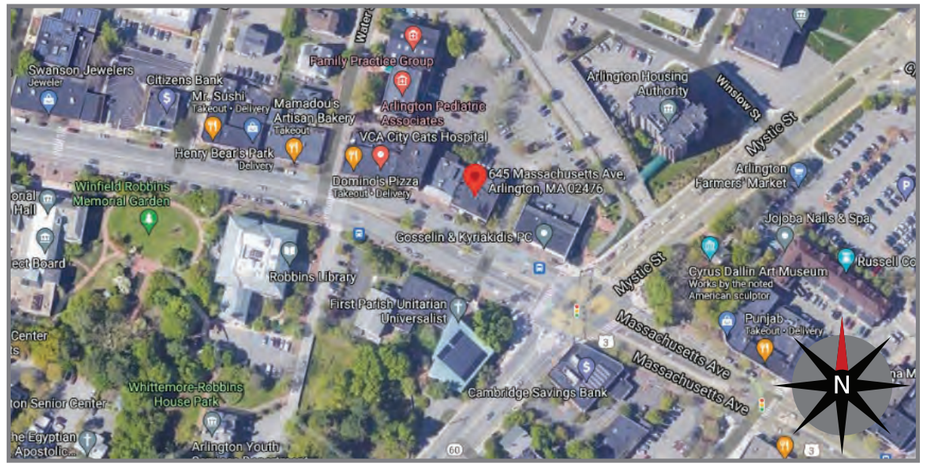
CHS.NB.961

ARLINGTON MASS AVENUE

645 Massachusetts Avenue
Arlington, MA 02476

REVISION NOTES:

06.23.21	JM	Delete Octagons from All Awnings. Add Option 2, E12 and E13
07.01.21	RJW	Revised as noted.



B95122

PHILADELPHIASIGN
BRINGING THE WORLD'S BRANDS TO LIFE

Aerial Plan

ARLINGTON, MA

Atlantic RETAIL

ATLANTICRETAIL.COM

CHASE

CHS.NB.961 - Arlington Mass Avenue

645 Massachusetts Avenue
Arlington, MA 02476

DESIGNER - JM

CREATED - 06.17.21

DRAWING - B95122

PHILADELPHIASIGN
BRINGING THE WORLD'S BRANDS TO LIFE
707 WEST SPRING GARDEN ST • PALMYRA, NJ • 08065
P: 856-829-1460 • F: 856-829-8549 • WEB: http://www.philadelphiasign.com

Site Plan

Exterior Scope of Work

E01	LIF-R-BLK-18-LED	Black w/ Blue Oct Day/Night Illum Chnl Ltrs - Top Mtd Rcwy	20.7sf
E02	NMP-XX	Dark Nickel Metal Panel	
E03	F-6	Double-Faced Illuminated Flag Mounted Sign	8.2sf
E04	NMP-XX	Dark Nickel Metal Panel	
E05	LIF-WBO-14-LED	White w/ Blue Octagon Illum Channel Letters	12.7sf
E06	NMP-XX	Dark Nickel Metal Panel	
E07	TC-W-A-RE	Wall Mtd Regulatory Sign - Bank Patrons Only (Re-Engineered)	2sf
E08	TC-W-A-RE	Wall Mtd Regulatory Sign - Bank Patrons Only (Re-Engineered)	2sf
E09	TC-W-A-RE	Wall Mtd Regulatory Sign - Bank Patrons Only (Re-Engineered)	2sf

ALL SIGNS TO BE REVIEWED/APPROVED BY HRC

N

Exterior Signs

Interior Signs

CHASE

CHS.NB.961 - Arlington Mass Avenue

645 Massachusetts Avenue
Arlington, MA 02476

DESIGNER - JM

CREATED - 06.17.21

DRAWING - B95122

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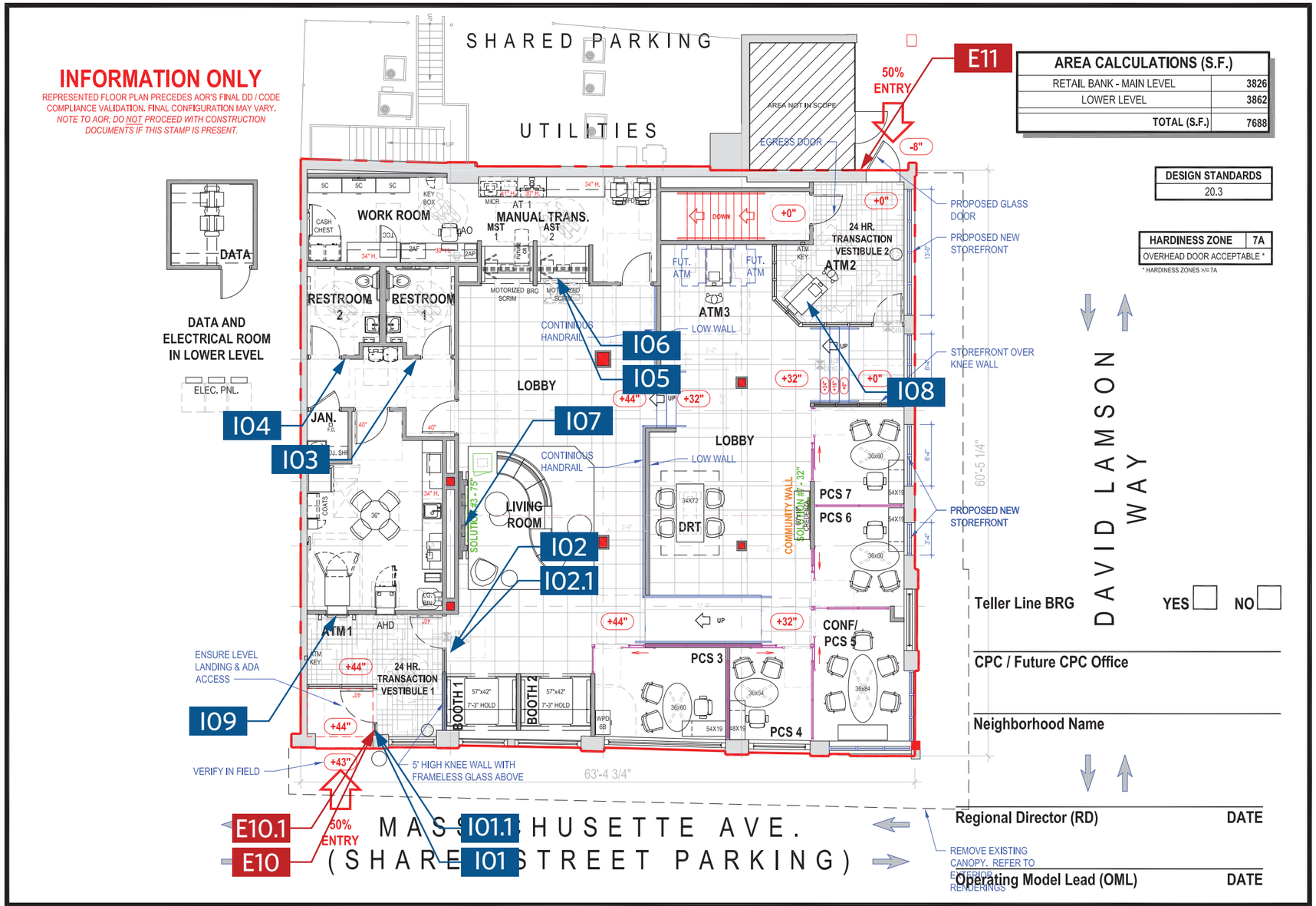
Floor Plan

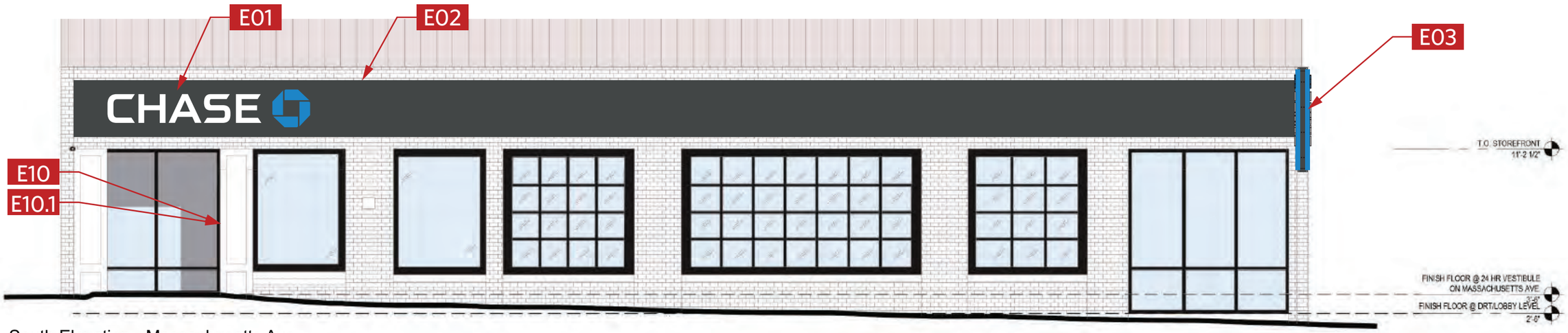
Interior Scope of Work

I01	ADA-EX	ADA Handicapped Exit Plaque	.25SF
I01.1	CUST-VIN	Matching Bronze Vinyl Backer	
I02	ADA-EX	ADA Handicapped Exit Plaque	.25SF
I02.1	CUST-VIN	Matching Bronze Vinyl Backer	
I03	ADA-RRAG-A-G	ADA All Gender Restroom Sign - Accessible	1.4SF
I04	ADA-RRAG-A-G	ADA All Gender Restroom Sign - Accessible	1.4SF
I05	ADA-TW	ADA Teller Wall Sign	.1SF
I06	ADA-TW-ALS	ADA Teller Window - Assistive Listening System	.1SF
I07	TPL-BTR-B-24	24" Thin Profile Illuminated Interior Blue Octagon	4SF
I08	eATM-PANEL-SINGLE-SIDE-CAR	Branding Panel for Single Side Car eATM 3.0	
I09	SUR-TTW-U-4-TP	Illuminated Thin Profile ATM Surround	33SF
E10	ADA-EP	ADA Handicapped Entrance Plaque	.25SF
E10.1	CUST-VIN	Matching Blue Vinyl Backer	
E11	ADA-EP-NA	ADA Handicapped Entrance Plaque - Not Accessible	.38SF



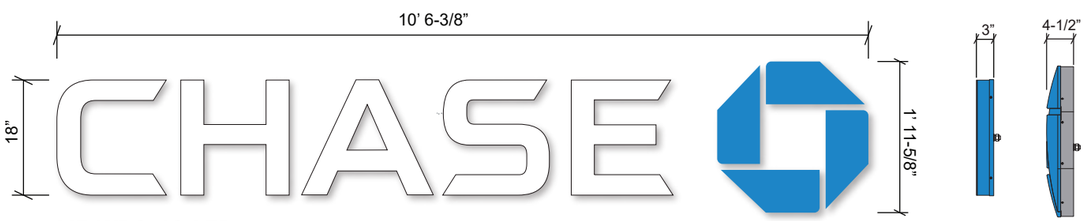
Exterior Signs

Interior Signs



South Elevation - Massachusetts Ave

SIGN E10 NOT SEEN FROM THIS ANGLE



LIF-WBO-18-LED

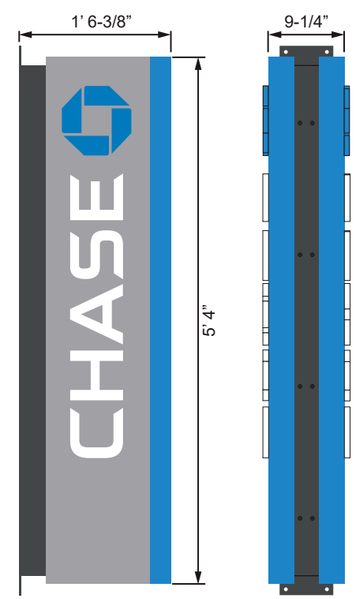
White w/ Blue Octagon Illum Channel Letters - 20.7 SF

Scale: NTS

NMP-XX

Dark Nickel Metal Pan - Dimensions TBD

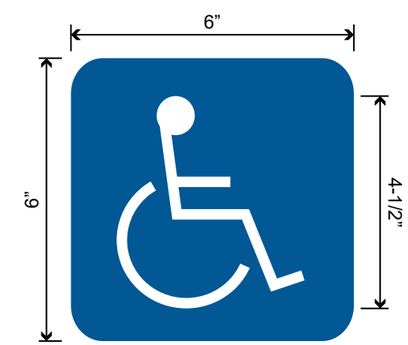
Scale: NTS



F-6

Illuminated Flag Sign

Scale: NTS



ADA-EP

Handicapped Entrance Plaque

Scale: NTS

CUST-VIN

Matching Blue Vinyl Backer

Scale: NTS

SIGN E10 MOUNTS BACK-TO-BACK WITH SIGN I01.

CHASE

CHS.NB.961 - Arlington Mass Avenue

645 Massachusetts Avenue
Arlington, MA 02476

DESIGNER - JM

CREATED - 06.17.21

DRAWING - B95122

PHILADELPHIASIGN
BRINGING THE WORLD'S BRANDS TO LIFE
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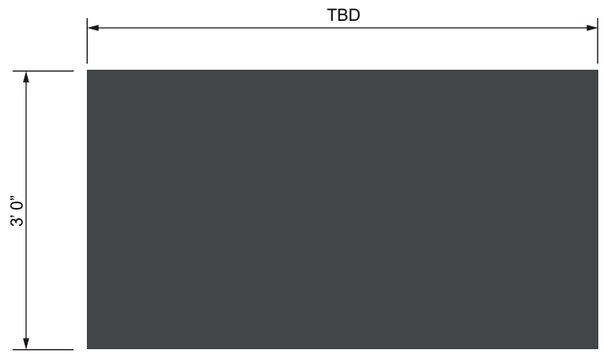
E04 - NMP-XX

East Elevation - David Lamson Way



NMP-XX

DARK NICKEL METAL PAN - DIMENSIONS TBD

SCALE: NTS

CHASE

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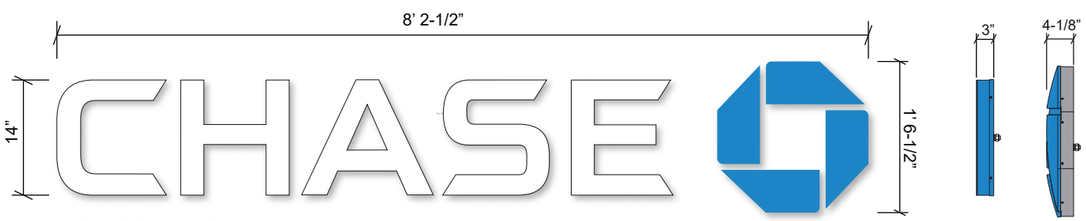
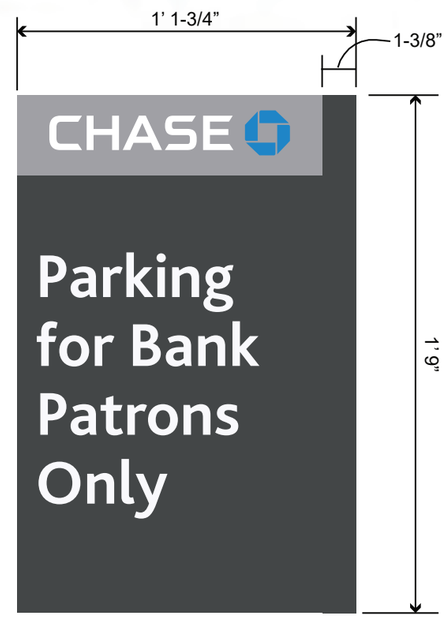
North Elevation - Shared Parking



LIF-WBO-14-LED
White w/ Blue Octagon Illum Channel Letters - 12.7 SF
Scale: NTS

NMP-XX
Dark Nickel Metal Pan - Dimensions TBD
Scale: NTS



TC-W-A-RE
Wall Mounted Regulatory Sign (Re-engineered) - 2sf
Scale: NTS

ADA-EP-NA
Handicapped Entrance Plaque - Not Accessible
Scale: NTS

CHASE

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ARCHITECTURAL RENDERING - CORNER ELEVATION

CHASE

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ARCHITECTURAL ELEVATION - MASSACHUSETTS AVENUE

CHASE

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DESIGNER - JM

CREATED - 06.17.21

DRAWING - B95122

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ARCHITECTURAL ELEVATION - DAVID LAMSON WAY ELEVATION

CHASE

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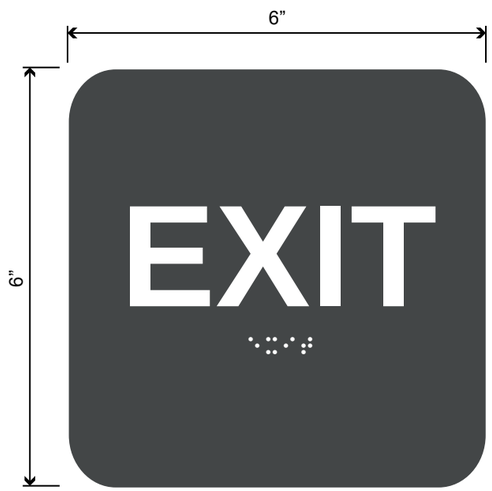
I01, I02 - ADA-EX

I01.1, I02.1 - CUST-VIN



ADA-EX
HANDICAPPED EXIT PLAQUE
SCALE: NTS

CUST-VIN
MATCHING BRONZE VINYL BACKER
SCALE: NTS

SIGN I01 MOUNTS BACK-TO-BACK WITH SIGN E10.

CHASE

CHS.NB.961 - Arlington Mass Avenue

645 Massachusetts Avenue
Arlington, MA 02476

DESIGNER - JM
CREATED - 06.17.21
DRAWING - B95122

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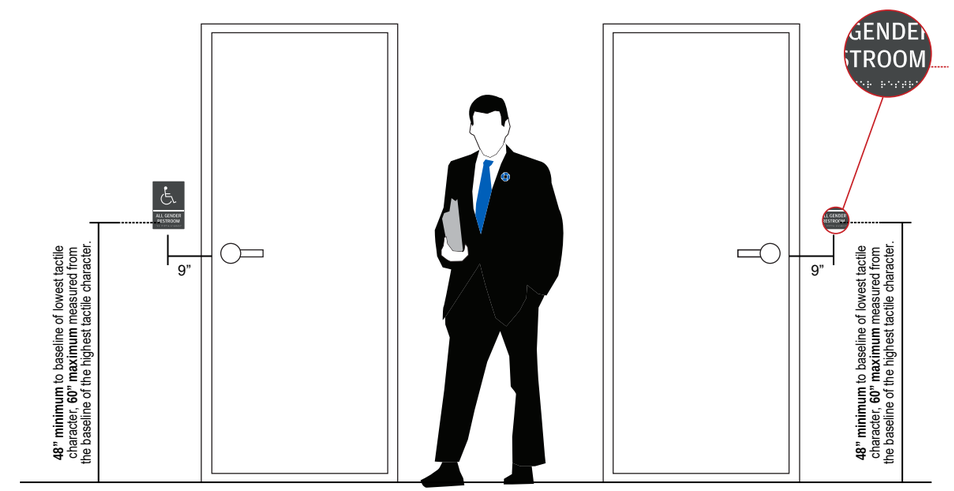
I03, I04- ADA-RRAG-A-G

ADA-RRAG-A-G All Gender Neutral Restroom Signage for Restrooms that **ARE** Accessible

- Tactile sign identifying an accessible restroom entrance.
- Mounted on the wall, next to the door, on the latch side of the door.
- Acrylic tactile signs designed to meet Federal ADA 2010 ADAAG standards.



If latch is on left side of door, sign installed on left side

If latch is on right side of door, sign installed on right side

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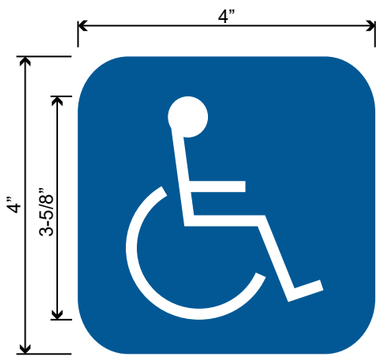
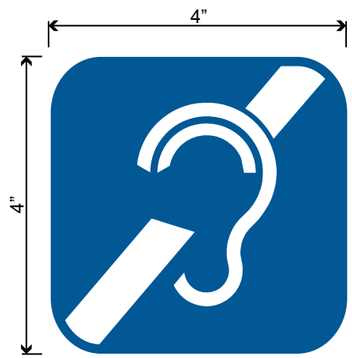
I05 - ADA-TW

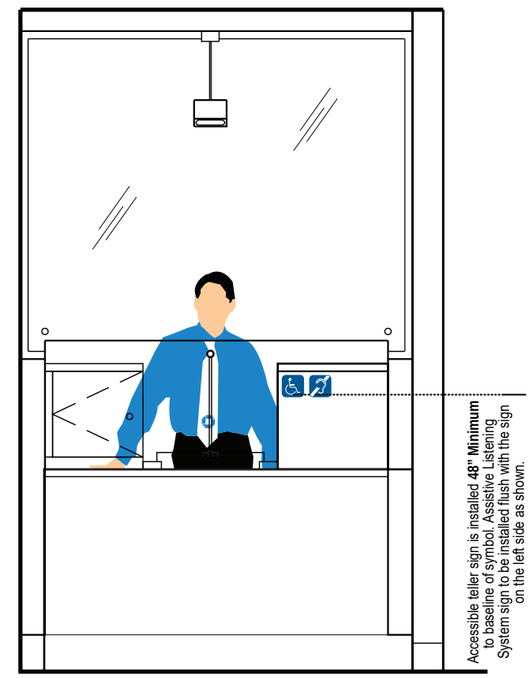
I06 - ADA-TW-ALS



ADA-TW
ADA Teller Wall Sign
Scale: NTS



ADA-TW-ALS
ADA Teller Wall Sign - Assistive Listening System
Scale: NTS



Placement at Modular Teller Stations with Bullet-Resistant Glass

CHASE

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645 Massachusetts Avenue
Arlington, MA 02476

DESIGNER - JM
CREATED - 06.17.21
DRAWING - B95122

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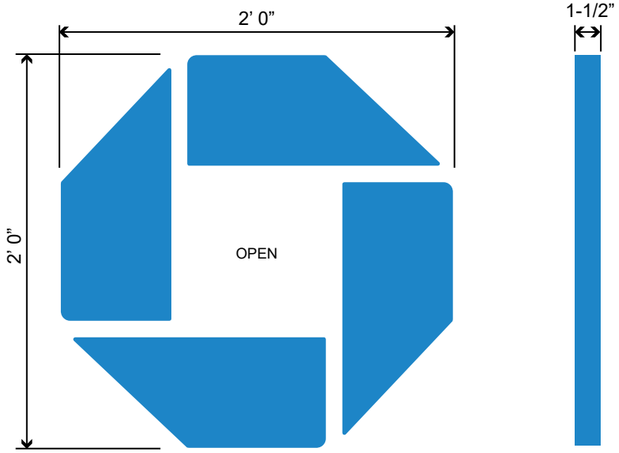
I07 - TPL-BTR-B-24

For Reference Only



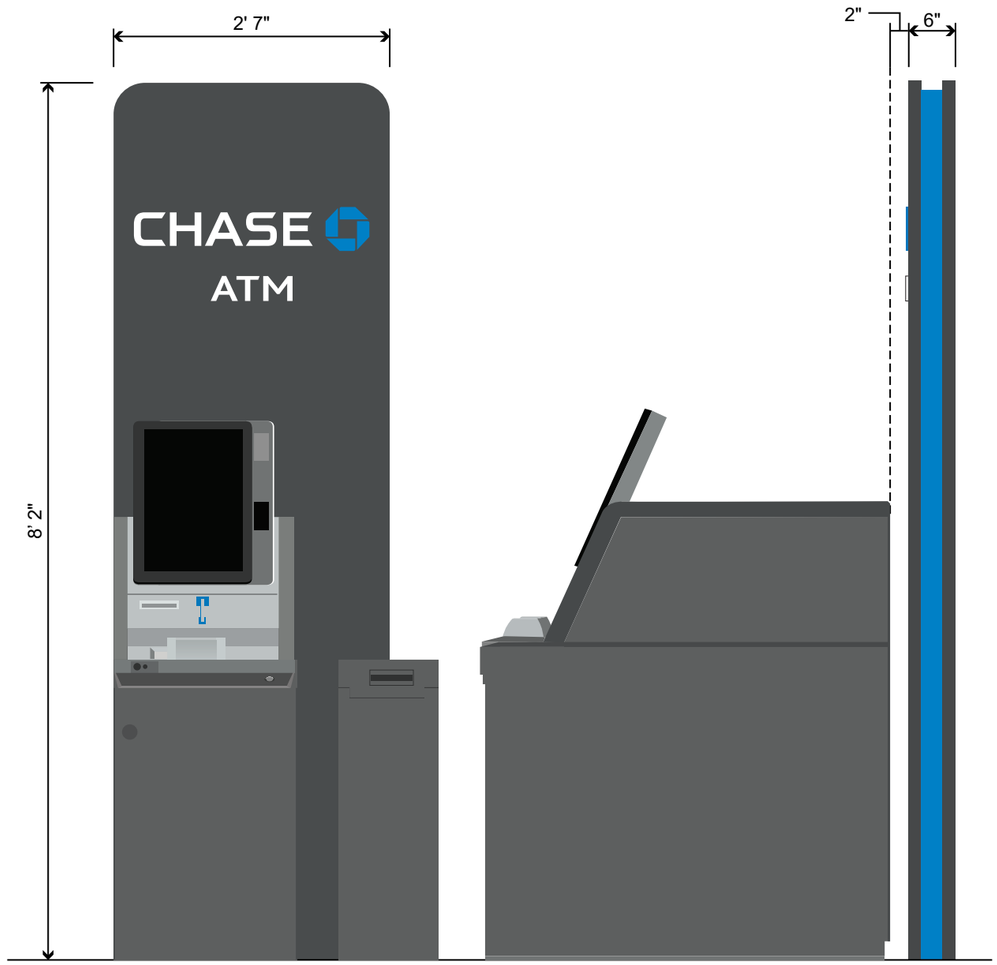
TPL-BTR-B-24

24" Thin Profile Illuminated Interior Blue Octagon - 4sf

Scale: NTS

NOTE: Octagon to be Purchased from Bitro.

I08 - eATM-PANEL-SINGLE-SIDE-CAR

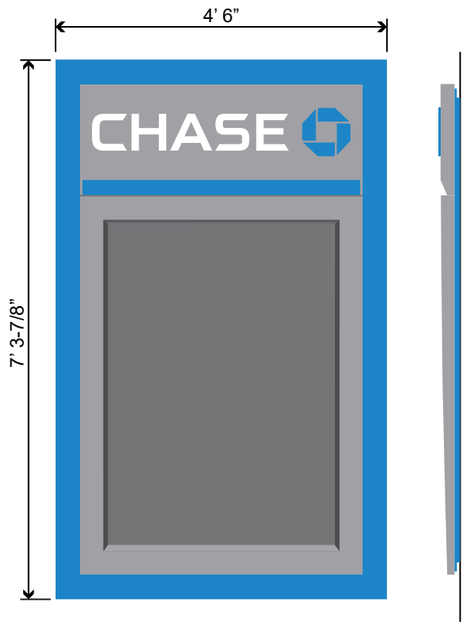


eATM-PANEL-SINGLE-SIDE-CAR

Branding panel for Single Side Care eATM 3.0

Scale: NTS

I09 - SUR-TTW-U-4-TP

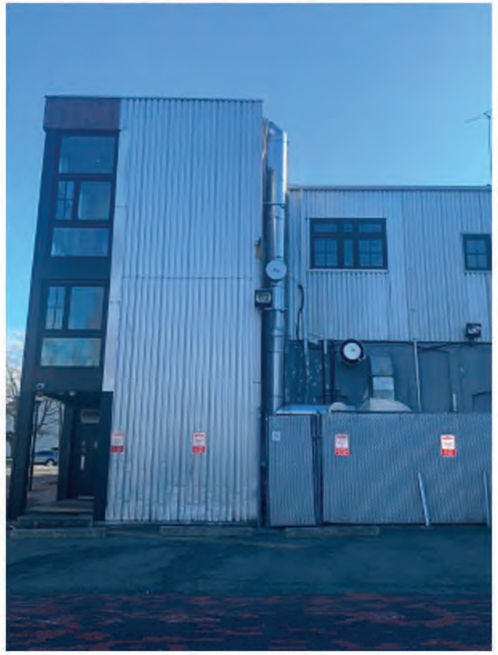


SUR-TTW-U-4-TP

Thin Profile ATM Surround - 33sf

Scale: NTS

Survey Photos



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Approach Photos

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Code Allowances - Completed by Sign Vendor

General Info	
Zoning Designation	B-5, Central Business
Temporary Signs	
Is the 8' x 4' Coming Soon Ground Sign Allowed? If so, how many?	Downtown location not suitable for F/S signs
Are temporary banners allowed? If so, for how long?	n/a
Are fly guys allowed? If so, for how long?	n/a
Primary Ground Sign	
Will code allow our standard pylon or monument? If yes, which one (ex: P-100, P-75, M-50, M-25, refer to reference guide for all standard options). Is more than 1 primary ground sign allowed?	Downtown location not suitable for F/S signs
If not, what are the pylon / monument restrictions? (include illumination restrictions/overall allowable sf / custom design requirements)	n/a
List the set back requirements.	n/a
Building Sign	
Prototypical FS branch - Will code allow our standard illuminated bldg. sign package (30" letterset on front & sides, 24" letterset on the rear)? Please list size and locations of signs allowed.	1 sign per street, no SF limitations; no flashing/blinking
If not, what are the variables/restrictions (include illumination and sf restrictions, as well as custom sign requirements)?	n/a
In-line / Urban branch - what are the exterior wall sign restrictions (sign type, including max. sf and illumination).	n/a
Interior Window Signs- List all interior window signs restrictions, including storefront set back requirements.	25% window area
ATM Topper- does this count against our overall allowable sf.? Is illumination allowed?	ATM signs subject to review and approval
Are decorative logos allowed (EFIS octagon)? Does it count against overall SF?	n/a
Directional / Regulatory Signs	
Is our standard directional and regulatory sign package allowed?	Downtown location not suitable for F/S signs
If not, what are the variables/restrictions?	n/a
Awnings / ATM Sunscreens	
Are branded awnings allowed?	yes
What if any restrictions are there (Illumination, color/materials, min & max projection)?	8' clearance, no SF limitation, see add'l comments
Are ATM sunscreens allowed? Do they count against overall SF?	ATM signs subject to review and approval
Other Governing Agencies	
Identify other governing agencies that could override code (ARB, HRB, PUD, etc) and list the known restrictions.	ALL SIGNS TO BE REVIEWD AND APROVED BY the ARLINGTON HISTORICAL COMMISSION
Permitting / Variance Process	
What is the application process and timing for variance approval ? What are the variance application fees?	2-3 months
What is the likelihood of being granted a variance with this municipality?	25%
Architectural Lighting	
Is Architectural lighting allowed? Does it count against overall SF? List provisions.	no flashing/blinking. NO SIGNS TO BE ILLUMINATED FROM 12am - 6am
Additional Comments	
Please list any additional comments	Permitted in the following categories of signs: wall sign, window sign, awning sign

2007 00168668
Bk: 50073 Pg: 393 Doc: DEED
Page: 1 of 2 09/07/2007 04:01 PM

QUITCLAIM DEED

645 Mass. Ave. LLC, a Massachusetts limited liability company with a principal place of business at 11 Pepper Hill Drive, Winchester, MA 01890 for ONE MILLION NINE HUNDRED FIFTY THOUSAND and 00/100 ($1,950,000.00) DOLLARS consideration paid grants to Key West Realty LLC, a Massachusetts limited liability company with a principal place of business at 63 Tropelo Road, Waltham, MA with quitclaim covenants the land and buildings thereon situated in Arlington, Middlesex County, Massachusetts and now numbered 645 to 651 Massachusetts Avenue and shown on a plan of "Land in Arlington, Mass., Boston & Maine Railroad to Myer Dana, E. D. Chapman, R. E. Engr. June 1922" recorded with Middlesex South District Deeds Book 4534, End, and bounded and described as follows:

SOUTHWESTERLY	by said Massachusetts Avenue, as now laid out and existing, 64 feet;
NORTHWESTERLY	by land now or late of Menotomy Trust Company, 100 feet;
NORTHEASTERLY	by land now or formerly of Boston & Maine Railroad, 64 feet; and
SOUTHEASTERLY	by other land now or formerly of Boston & Maine Railroad, 100 feet.

Containing about 6,400 square feet according to said plan.

Be any or all of said measurements or contents more or less or however otherwise said premises may be measured, bounded or described.

Together with the right to sue for the ordinary purposes of a passageway to and from the granted premises, the existing passageway on the Southeasterly side of the granted premises, said passageway being shown on said plan and extending 100 feet Northeasterly from Massachusetts Avenue.

Subject to and with the benefit of easements, rights, restrictions and agreements of record so far as now in force and applicable.

For title, see deed recorded with Middlesex South District Registry of Deeds at Book 49471, Page 543.

MASSACHUSETTS EXCISE TAX
Southern Middlesex District ROD # 001
Date: 09/07/2007 04:01 PM
Ctrl# 099332 27101 Doc# 00168668
Fee: $6,892.00 Cons: $1,950,000.00

645 MASS. AVE. LLC

By: _____
Donald A. Calareso, Manager

CLARK, HUNT AND EMBRY
55 CAMBRIDGE PARKWAY
CAMBRIDGE, MA 02142

COMMONWEALTH OF MASSACHUSETTS

Middlesex, ss.

On this 7th day of September, 2007, before me, the undersigned notary public, personally appeared 645 Mass. Ave. LLC by Donald A. Calareso, its Manager, proved to me to be the person whose name is signed on the preceding or attached document, through satisfactory evidence of identification, namely, a driver's license issued by the State of Massachusetts bearing the photographic image of the face and signature of Donald A. Calareso, and acknowledged to me that he signed it voluntarily for its stated purpose.

Notary Public Michael G Goth
My Commission Expires: March 14, 2008

S:\WDOX\Bank_RE\RE\06423\00009\00010931.DOC

Eugene C. Brune
Attest Middlesex S. Register

Town of Arlington, Massachusetts

Department of Planning & Community Development

730 Massachusetts Avenue, Arlington, Massachusetts 02476

Public Hearing Memorandum - Update

The purpose of this memorandum is to provide the Arlington Redevelopment Board and public with technical information and a planning analysis to assist with the regulatory decision-making process.

To: Arlington Redevelopment Board

From: Jennifer Raitt, Secretary Ex Officio

Subject: Environmental Design Review, 455-457 Massachusetts Avenue, Arlington, MA Docket #3673

Date: October 21, 2021

This memo is provided as an update to the last memo provided on September 14, 2020. The following items were provided by the Applicant since the last hearing:

- Architectural and site plan sets, including elevations, schematics, renderings, and signage dated October 19, 2021
- A document responding to the Redevelopment Board's requests during the prior hearing.

These items largely address the follow-up items requested as follows:

- *Optimize residential unit layout, including locations of windows on alley units;*
  Units A11 and A12 have been adjusted, and the alley-facing windows have been realigned to maintain privacy for the occupants. Unit A5 has remained in its prior configuration as a shared-light one-bedroom unit.

- *Introduce another chase on the Mass Ave side fronting buildings and provide required number of chases on Medford Street buildings;*
  The applicant has provided additional locations for restaurant chases for Medford Street tenants as noted on sheet A1.02. A long-term tenant with a non-restaurant use has been identified for the former restaurant space on Mass Ave.

- *Provide information and/or samples on material selection;*
  Material selections are noted in the applicant's response to the Redevelopment Board's

requests.

- *Indicate that roof will be solar ready and provide solar field potential on plans;*
  The applicant has confirmed that the roof will be solar ready. Additionally, in their original application materials the applicant submitted a LEED Project Checklist indicating that the project would be eligible to receive one credit for active solar ready design. The Applicant should note this on the roof plan.

- *Provide additional detail on parking including, identification of EV charging station, ADA parking space, and whether parking is designated for residents only;*
  Sheet C-102 of the revised site plan shows that the EV charging parking space is to be located in one of two parking spaces in the surface parking area, and that the ADA parking space will be van accessible. Resident-only parking signs have not been indicated in plan, however a sign is shown on the rendering on Sheet A9.04.

- *Explore potential for introducing trees onto property line;*
  The applicant maintains their intent to plan a row of arborvitae as originally proposed. DPCD staff attended a site walk with the Applicant and note that there is insufficient space along the narrow grass strip, parking lot and adjacent parking lot for tree plantings without removing parking and reducing the travel aisle.

- *Provide a Transportation Demand Management (TDM) plan;*
  At the direction of DPCD, the applicant is surveying existing tenants and their employees to determine the appropriate TDM methods from Section 6.1.5 of the Zoning Bylaw to include in a TDM plan. The plan can be submitted and approved administratively.

- *Provide a rendering of the rear of the structure, alleyway, and garage elevations;*
  These materials may be found in the revised architectural plan set on Sheets A2.01, A2.02, A9.03, and A9.04.

- *Share details on commercial loading and delivery;*
  At present, commercial loading and delivery takes place on Medford Street while the parklet is installed, and along Park Terrace during winter and spring months. The arrangement would remain the same under the proposal. DPCD staff attended a site walk with the Applicant and note the potential for a designated loading zone along Medford Street. This location will require further review and approval by the Arlington Police Department and Select Board.

- *Share details on dumpster enclosure;*
  These details may be found on sheet C-503 of the revised site plan materials.

- *Indicate height of mechanical units relative to parapet;*

The exact heights of mechanical units has not been provided. The applicant notes that the parapets have been designed to conceal HVAC equipment from view from the sidewalks opposite the Medford Street and Massachusetts Avenue facades.

- *Provide a larger context on the site plan;*
  This has been provided on Sheet FIG-1 of the revised site plan materials.

- *Provide a plan or guidelines for future tenant improvements to facade, treatment of the sign band, awnings, and signage; and*
  The applicant has provided a sample preferred sign for future or replacement signage on Sheet A9.05 of the revised architectural plan set for Medford Street tenants. The applicant also indicated a preferred signage treatment for a future Mass Ave tenant. The applicant further noted clean up of abandoned hardware on both façades. Still outstanding is confirmation of how the applicant will repoint and paint the original building elements after removing additions and various façade treatments along the Mass Ave façade.

- *Correct the note on the site plan suggesting that Park Terrace is a private way.*
  This has been addressed on Sheet C-102 of the revised site plan set.

- Revisit layout of A5, A12, A11 / optimize residential unit layout
    - A5 will remain a shared-light 1-bed unit due to location. A12 has been reconfigured to push the living area to the corner. A11 has been altered slightly as a consequence.

- Revisit location of windows on alley units (offset windows)
    - All windows in new structure (over parking) have been reconsidered to avoid undesirable alignment

- Look at adding in another chase for restaurant duct / add a blackiron chase on the Mass Ave side; provide required number of chases on Medford St side
    - We have a long term tenant in the mass ave space, which is the old Papa Ginos space, and with the size of the space won't be adding the chase on that side. The space currently occupied by Grace Nails would not need a chase for conversion as it has an unobstructed rooftop. Locations have been identified for restaurant chases for Medford St. tenants as noted on plans.

- Provide information and/or samples on material selection
    - The current plan for the façade is to use painted fiber cement board and batten for the second floor addition as shown in the provided renderings. The existing brick arcade at the first floor will not be covered or painted. A cornice will be constructed as shown in a style similar to the surrounding context. Windows will be black vinyl 6-lite (6/6) double hung as shown (aligned with the arcade below). Siding color is intended to be James Hardie Cypress, with white trim.

- Indicate that roof will be solar ready and provide solar field potential on plans
    - The new roof will be solar ready to handle any potential solar addition.

- Identify EV charging on parking plan (applicant stated it will be in one of the two surface spaces)
    - See Civil Plans - Sheet C-102 updated.

- Provide specific details on HP space (van or not) on parking plan
    - Garage opening and ceiling will exceed the 98" minimum van accessible height

- Explore potential for introducing trees onto property line, whether in addition to or en lieu of arborvitae
    - Keeping arborvitae row

- Provide a TDM plan using three of eight suggested methods in the Zoning Bylaw
    - Applicant is considering a few options for the third choice but requests to work with the planning department to finalize our plan. We would like to speak with our tenants and have them find out from their employees what might be most desirable.

- Provide a rendering of the back of building, showing access, lighting, circulation in alleyway
    - See Arch set - A9.03 and A9.04

- Provide additional elevations of the garage
    - See Arch set - A2.02

- Share details on commercial loading and delivery
    - Applicant commits to working with the Town, tenants, and the planning department to try to come up with a plan that works for everyone in the immediate vicinity, including abbutters.

- Share details on dumpster enclosure
    - Sheet C-503 of Civil Plans

- Indicate height of mechanical units relative to parapet
    - The parapets will be sufficiently tall to obscure all HVAC mechanical equipment from view from the sidewalk opposing the building frontage on all sides.

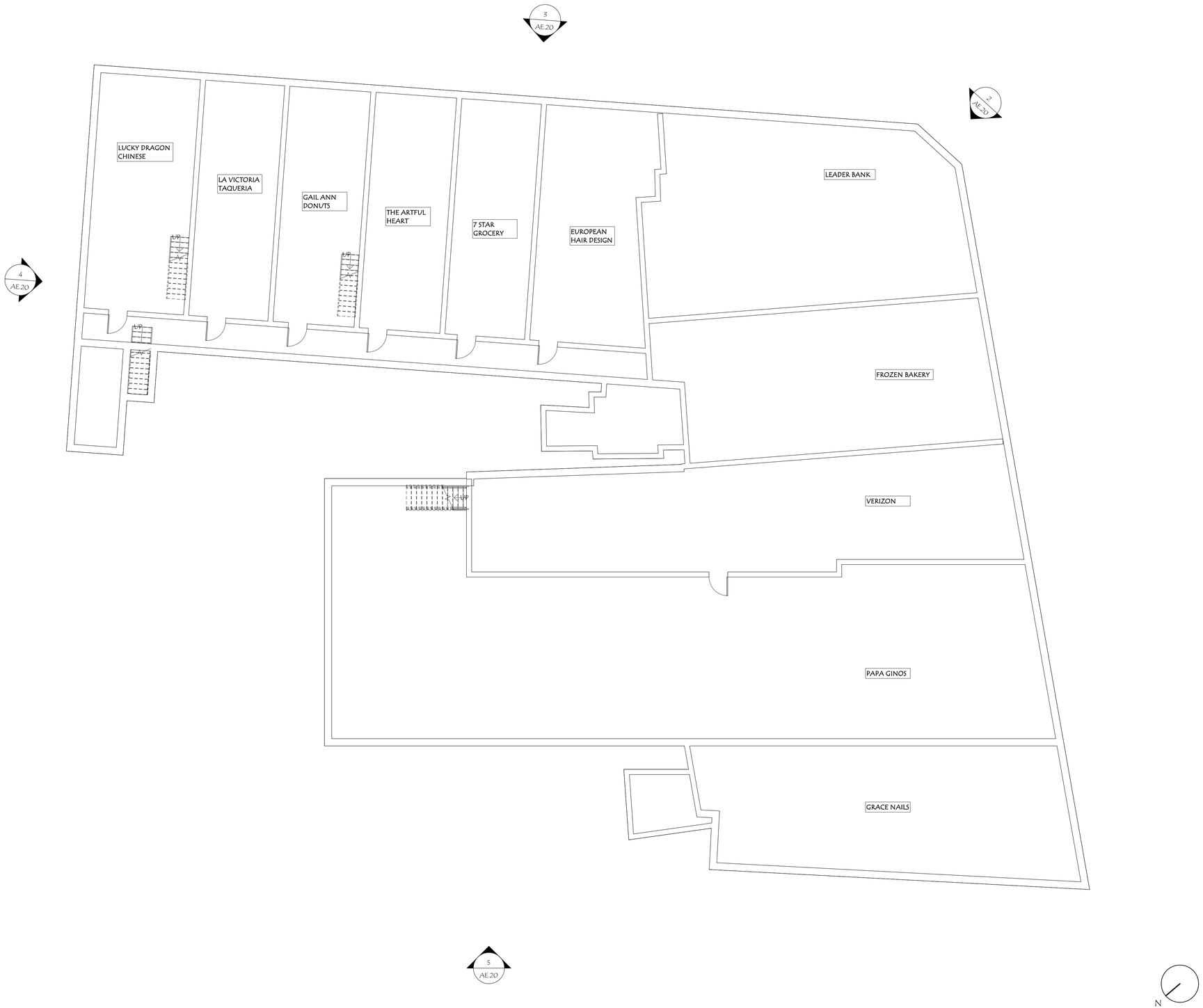
- On the site plan, provide a larger context showing the buildings across Medford Street and Park Terrace, the structure next door, boundaries of church
    - Fig-1 Site Plan on Aerial Image

- Confirm parking is designated for residents; provide intended signage
    - Note signage in rendering on garage

- Provide a plan or guidelines for future tenant improvements to facade, treatment of the sign band, awnings, and signage. This should include identification of tenants with nonconforming signage, landlord standards for replacement signs, addressing abandoned lighting and hardware, etc. Specific problems cited for Leader Bank, abandoned lighting above awnings on Medford St, old or out of date signage.
    - Before any lease renewals happen, or new tenants move in, signage review will always be a part of the process. We show a sample of a sign that we prefer above the new

space on Mass Ave, which holds similar context to Grace Nails. Awnings would be optional, and if a tenant proposes an awning they would be following the Town bylaws and/or working with the planning department directly or coming in front of the ARB for approval.
    - We can commit to removing any abandoned lighting, hardware, hooks, hangers etc..
    - We cannot commit to forcing any tenants to make sign changes that were already approved by the Town, in the middle of a lease.

- Correct the note on the site plan suggesting that Park Terrace is a private way
    - To be updated on final permit set submission



1 BASEMENT FLOOR PLAN
1/8" = 1'-0"

MARKET SQUARE ARCHITECTS
1104 Congress St., STE 205
Portsmouth, NH 03801
PH: 603-431-0202

NOT FOR CONSTRUCTION

455 MASSACHUSETTS AVENUE

455 MASSACHUSETTS AVENUE
ARLINGTON, MA

Revisions:

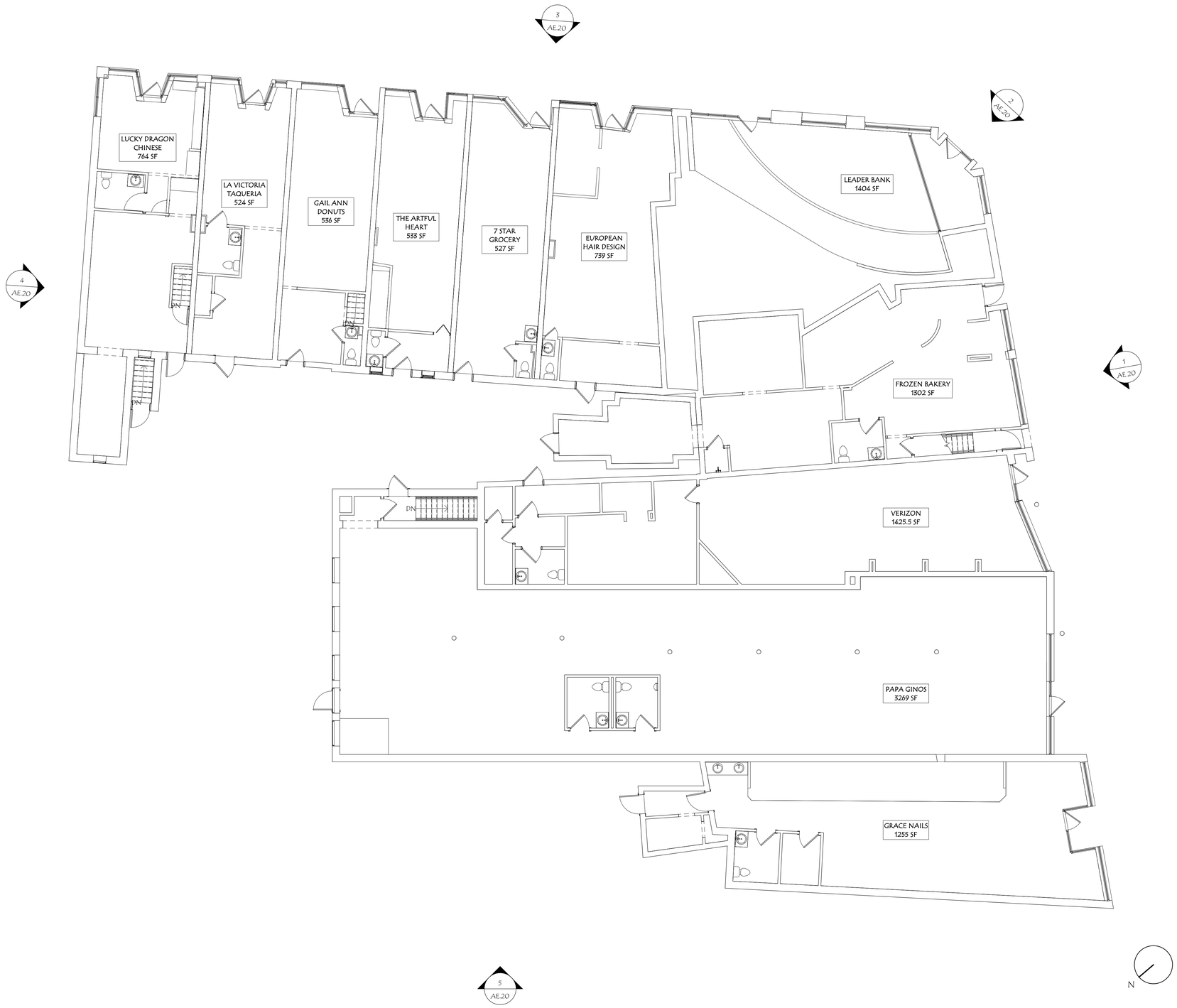
#	Description	Date

Scale:	1/8" = 1'-0"
Drawn By:	VLA, WCB
Checked By:	Checker
Project No.:	202034
Date:	07/27/20

Title: EXISTING BASEMENT PLAN

AE.00

8/19/2021 8:51:07 PM



1 EXISTING FIRST FLOOR PLAN
1/8" = 1'-0"

MARKET SQUARE ARCHITECTS
1104 Congress St., STE 205
Portsmouth, NH 03801
PH: 603-501.0202

NOT FOR CONSTRUCTION

455 MASSACHUSETTS AVENUE

455 MASSACHUSETTS AVENUE
ARLINGTON, MA

Revisions:

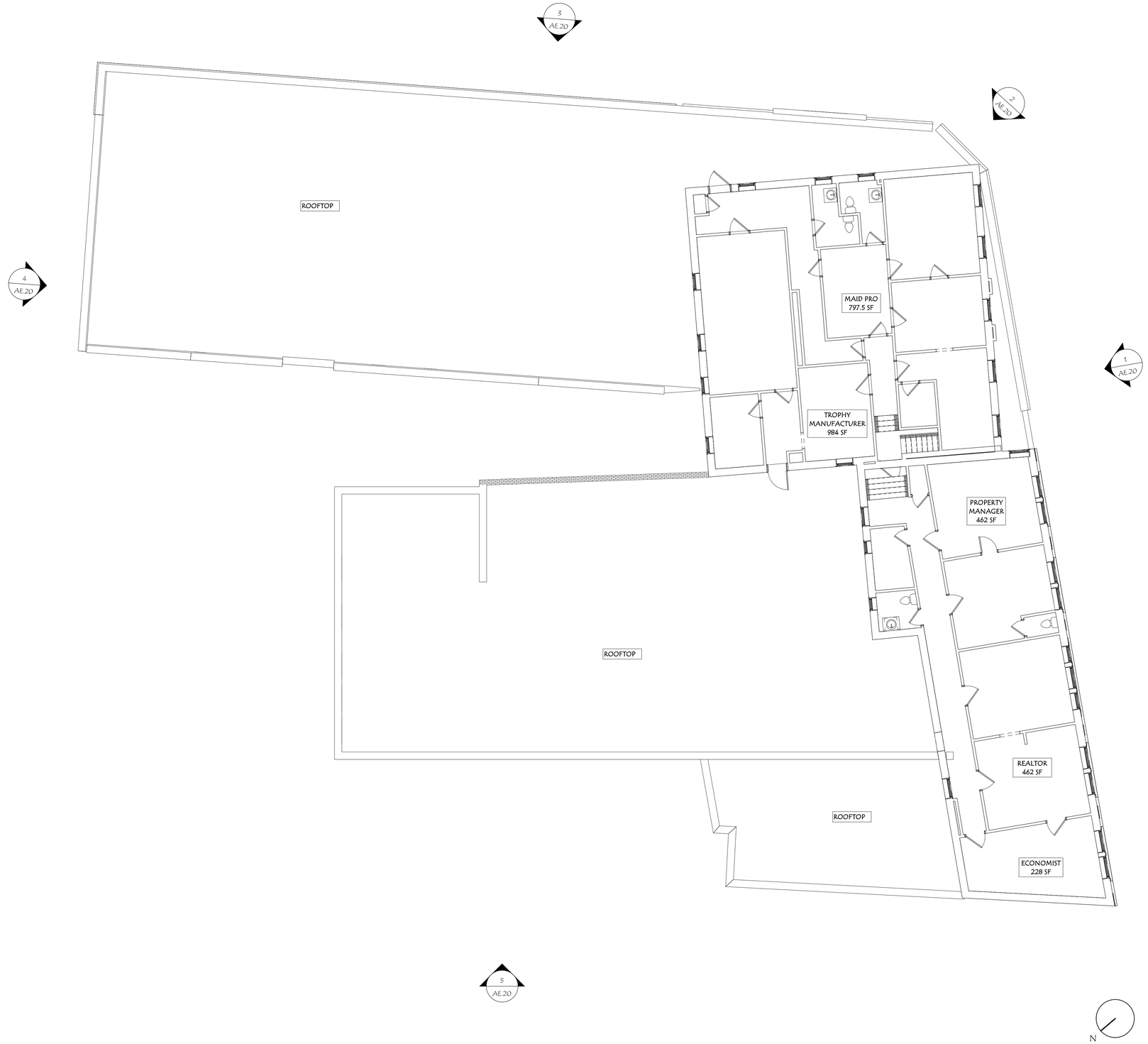
#	Description	Date

Scale:	1/8" = 1'-0"
Drawn By:	VLA, WCB
Checked By:	Checker
Project No.:	202034
Date:	07/27/20

Title: EXISTING FIRST FLOOR PLAN

AE.01

8/19/2021 8:51:07 PM



1 EXISTING SECOND FLOOR PLAN
1/8" = 1'-0"

MARKET SQUARE ARCHITECTS
1104 Congress St., STE 205
Portsmouth, NH 03801
PH: 603-501.0202

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455 MASSACHUSETTS AVENUE

455 MASSACHUSETTS AVENUE
ARLINGTON, MA

Revisions:

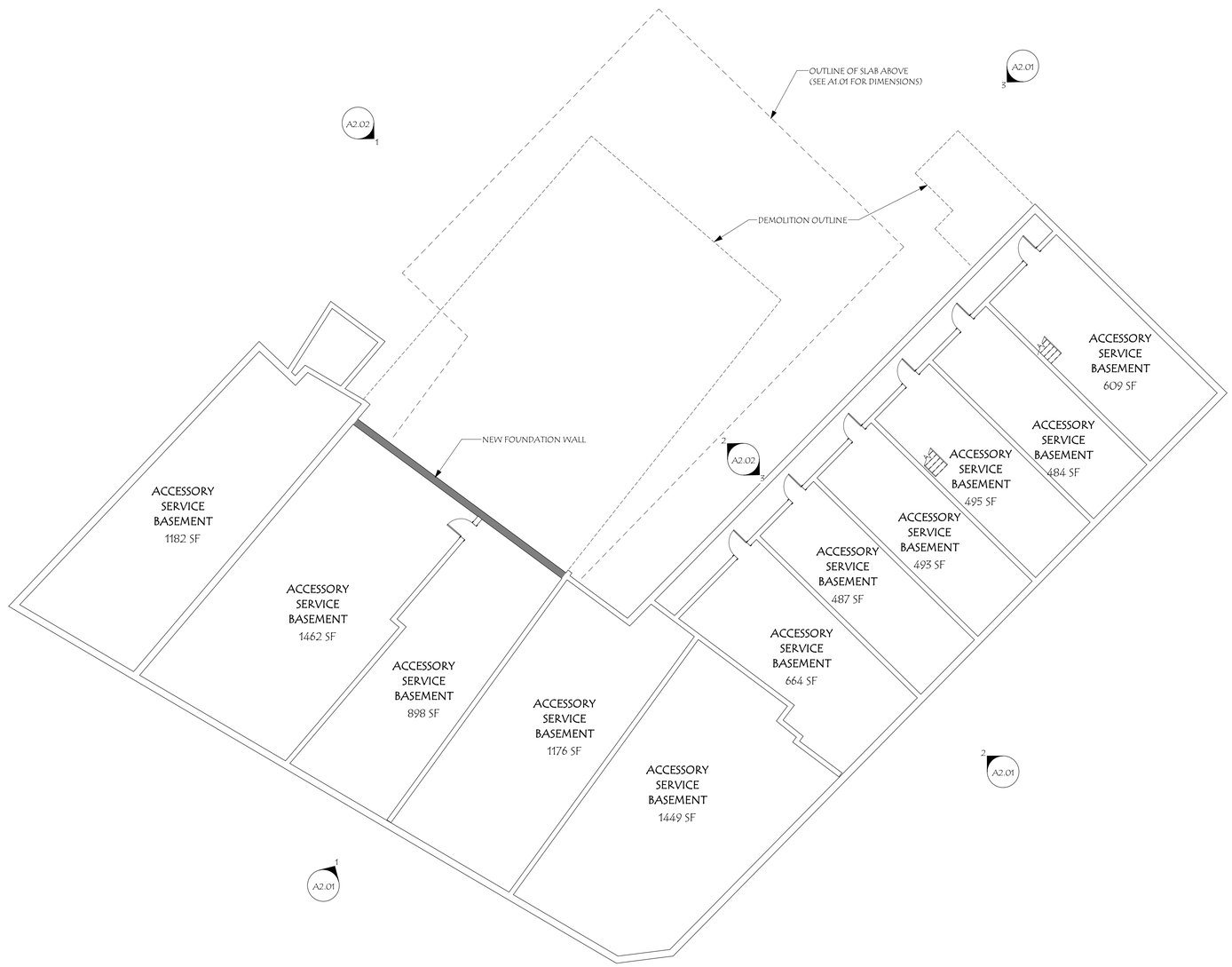
#	Description	Date

Scale:	1/8" = 1'-0"
Drawn By:	VLA, WCB
Checked By:	Checker
Project No.:	202034
Date:	07/27/20

Title: EXISTING SECOND FLOOR PLAN

AE.02

 8/19/2021 8:51:07 PM



1 BASEMENT

SCALE: 3/32" = 1'-0"

MARKET SQUARE ARCHITECTS
104 Congress St., STE 205
Portsmouth, NH 03801
PH: 603.501.0202

ARLINGTON MIXED USE

455 MASS AVE
ARLINGTON, MA

NOT FOR CONSTRUCTION

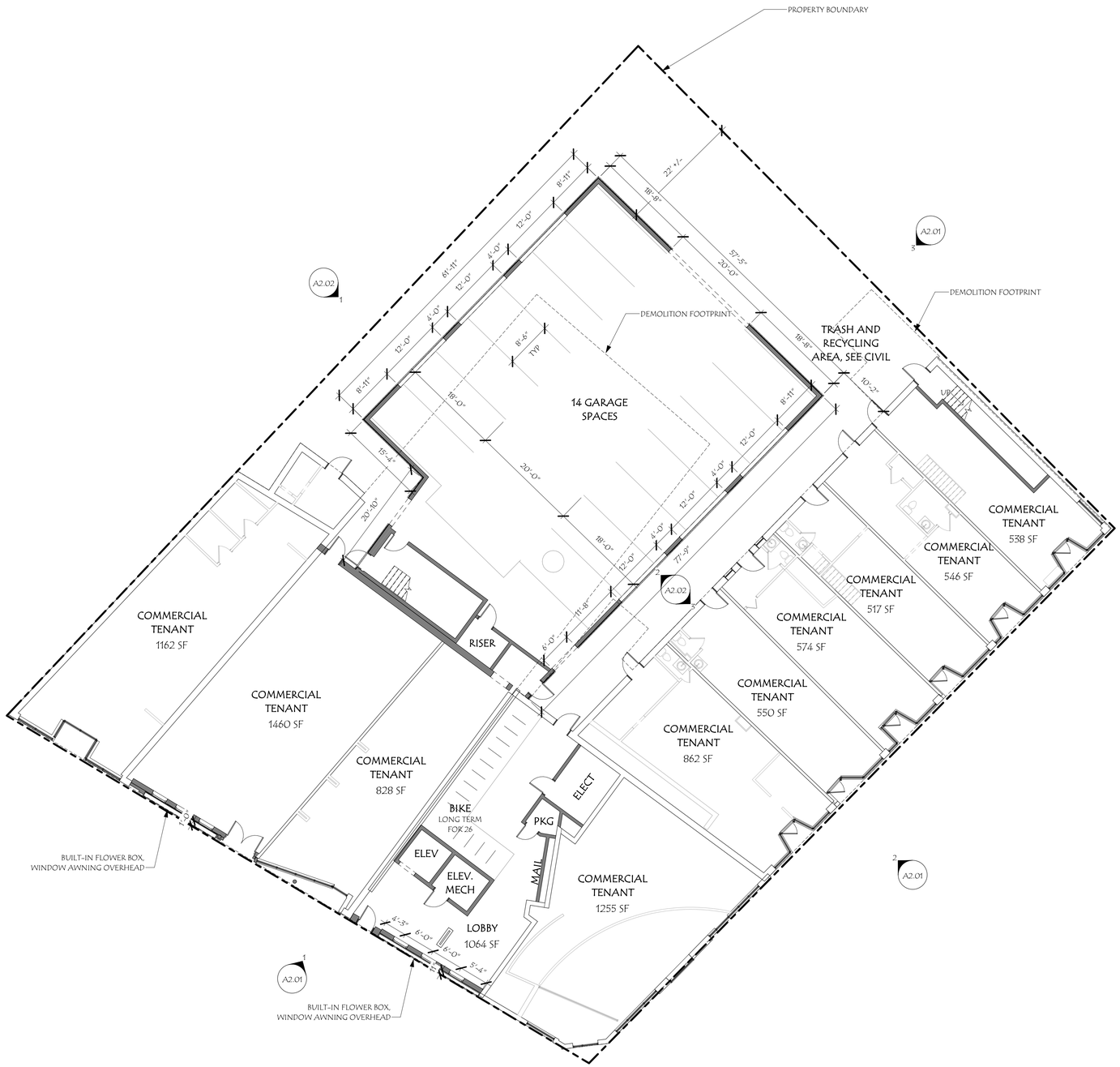
Revisions:
Description
Date

Scale: As indicated
Drawn By: PPS
Checked By: ALW
Project No.: 2020034
SCHEMATIC DESIGN
Date: 08/20/21

Title: OVERALL FOUNDATION FLOOR PLAN

A1.00

10/19/2021 2:57:56 PM



KEY:

EXISTING TO REMAIN

NEW CONSTRUCTION

1 LEVEL 1

SCALE: 3/32" = 1'-0"

MARKET SQUARE ARCHITECTS
104 Congress St., STE 205
Portsmouth, NH 03801
PH. 603.501.0202

ARLINGTON MIXED USE

455 MASS AVE
ARLINGTON, MA

NOT FOR CONSTRUCTION

Revisions:
Description
Date

Scale: As indicated
Drawn By: PPS
Checked By: ALW
Project No.: 2020034
SCHEMATIC DESIGN
Date: 08/20/21

Title: OVERALL FIRST FLOOR PLAN

A1.01

10/19/2021 2:37:56 PM

UNIT MATRIX:

	#	%
TOTAL:		
STUDIO	1 UNIT	8%
1 BED	12 UNITS	92%
TOTAL	13 UNITS	

NOTE:
PRELIMINARY INTERNAL UNIT LAYOUT SHOWN, FINAL LAYOUT MAY VARY.
DIMENSIONS APPROXIMATE WHERE REFERENCING EXISTING CONDITIONS.

KEY:
EXISTING TO REMAIN
NEW CONSTRUCTION

1 LEVEL 2

SCALE: 3/32" = 1'-0"

MARKET SQUARE ARCHITECTS
104 Congress St., STE 205
Portsmouth, NH 03801
PH: 603.501.0202

ARLINGTON MIXED USE

455 MASS AVE
ARLINGTON, MA

NOT FOR CONSTRUCTION

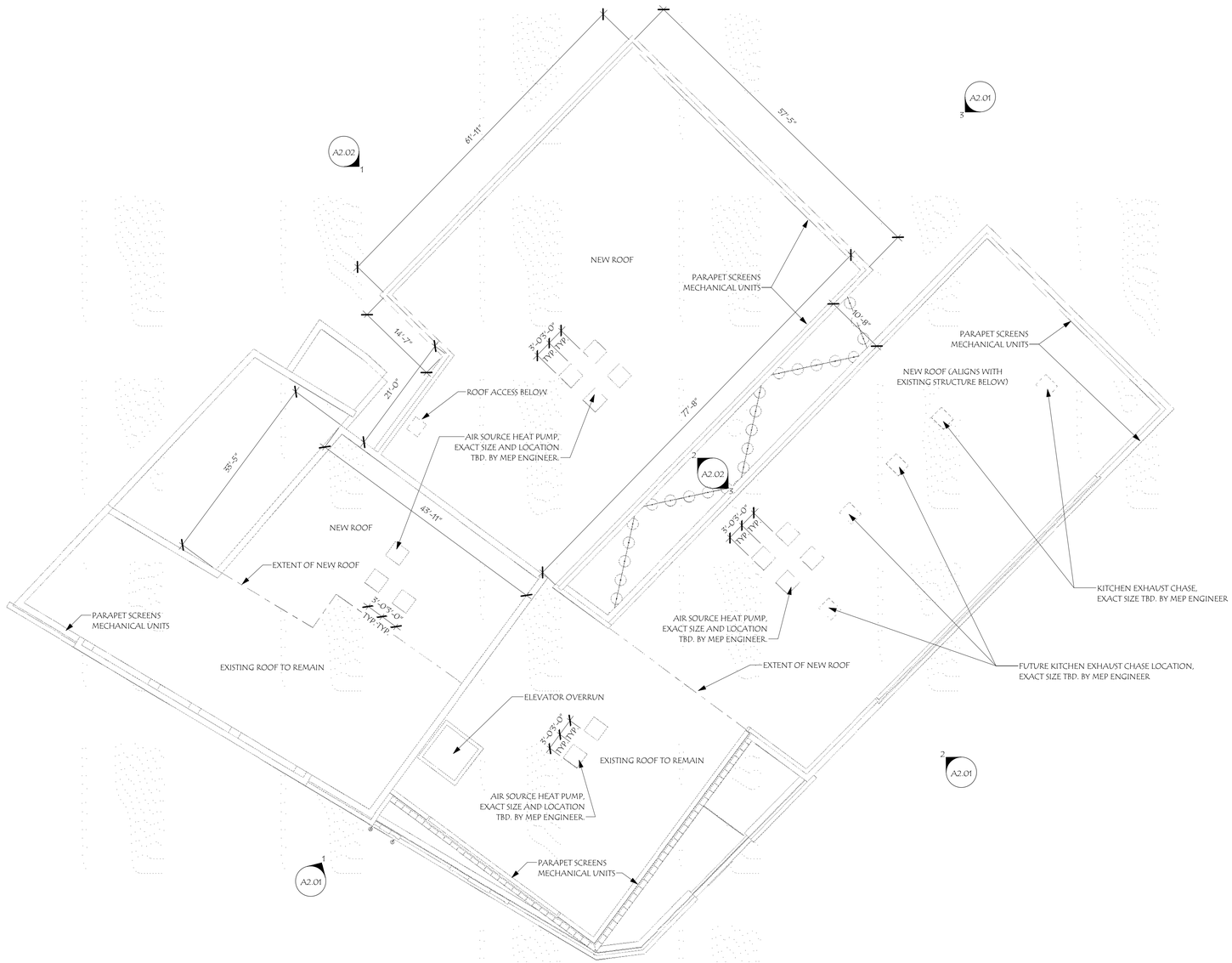
Revisions:
Description
Date

Scale: As Indicated
Drawn By: PPS
Checked By: ALW
Project No.: 2020034
SCHEMATIC DESIGN
Date: 08/20/21

Title: OVERALL SECOND FLOOR PLAN

A1.02

10/19/2021 2:57:59 PM



1 ROOF

SCALE: 3/32" = 1'-0"

MARKET SQUARE ARCHITECTS
104 Congress St., STE 205
Portsmouth, NH 03801
PH: 603.501.0202

ARLINGTON MIXED USE

455 MASS AVE
ARLINGTON, MA

NOT FOR CONSTRUCTION

Revisions:

	Description	Date

Scale:	3/32" = 1'-0"
Drawn By:	PPS
Checked By:	ALW
Project No.:	2020034
SCHEMATIC DESIGN Date:	08/20/21

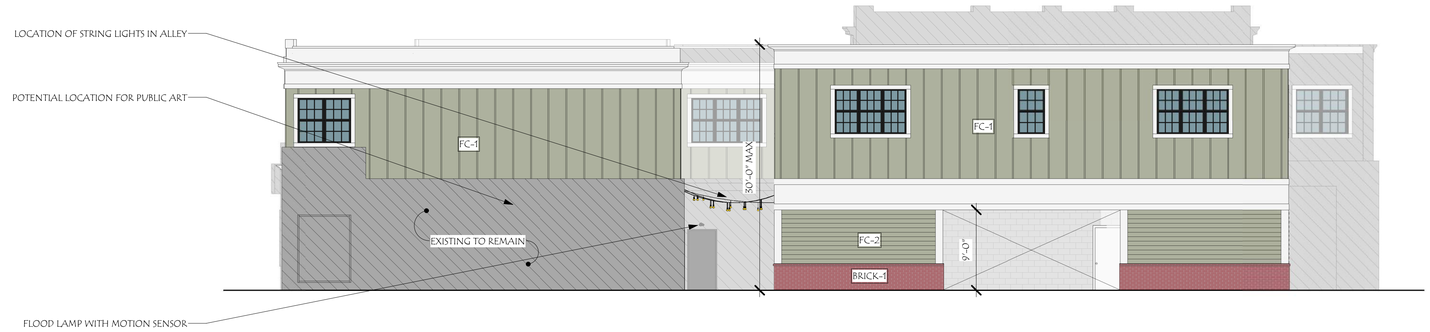
Title: ROOF PLAN

A1.03

10/19/2021 2:38:03 PM

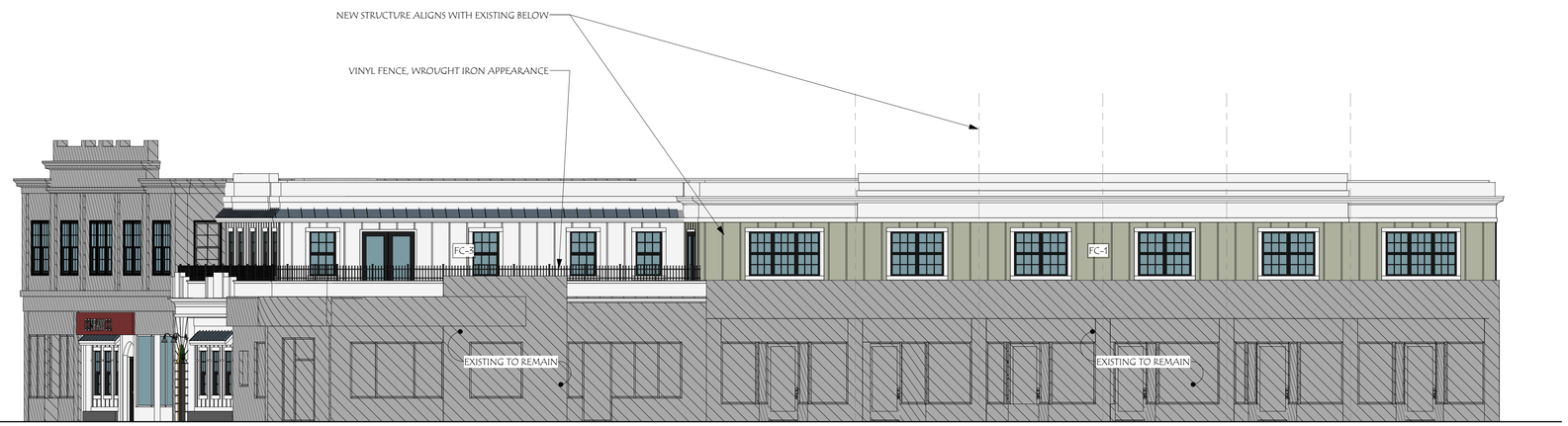
FINISH KEY:

FC-1	BOARD AND BATTEN FIBER CEMENT (LT GREEN)
FC-2	CLAPBOARD FIBER CEMENT (LT GREEN)
FC-3	BOARD AND BATTEN FIBER CEMENT (WHITE)
BRICK-1	BRICK FINISH



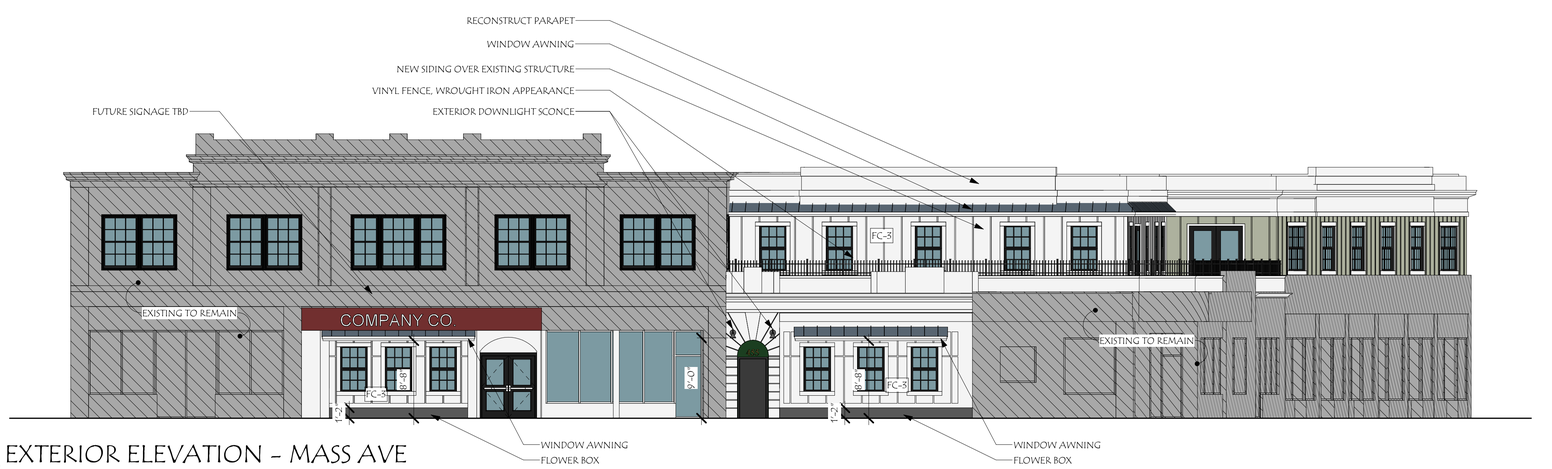
3 EXTERIOR ELEVATION - REAR

SCALE: 1/8" = 1'-0"



2 EXTERIOR ELEVATION - MEDFORD ST

SCALE: 1/8" = 1'-0"



1 EXTERIOR ELEVATION - MASS AVE

SCALE: 1/8" = 1'-0"

MARKET SQUARE ARCHITECTS
104 Congress St., STE 205
Portsmouth, NH 03801
PH: 603.501.0202

ARLINGTON MIXED USE

455 MASS AVE
ARLINGTON, MA

NOT FOR CONSTRUCTION

Revisions:

	Description	Date

Scale: 1/8" = 1'-0"
Drawn By: PPS
Checked By: ALW
Project No.: 2020034
SCHEMATIC DESIGN
Date: 08/20/21

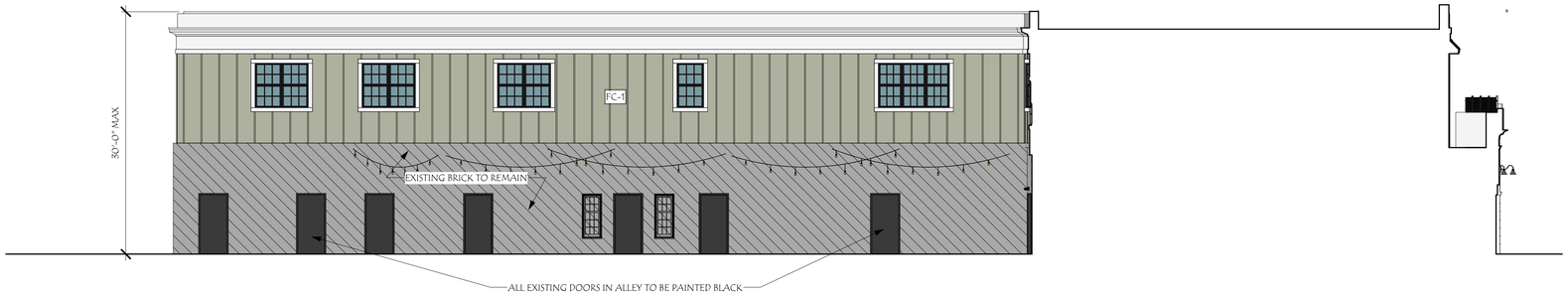
Title: EXTERIOR ELEVATIONS

A2.01

 10/19/2021 2:38:08 PM

FINISH KEY:

FC-1	BOARD AND BATTEN FIBER CEMENT (LT GREEN)
FC-2	CLAPBOARD FIBER CEMENT (LT GREEN)
FC-3	BOARD AND BATTEN FIBER CEMENT (WHITE)
BRICK-1	BRICK FINISH

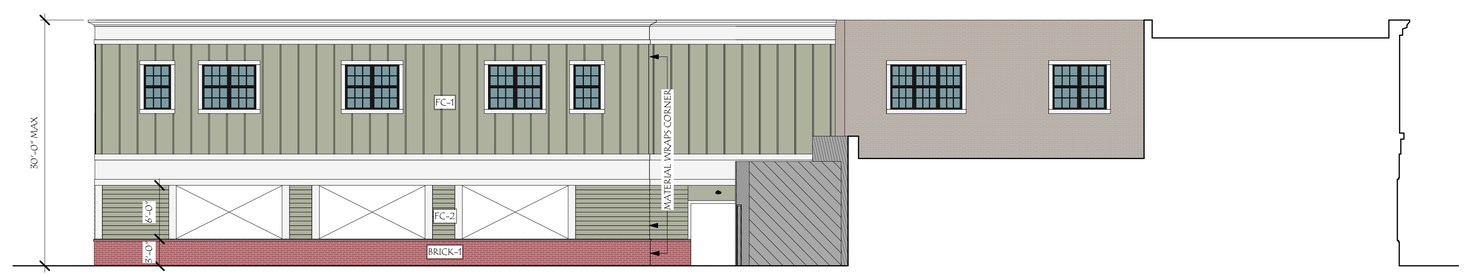


3 EXTERIOR ELEVATION - ALLEY SIDE 2

SCALE: 1/8" = 1'-0"

2 EXTERIOR ELEVATION - ALLEY SIDE 1

SCALE: 1/8" = 1'-0"



1 EXTERIOR ELEVATION - INT. PROPERTY LINE

SCALE: 1/8" = 1'-0"

MARKET SQUARE ARCHITECTS
104 Congress St., STE 205
Portsmouth, NH 03801
PH: 603.501.0202

ARLINGTON MIXED USE

455 MASS AVE
ARLINGTON, MA

NOT FOR CONSTRUCTION

Revisions:

	Description	Date

Scale:	1/8" = 1'-0"
Drawn By:	PPS
Checked By:	ALW
Project No.:	2020034
SCHEMATIC DESIGN	
Date:	08/20/21

Title: EXTERIOR ELEVATIONS

A2.02

10/19/2021 2:38:14 PM

MARKET SQUARE ARCHITECTS
104 Congress St., STE 205
Portsmouth, NH 03801
PH. 603.501.0202

ARLINGTON MIXED USE

455 MASS AVE
ARLINGTON, MA

NOT FOR CONSTRUCTION

Revisions:
1 Description Date

Scale:
Drawn By: PPS
Checked By: ALW
Project No.: 2020034
SCHEMATIC DESIGN
Date: 08/20/21

Title: CORNER RENDER

A9.01

10/19/2021 2:38:14 PM

MARKET SQUARE ARCHITECTS
104 Congress St., STE 205
Portsmouth, NH 03801
PH. 603.501.0202

ARLINGTON MIXED USE

455 MASS AVE
ARLINGTON, MA

NOT FOR CONSTRUCTION

Revisions:
1 Description Date:

Scale:
Drawn By: PPS
Checked By: ALW
Project No.: 20200354
SCHEMATIC DESIGN
Date: 08/20/21

Title:
MEDFORD ST RENDER

A9.02

10/19/2021 2:38:15 PM

Title:
ALLEY RENDER

A9.03

Scale:
Drawn By: PPS
Checked By: ALW
Project No.: 2020034
SCHEMATIC DESIGN
Date: 08/20/21

Revisions:
Description Date

NOT FOR CONSTRUCTION

ARLINGTON MIXED USE

455 MASS AVE
ARLINGTON, MA

MARKET SQUARE
ARCHITECTS
104 Congress St., STE 203
Portsmouth, NH 03801
PH: 603.501.0202

 10/19/2021 2:58:15 PM

Title:
REAR RENDER

A9.04

Scale:
Drawn By: PPS
Checked By: ALW
Project No.: 2020034
SCHEMATIC DESIGN
Date: 08/20/21

Revisions:
Description Date

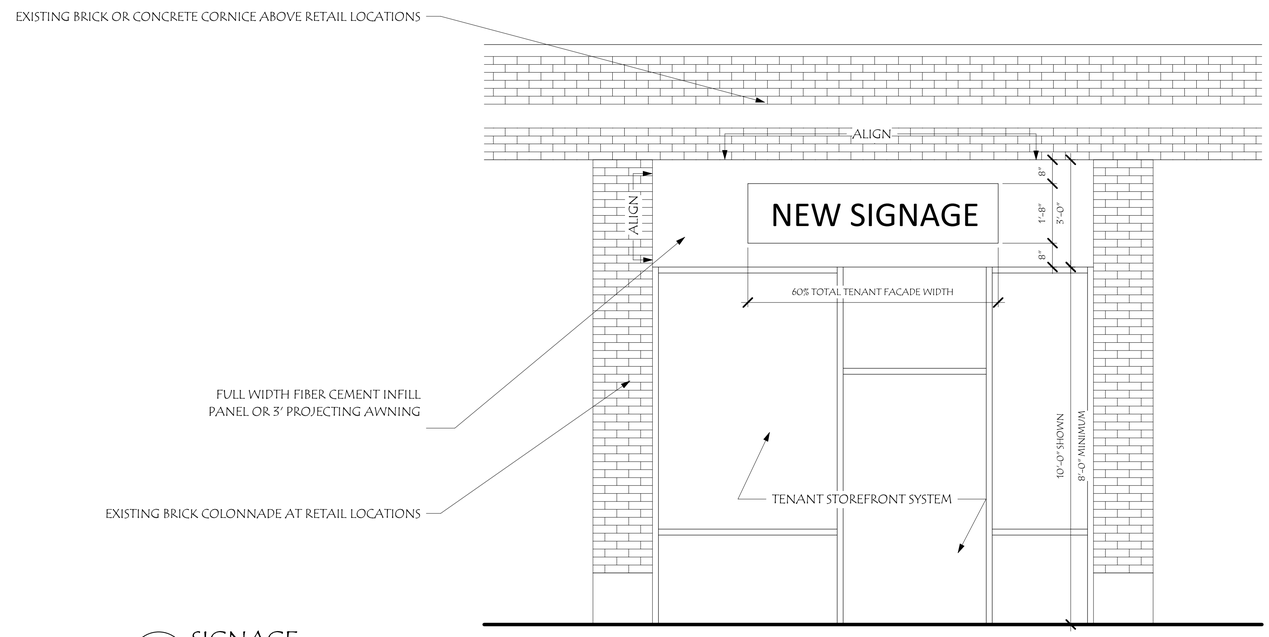
NOT FOR CONSTRUCTION

ARLINGTON MIXED USE

455 MASS AVE
ARLINGTON, MA

MARKET SQUARE
ARCHITECTS
104 Congress St., STE 203
Portsmouth, NH 03801
PH: 603.501.0202

 10/19/2021 2:58:17 PM



1 SIGNAGE

SCALE: 1/2" = 1'-0"

MARKET SQUARE ARCHITECTS
104 Congress St., STE 205
Portsmouth, NH 03801
PH: 603.501.0202

ARLINGTON MIXED USE

455 MASS AVE
ARLINGTON, MA

NOT FOR CONSTRUCTION

Revisions:
Description
Date

Scale: 1/2" = 1'-0"
Drawn By: PPS
Checked By: ALW
Project No.: 2020034
SCHEMATIC DESIGN
Date: 08/20/21

Title: SIGNAGE

A9.05

10/19/2021 2:38:17 PM

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NOTES

1. WRITTEN DIMENSIONS ON THIS PLAN TAKE PRECEDENCE OVER SCALED DIMENSIONS. THE CONTRACTOR SHALL USE CAUTION WHEN SCALING REPRODUCED PLANS. IN THE EVENT OF A CONFLICT BETWEEN THIS PLAN SET AND ANY OTHER DRAWINGS AND/OR SPECIFICATIONS OR CONDITIONS, THE ENGINEER SHALL BE NOTIFIED BY THE CONTRACTOR. ALL SITE ITEMS SHALL BE LAID OUT AND AS BUILT BY A LICENSED LAND SURVEYOR.

2. THE INFORMATION SHOWN ON THIS PLAN IS THE SOLE PROPERTY OF ALLEN & MAJOR ASSOCIATES, INC. ITS INTENDED USE IS TO PROVIDE INFORMATION. ANY ALTERATION, MISUSE, OR RECALCULATION OF INFORMATION OR DATA WITHOUT THE EXPRESSED, WRITTEN CONSENT OF ALLEN & MAJOR ASSOCIATES, INC. IS STRICTLY PROHIBITED.

GRAPHIC SCALE

40 0 20 40 80 160

(IN FEET)
1 inch = 40 ft.

PROGRESS PRINT
ISSUED FOR REVIEW
OCTOBER 18, 2021

PROFESSIONAL ENGINEER FOR
ALLEN & MAJOR ASSOCIATES, INC.

REV	DATE	DESCRIPTION

APPLICANT\OWNER:

2-14 MEDFORD STREET, LLC
455 MASSACHUSETTS AVE, STE 1
ARLINGTON, MA 02474

PROJECT:

455-457
MASSACHUSETTS AVE
ARLINGTON, MA 02476

PROJECT NO.	2729-03	DATE:	8/20/2021
SCALE:	1" = 40'	DWG. NAME:	C2729-03
DESIGNED BY:	JG/ARM	CHECKED BY:	ARM/BDJ

PREPARED BY:

ALLEN & MAJOR ASSOCIATES, INC.
civil engineering ◆ land surveying
environmental consulting ◆ landscape architecture
www.allenmajor.com
100 COMMERCE WAY, SUITE 5
WOBURN MA 01801
TEL: (781) 935-6889
FAX: (781) 935-2896

WOBURN, MA ◆ LAKEVILLE, MA ◆ MANCHESTER, NH

DRAWING TITLE: CONTEXT PLAN

SHEET No. FIG-1

R:\PROJECTS\2729-03\CIVIL\DRAWINGS\CURRENT\C-2729-03_LAYOUT & MATERIALS.DWG

SHARED PARKING SUMMARY TABLE

"MIXED-USE" SHALL BE THE SUM OF USES COMPUTED SEPARATELY	REQUIRED PARKING
*FOR MIXED-USE DEVELOPMENT, THE FIRST 3,000 SQUARE FEET OF NON-RESIDENTIAL SPACE IS EXEMPT FROM THE PARKING REQUIREMENTS OF SECTION 6.1 IN THE ARLINGTON ZONING BYLAW	–
SERVICE: 5,377 S.F. 1 PER 300 S.F. → 1 x (5,377-3,000)/300 = 7.9	7.9
BUSINESS: 1,255 S.F. 1 PER 500 S.F. → 1 x 1,255/500 = 2.5	2.5
OTHER RETAIL: 1,951 S.F. 1 PER 300 S.F. → 1 x 1,951/300 = 6.5	6.5
ONE-BEDROOM APARTMENT UNIT (APARTMENT: 13 UNITS) 1.15 PER UNIT → 1.15 x 13 = 15.0	15.0
TOTAL REQUIRED PARKING BASED ON INDIVIDUAL USES:	31.9
TOTAL PARKING PROVIDED:	16*

ADA SPACES REQUIRED:
(1-25) TOTAL PARKING SPACES PROVIDED, 1 SHALL BE THE MINIMUM ADA PARKING PROVIDED, 1 SPACES BEING VAN ACCESSIBLE.

PROVIDED 1 SPACES, 1 BEING VAN ACCESSIBLE.

PARKING TABLE NOTES:
1. SECTION 6.1.10, C. FOR A MIXED-USE DEVELOPMENT THE FIRST 3,000 SF OF NON-RESIDENTIAL SPACE IS EXEMPT FROM THE PARKING REQUIREMENTS OF THIS SECTION 6.1.
2. SECTION 6.1.11, STANDARD PARKING STALLS SHALL BE 8.5'X18', AND DRIVE AISLE WIDTH SHALL BE 24' FOR TWO-WAY TRAFFIC.

* RELIEF REQUESTED FROM THE ARB FOR THE REQUIRED NUMBER OF PARKING STALLS AND THE DRIVE AISLE REDUCED FROM 24' TO 20'.

BICYCLE PARKING SUMMARY TABLE

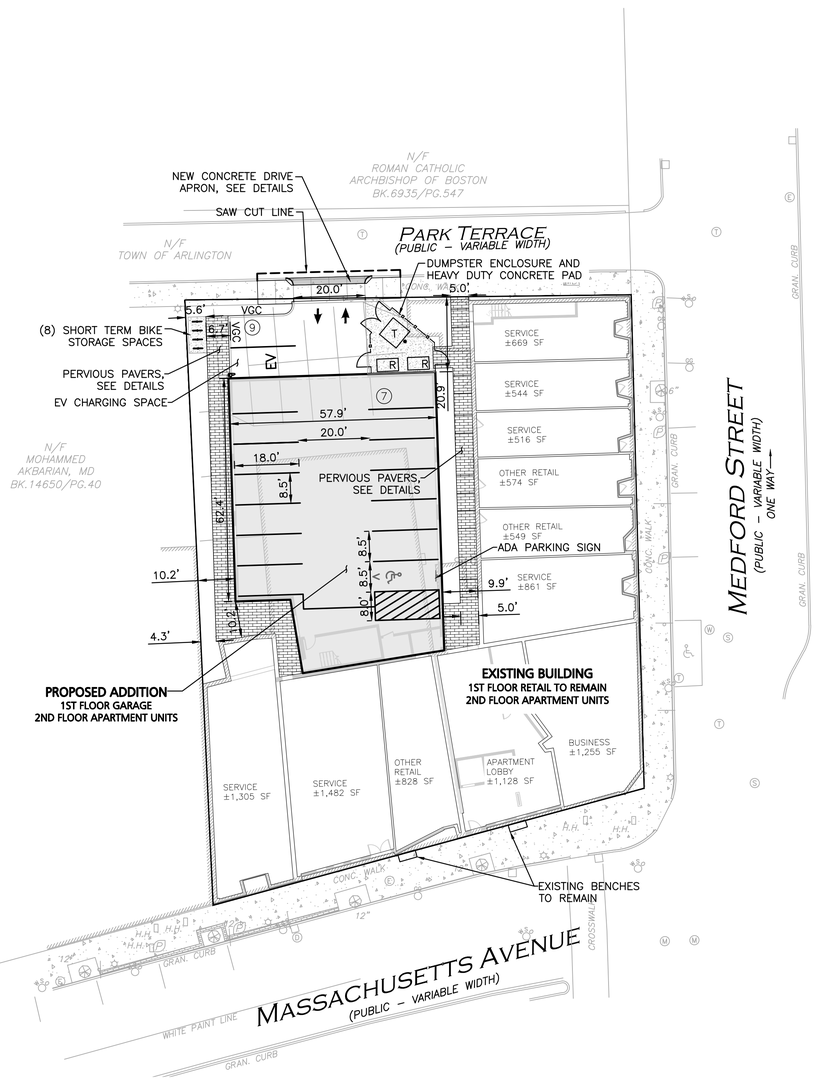
SHORT TERM BICYCLE PARKING (EXTERIOR)

USE	CALCULATION	MIN. REQUIRED	TOTAL PROPOSED
APARTMENT BUILDING	0.1 PER UNIT 13 X 0.1 = 1.3 REQUIRED	1.3	2
OTHER RETAIL OR SERVICE	0.6 PER 1,000 SF (7,328/1000) X 0.6 = 4.4 REQUIRED	4.4	5
BUSINESS	0.5 PER 1,000 SF (1,255/1000) X 0.5 = 0.6 REQUIRED	0.6	1
	TOTAL	6.3	8

LONG TERM BICYCLE PARKING (INTERIOR)

USE	CALCULATION	MIN. REQUIRED	TOTAL PROPOSED
APARTMENT BUILDING	1.5 PER UNIT 13 X 1.5 = 19.5 REQUIRED	19.5	20
OTHER RETAIL OR SERVICE	0.1 PER 1,000 SF (7,328/1000) X 0.1 = 0.7 REQUIRED	0.7	3
BUSINESS	0.3 PER 1,000 SF (1,255/1000) X 0.3 = 0.4 REQUIRED	0.4	3
	TOTAL	20.6	26

BICYCLE PARKING TABLE NOTES:
1. REQUIRED NUMBER OF SPACES ARE FROM BICYCLE PARKING GUIDELINES, APPENDIX A BIKE PARKING BY-LAW.



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LEGEND

PROP. PROPERTY LINE
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BOLLARD
BUILDING
BUILDING ARCHITECTURE
BUILDING INTERIOR WALLS
CURB
PARKING STRIPING
HEAVY DUTY CONCRETE
SIDEWALK
ADA ACCESSIBLE RAMP
PERMEABLE PAVERS
SAW-CUT LINE
PARKING COUNT
COMPACT PARKING STALL

ZONING SUMMARY TABLE
B3-VILLAGE BUSINESS (MIXED-USE <=20,000SF)

ITEM	REQUIRED/ ALLOWED	EXISTING	PROPOSED
MINIMUM LOT AREA	N/A	18,929± SF	18,929± SF
MINIMUM LOT AREA PER UNIT	N/A	N/A	N/A
MINIMUM FRONTAGE	50 FT	124.8± FT MASS. AVE	124.8± FT MASS. AVE
MINIMUM FRONT YARD SETBACK	0 FT	0 FT	0 FT
MINIMUM SIDE YARD SETBACK	0 FT	0 FT	0 FT
MINIMUM REAR YARD SETBACK	(H+L)/6 (1)	0.6 FT	0.6 FT
LANDSCAPED OPEN SPACE	10% (2)	0%	24.1%
USABLE OPEN SPACE	20% (2)	0%	0%
MAXIMUM HEIGHT	60 FT	<35 FT	<35 FT
MAXIMUM HEIGHT STORIES	5	2	2
FLOOR AREA RATIO	1.50	0.98	1.50(3)

ZONING TABLE NOTES:
1. THE REAR SETBACK (H+L)/6, FOR THE NEW ADDITION IS APPROXIMATELY 15.5 FT. (36+57)/6 = 15.5 FT. THE PROPOSED ADDITION HAS BEEN SITUATED OUTSIDE OF THIS SETBACK. THE EXISTING FOUNDATION IS SITUATED 0.6 FT FROM THE REAR PROPERTY LINE. THIS FOUNDATION IS TO BE MAINTAINED.
2. SECTION 5.3.21. SUPPLEMENTAL REQUIREMENTS IN THE BUSINESS AND INDUSTRIAL DISTRICTS, D. FOR MIXED USES AND ANY PERMITTED RESIDENTIAL USE NOT SPECIFICALLY IDENTIFIED IN THE TABLES IN SECTION 5.5.2, THE MINIMUM OPEN SPACE REQUIREMENTS (COMPUTED FROM THE RESIDENTIAL FLOOR AREA ONLY) SHALL BE 10% LANDSCAPED AND 20% USABLE IN THE B1, B2, B2A, B3, AND B4 DISTRICTS, AND 15 PERCENT USABLE IN THE B5 DISTRICT.
3. PROPOSED BUILDING GROSS SQUARE FEET = 28,373 GSF. THE EXISTING LOT AREA IS 18,929 SQUARE FEET. THE FAR = 28,373 GSF / 18,929 SF = 1.50

NOTES

1. WRITTEN DIMENSIONS ON THIS PLAN TAKE PRECEDENCE OVER SCALED DIMENSIONS. THE CONTRACTOR SHALL USE CAUTION WHEN SCALING REPRODUCED PLANS. IN THE EVENT OF A CONFLICT BETWEEN THIS PLAN SET AND ANY OTHER DRAWINGS AND/OR SPECIFICATIONS OR CONDITIONS, THE ENGINEER SHALL BE NOTIFIED BY THE CONTRACTOR. ALL SITE ITEMS SHALL BE LAID OUT AND AS BUILT BY A LICENSED LAND SURVEYOR.
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GRAPHIC SCALE
20 0 10 20 40 80
(IN FEET)
1 inch = 20 ft.

PROGRESS PRINT
ISSUED FOR REVIEW
OCTOBER 18, 2021

PROFESSIONAL ENGINEER FOR
ALLEN & MAJOR ASSOCIATES, INC.

REV	DATE	DESCRIPTION

APPLICANT\OWNER:
2-14 MEDFORD STREET, LLC
455 MASSACHUSETTS AVE, STE 1
ARLINGTON, MA 02474

PROJECT:
455-457
MASSACHUSETTS AVE
ARLINGTON, MA 02476

PROJECT NO.	2729-03	DATE:	8/20/2021
SCALE:	1" = 20'	DWG. NAME:	C2729-03
DESIGNED BY:	JG/ARM	CHECKED BY:	ARM/BDJ

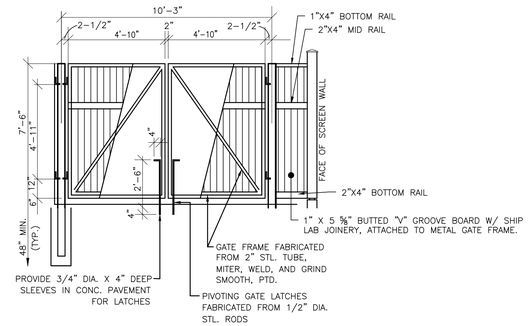
PREPARED BY:

ALLEN & MAJOR ASSOCIATES, INC.
civil engineering ◆ land surveying
environmental consulting ◆ landscape architecture
www.allenmajor.com
100 COMMERCE WAY, SUITE 5
WOBURN MA 01801
TEL: (781) 935-6889
FAX: (781) 935-2896
WOBURN, MA ◆ LAKEVILLE, MA ◆ MANCHESTER, NH

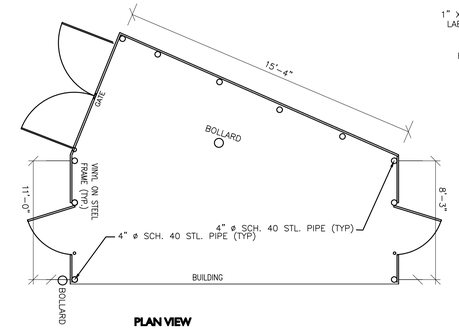
DRAWING TITLE: LAYOUT & MATERIALS PLAN

SHEET No. C-102

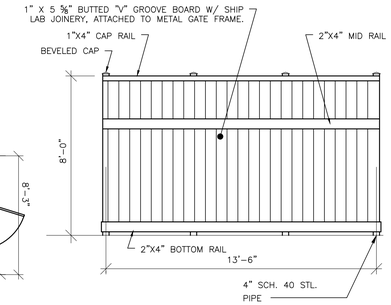
R:\PROJECTS\2729-03\CIVIL\DRAWINGS\CURRENT\C-2729-03_LAYOUT & MATERIALS.DWG



GATE DETAIL



PLAN VIEW



SIDE ELEVATION

NOTE:
1. SUBMIT COLOR TO ARCHITECT FOR APPROVAL.

VINYL DUMPSTER ENCLOSURE (1)
NOT TO SCALE

PROGRESS PRINT
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OCTOBER 18, 2021

PROFESSIONAL ENGINEER FOR
ALLEN & MAJOR ASSOCIATES, INC.

REV	DATE	DESCRIPTION

APPLICANT\OWNER:
2-14 MEDFORD STREET, LLC
455 MASSACHUSETTS AVE, STE 1
ARLINGTON, MA 02474

PROJECT:
455-457
MASSACHUSETTS AVE
ARLINGTON, MA 02476

PROJECT NO.	2729-03	DATE:	8/20/2021
SCALE:	AS SHOWN	DWG. NAME:	C2729-03
DESIGNED BY:	JG/ARM	CHECKED BY:	ARM/BDJ

PREPARED BY:

ALLEN & MAJOR ASSOCIATES, INC.
civil engineering ◆ land surveying
environmental consulting ◆ landscape architecture
www.allenmajor.com
100 COMMERCE WAY, SUITE 5
WOBURN MA 01801
TEL: (781) 935-6889
FAX: (781) 935-2896
WOBURN, MA ◆ LAKEVILLE, MA ◆ MANCHESTER, NH

DRAWING TITLE: DETAILS

SHEET No. C-503

R:\PROJECTS\2729-03\CIVIL\DRAWINGS\CURRENT\C-2729-03_DETAILS.DWG

From: Peter Slowik <pslowik@marketsquarearchitects.com>
Subject: 455 Mass Ave supplemental info - bike rack specs and lighting intent
Date: September 27, 2021 at 11:51:56 AM EDT
To: John Murphy <john@summit-res.net>

John,

Should the questions arise, please see attached for bike rack specifications for indoor bike storage. These were used in another conforming project in Arlington, and have been spaced to exceed the Bylaw guide minimums.

For exterior lighting, we intend to provide the minimum feasible to maintain a safe living environment, particularly at the two rear pedestrian corridors surrounding the garage. Existing street lights will provide ample illumination in proximity to Park Terrace, and will be supplemented with a single adjustable downlight fixture provided at each entrance. The interior of the garage will be illuminated with ceiling lights, and the wall opening shall be constructed such that the head height sufficiently shields direct light from neighboring structures. Mass Ave and both side streets are illuminated by street lights.

Peter Slowik
Market Square Architects, PLLC
Project Coordinator
P: (603) 501-0202
www.MarketSquareArchitects.com

SECTION 129310

BICYCLE RACKS

PART 1 - GENERAL

1.1 GENERAL PROVISIONS

A. Attention is directed to the CONTRACT AND GENERAL CONDITIONS and all Sections within DIVISION 01 - GENERAL REQUIREMENTS which are hereby made a part of this Section of the Specifications.

1.2 DESCRIPTION OF WORK

A. Work Included: Provide labor, materials and equipment necessary to complete the work of this Section, including but not limited to the following:

1. Bicycle racks.

B. Related Work: The following items are not included in this Section and will be performed under the designated Sections:

1. Section 033000 - CAST-IN-PLACE CONCRETE for lintels, sleeves, anchors, inserts, plates and similar items.

1.3 SUBMITTALS

A. Product Data: Include for each product specified.

1.4 QUALITY ASSURANCE

A. Source Limitations: Obtain products through one source from a single manufacturer.

1.5 COORDINATION

A. Coordinate size and location of blocking and mounting details.

PART 2 - PRODUCTS

2.1 BICYCLE RACKS

A. Basis of Design: Dero Bike Rack Co.; Hoop Rack.

B. Materials:

1. Pipe: 1.5 inch schedule 40 steel pip with powder coat finish.
2. Mounting Rails: 3 x 1.4 x 3/16 inch thick galvanized steel rails.

C. Configuration:

1. U-Hoop Style bike rack for mounting 2 bikes per hoop.

2. Hoops are bolted to two parallel rails, anchored to the ground.
3. Angle: 90 degrees.
4. Distance Between Racks: 3 feet.

PART 3 - EXECUTION

3.1 EXAMINATION

A. Examine substrates, areas, and conditions, with Installer present, for compliance with requirements for installation tolerances, operational clearances, and other conditions affecting performance.

B. Proceed with installation only after unsatisfactory conditions have been corrected.

3.2 INSTALLATION

A. Install in accordance with Manufacturer's recommendations.

3.3 CLEANING AND PROTECTION

A. Protect installed products until completion of project and touch-up, repair or replace damaged products before Substantial Completion.

END OF SECTION

Town of Arlington, Massachusetts

Department of Planning & Community Development

730 Massachusetts Avenue, Arlington, Massachusetts 02476

Public Hearing Memorandum

The purpose of this memorandum is to provide the Arlington Redevelopment Board and public with technical information and a planning analysis to assist with the regulatory decision-making process.

To: Arlington Redevelopment Board

From: Jennifer Raitt, Secretary Ex Officio

Subject: Environmental Design Review, 455-457 Massachusetts Avenue, Arlington, MA, Docket #3673

Date: September 27, 2021

I. Docket Summary

This is an application by 2-14 Medford Street, LLC, 455 Massachusetts Avenue, Suite 1, Arlington, MA, to open Special Permit Docket #3673 for the construction of a mixed-use building containing existing retail, restaurants, and adding 13 residential units, including two affordable rental units, at 455-457 Massachusetts Avenue and 2-14 Medford Street in the B3 Village Business District. The opening of the hearing is to allow the Board to review and approve the development under Section 3.4, Environmental Design Review Special Permit of the Arlington Zoning Bylaw.

The Applicant proposes to convert a portion of existing ground floor retail and second floor office spaces to residential units and to add a second floor to a one-story brick commercial structure, consolidating two buildings into one mixed use building. The proposed building is a two-story mixed-use structure. The 13 residential units are comprised of one studio and 12 one-bedroom apartments. Two affordable units, or 15% of the total units, are provided. Ten of the 11 existing commercial spaces would remain, resulting in a total commercial area of 7,802 square feet. Parking is provided onsite in a garage with 14 parking spaces.

Materials submitted for consideration of this application:

- Application for EDR Special Permit, including an Environmental Impact Statement;

- Site Development Plan Set, prepared by Allen & Major Associates, Inc. dated August 20, 2021;
- Architectural Drawing Set, including floor plans, elevations, and renderings, prepared by Market Square Architects, dated July 27, 2021;
- Description of project construction process by project estimated, no date;
- Mixed-Use Redevelopment Drainage Summary prepared by Allen & Major Associates, dated August 20, 2021; and
- LEED Checklist.

II. Application of Special Permit Criteria (Arlington Zoning Bylaw, Section 3.3)

1. Section 3.3.3.A.

The use requested is listed as a Special Permit in the use regulations for the applicable district or is so designated elsewhere in this Bylaw.

Mixed-use is allowed by Special Permit in the B3 Village Business District. The Zoning Bylaw, in Section 5.5.1.D, indicates that the district's predominant uses include retail, service, and office establishments catering to both convenience and comparison-good shoppers and oriented to pedestrian traffic. Mixed-use buildings are allowed and encouraged, including in the principal business area at Medford Street and Massachusetts Avenue. Mixed-use is a combination of two or more distinct land uses, such as those proposed by this applicant, and the definition encourages such uses to be in a single, multi-story structure, such as that proposed by the applicant.

The Arlington Center area, which radiates outward from the intersection of Massachusetts Avenue and Mystic and Pleasant Streets, is a major shopping district in the town. It is comprised of a blend of zoning districts from R1 through R7 and B1 through the town's only B5 districts. Arlington Center includes several B3 districts; the district in which this parcel sits stretches from the southeast edge of Whittemore Park (Old Mystic Street) and Massachusetts Avenue to just beyond Franklin Street. To the southwest across Massachusetts Avenue is the B5 district, there are predominately single-story and two-story commercial buildings. To the northwest within the B3 district are additional two-story commercial storefronts along Massachusetts Avenue, there is a two-and-a-half story apartment building facing the Russell Common Parking Lot; to the southeast across Medford Street is another single-story and two-story commercial block, which includes the Regent Theater. To the northeast is the three-story Arlington Catholic High School and Saint Agnes Parish, and the Russell Common Parking Lot, all within the R1 district; directly east is the R2 zoning district with a predominantly two-family residential streetscape of Arlington Center is dominant.

The Board can find that this condition is met.

2. **<u>Section 3.3.3.B.</u>**
The requested use is essential or desirable to the public convenience or welfare.

The requested use is essential and desirable. The second key finding in the Master Plan notes that "Massachusetts Avenue has the capacity for growth. It can support mixed-use development commensurate with its function as Arlington's primary commercial corridor. Massachusetts Avenue is accessible to neighborhoods throughout the town; it has frequent bus service, bicycle routes, and good walkability. Increased density through greater building heights and massing would benefit the corridor from an urban design perspective and benefit the town from a fiscal perspective"(p.8).

This proposal will bring one new studio and 12 one-bedroom residential apartment units, of which two will be affordable to households earning at or below 70% of the area median income. The Town has clearly established affordable housing priorities described in its Housing Production Plan (adopted by the Select Board and Redevelopment Board and approved by the State in 2016). New housing opportunities, including market-rate and affordable homes, are needed in the community; this project helps address that demand.

The ground floor commercial spaces along Medford Street and Massachusetts Avenue will largely remain, although several will be reduced in area as part of the renovation, and one will be converted to a lobby. Ten commercial spaces (noted as commercial tenants on the architectural plans) would remain following the redevelopment of the site, as would venting for the existing restaurants. The office spaces on the second floor would be eliminated. Overall, there will be net loss of approximately 6,900 square feet of commercial space. The loss is due to the need to provide an entry and circulation for the second-floor housing units, the elimination of office spaces on the second floor, and the need to also provide parking on the site.

The Board can find that this condition is met.

3. **<u>Section 3.3.3.C.</u>**
The requested use will not create undue traffic congestion or unduly impair pedestrian safety.

The proposed project includes 16 parking spaces for cars, comprised of two surface parking spaces, and 13 garage parking spaces, and one ADA accessible parking space. Regarding bicycle parking, the project includes eight short-term outdoor parking spaces and 26 long-term indoor spaces for building tenants and visitors. It is not expected that the proposed project will unduly impair pedestrian safety. The Board can find that this condition is met.

4. **<u>Section 3.3.3.D.</u>**
The requested use will not overload any public water, drainage or sewer system or any other municipal system to such an extent that the requested use or any

developed use in the immediate area or in any other area of the Town will be unduly subjected to hazards affecting health, safety, or the general welfare.

A Drainage Summary letter indicates that standards have been met with the proposed stormwater design, which includes the installation of pervious paver systems and an underground filtration system. In addition, a landscaped buffer will be introduced the site. Overall the proposal will result in a reduction of impervious area and quantity of stormwater flowing from the site. The proposed project will improve, not overload, public utilities. The Board can find that this condition is met.

5. **Section 3.3.3.E.**
Any special regulations for the use as may be provided in the Bylaw are fulfilled.

As a condition of any decision for the proposed mixed-use building, the Applicant will need to fulfill the requirements of Section 8.2 which outline the affordable housing requirements. A building with 13 units requires two affordable units that are representative of the mix of units in the building available to eligible households making up to 70% of the area median income. There are no other special regulations for the use that must be fulfilled. The Board can find that this condition is met.

6. **Section 3.3.3.F.**
The requested use will not impair the integrity or character of the district or adjoining districts, nor be detrimental to the health or welfare.

The proposed building largely maintains the existing first floor façade, with the additional improvement of restoration of decorative cement work at 455 Massachusetts Avenue, which is currently obscured by an awning. The façade of the commercial storefronts at 2-14 Medford Street is proposed to be retained, preserving the historic aesthetic of the structure consistent with other buildings in the business district.

The redevelopment of the second story and conversion into housing units will not impair the integrity or character of the district or the adjoining districts and it will not be detrimental to health or welfare. The proposed structure is generally consistent with the Design Standards for the Town of Arlington.

The upper floor façade is comprised of white fiber cement panels along Massachusetts Avenue, and light green fiber cement panels along Medford Street. The building also includes differentiation of the upper story and variation in the façade with a shared roof deck at the intersection of Massachusetts Avenue and Medford Street, which is encouraged in the Arlington Design Standards. Along Medford Street, the design of the second story is compatible the Regent Theater and its adjacent properties directly across the street. Additionally, the applicant has agreed to make the Park Terrace façade available for public art or mural installations to improve the relationship with the streetscape. The Applicant has also proposed installation of a more permanent

outdoor seating area with a canopy for more comfortable and year-round outdoor seating for the community.

While both 455-457 Massachusetts Avenue and 4-14 Medford Street are in the Arlington Center Historic District, only 4-14 Medford Street is a contributing historic structure. Both structures are under the jurisdiction of the Arlington Historical Commission, who will need to review the building design.

Long-term indoor bicycle parking is accessed through either the lobby or behind the building, short-term outside bicycle parking is available along Park Terrace; structured and surface vehicular parking is located on the ground floor and accessed via Park Terrace.

The Applicant is proposing a floor area ratio (FAR) of 1.5. The existing building's FAR is 0.98. The Zoning Bylaw allows a maximum FAR of 1.5 for a mixed-use building on a lot less than 20,000 square feet in this zoning district. The maximum FAR of 1.5 is at odds with the maximum story and height allowed in the Zoning Bylaw of up to 5 stories and 60 feet respectively.

The proposed mixed-use building is in keeping with adjacent land uses, both Massachusetts Avenue and Medford Street include residential uses, including The Legacy across Massachusetts Avenue to the east and multifamily buildings along Medford Street. While it is desired to maintain or increase the amount of commercial space, new residential units will not impair the integrity or character of the district, or the adjoining districts and it will not be detrimental to health or welfare.

7. **Section 3.3.3.G.**
The requested use will not, by its addition to a neighborhood, cause an excess of the use that could be detrimental to the character of said neighborhood.

The use will not be in excess or detrimental to the character of the neighborhood. The Board can find this condition is met.

III. Environmental Design Review Standards (Arlington Zoning Bylaw, Section 3.4)

1. **EDR-1 Preservation of Landscape**
The landscape shall be preserved in its natural state, insofar as practicable, by minimizing tree and soil removal, and any grade changes shall be in keeping with the general appearance of neighboring developed areas.

The existing property is entirely impervious and there is no natural landscape to preserve with the building fully saturating the building lot. As part of the project, two areas of approximately 2,457 square feet of impervious material will be replaced with a landscaped buffer of perennials, flowering trees, and arborvitae and pervious walkways along the side property line and a landscaped walkway to the lobby. The new

landscaping will introduce a buffer from the rear parking lot of the property at 473-475 Massachusetts Ave. The Board can find that this condition is met.

2. **<u>EDR-2 Relation of the Building to the Environment</u>**
Proposed development shall be related harmoniously to the terrain and to the use, scale, and architecture of the existing buildings in the vicinity that have functional or visible relationship to the proposed buildings. The Arlington Redevelopment Board may require a modification in massing so as to reduce the effect of shadows on the abutting property in an R0, R1 or R2 district or on public open space.

There are a range of architectural styles and zoning districts in the vicinity. Building heights in the vicinity range from single-story to 11-stories. The ground floor storefronts will largely be preserved, however the transparency of the lobby for the apartment units will be reduced as the existing glass storefront will be reconstructed with three smaller windows. The applicant proposes to remove the awning and restore the concrete detailing above the façade at 455 Massachusetts Avenue storefront; the existing benches along Massachusetts Avenue will be maintained, and flower boxes and awnings will be added along the façade. The second floor façade maintains a commercial mixed-use appearance and roofline consistent with adjacent structures in the district.

The applicant seeks relief from required setbacks. The structure is on a corner lot and subject to Section 5.3.8.A, which states that the setback should be the same as an adjacent lot. On the Medford Street, the adjoining property is the Arlington Catholic High School in the R1 Zoning District, which has a significantly larger setback than the existing structure at 2-14 Medford St. The Board can adjust this requirement per Section 5.3.16. The Board can find that this condition is met.

3. **<u>EDR-3 Open Space</u>**
All open space (landscaped and usable) shall be so designed as to add to the visual amenities of the vicinity by maximizing its visibility for persons passing by the site or overlooking it from nearby properties. The location and configuration of usable open space shall be so designed as to encourage social interaction, maximize its utility and facilitate maintenance.

As noted above, the proposed project will add areas of landscaping to an existing impervious site. The proposal includes approximately 2,457 square feet of landscaped open space along the rear of the building, which also provides a buffer with the adjacent building at 473-475 Massachusetts Avenue. This provides about half of the 10% landscaped open space requirement. The usable open space is located on the roof deck and is approximately 2,140 square feet. This is only 9% of the usable open space requirement and does not meet all the requirements for usable open space.

Lastly, the applicant seeks relief from the required 15-foot buffer in Section 5.3.21, as a landscaped buffer is precluded by the applicant's need to provide parking on-site. The Board may determine that a waiver is required as the adjacent R1 site is occupied by the Arlington Catholic High School, which is not a "buildable residential lot."

4. **EDR-4 Circulation**

With respect to vehicular and pedestrian and bicycle circulation, including entrances, ramps, walkways, drives, and parking, special attention shall be given to location and number of access points to the public streets (especially in relation to existing traffic controls and mass transit facilities), width of interior drives and access points, general interior circulation, separation of pedestrian and vehicular traffic, access to community facilities, and arrangement of vehicle parking and bicycle parking areas, including bicycle parking spaces required by Section 6.1.12 that are safe and convenient and, insofar as practicable, do not detract from the use and enjoyment of proposed buildings and structures and the neighboring properties.

The proposed project includes a total of 16 vehicle and 34 bicycle parking spaces (see Site Development Plan Set, Sheet C-102). The ground-level garage will provide 14 spaces for vehicles, including one van-accessible HP vehicle space. Two surface parking spaces are located adjacent to the garage. Parking access is provided via a drive aisle from Park Terrace. The proposed number of vehicle parking spaces is an increase over the existing conditions; at present the site provides 14 parking spaces, eight of which are tandem spaces blocked by the other six spaces and none of which are HP spaces.

The parking requirement is for mixed-use which calculates the parking required for each individual use; the parking required for the residential use totals 15 parking spaces, and while the commercial, business, and retail space would typically require 27 parking spaces, the first 3,000 square feet of non-residential space in mixed-use buildings is exempt from the parking requirements per Section 6.1.10.C. The applicant is requesting a reduction in the number of parking spaces provided to 16 spaces per Section 6.1.5 of the Zoning Bylaw. As such, the Applicant should submit a Transportation Demand Management Plan.

Pedestrian circulation behind the building would be improved, as two pervious walkways connecting the rear of each storefront and the lobby to Park Terrace, as well as the trash and recycling area, are proposed. The sidewalk on Park Terrace should be reconstructed. Any potential improvements in the public right-of-way will require additional review and approval by the Engineering Division. Additionally, benches along Massachusetts Avenue, often used by individuals waiting at the adjacent bus stop, will remain. Planters along the Massachusetts Avenue façade and landscaping at the rear of the building will improve the human scale elements of the ground floor space.

Regarding bicycle parking, the applicant proposes both short-term and long-term bicycle parking exceeding the required minimum. Eight short-term spaces are provided off Park Terrace, one more than the minimum required. Indoor long-term bicycle parking is provided behind the lobby and accessible through the Massachusetts Avenue

entry or via a walkway behind the storefronts at 2-14 Medford Street; 26 spaces are proposed, five more than the minimum required. Bike rack specifications are needed to determine compliance with Section 6.1.12.E.

Vehicle Parking Requirements*			
Number of Bedrooms/ Apartment Use	**Number of Units**	**Zoning Requirement**	**Total Parking Required**
1-bedroom	12	1.15	14
Studio	1	1	1
Commercial	Square feet	Zoning Requirement	Total Parking Required
Service	5,377	1 per 300sf	18
Business	1,255	1 per 500sf	2.5
Retail	1,951	1 per 300sf	6.5
		Total Required Vehicle Parking	**42**
		Total Proposed Vehicle Parking after Section 6.1.5 Reduction	**32**
* First 3,000sf of non-residential space in mixed-use buildings is exempt.			

Bicycle Parking Requirements		
Use	**Short-Term Parking**	**Long-Term Parking**
Residential	1.3 spaces	19.5 spaces
Retail	5 spaces	1.1 spaces
Total Required Bicycle Parking	**7**	**21**
Total Proposed Bicycle Parking	8	**26**

The proposed project is highly accessible by transit, bike, and walking, and since there is a reduction in commercial space, it is likely that the aggregate number of trips to this location will be reduced. The provided parking is intended for residential tenants and not for patrons of the commercial space, with commercial tenants and visitors relying on on-street parking or the Russell Common Parking Lot. As such, the actual impact of the current trips is dispersed across Massachusetts Avenue, Medford Street, Broadway Plaza, and the Russell Common Parking Lot.

The applicant is seeking relief from the drive aisle dimensions described in Section 6.1.11.C(3). The parking garage provides a 20-foot drive aisle, less than the required 24-foot aisle necessary for two-way traffic. A formalized concrete drive apron is proposed to improve access to parking. Due to the existing street network and one-way condition on Park Terrace, parking would be accessed through the Russell Common Parking Lot, and vehicles would exit the site from Park Terrace onto Medford Street. It may be appropriate for the ARB to request a trip distribution analysis to assess how people will access this site. The ARB may also wish to request additional details on how service vehicles and delivery trucks would access the site.

5. <u>EDR-5 Surface Water Drainage</u>

Special attention shall be given to proper site surface drainage so that removal of surface waters will not adversely affect neighboring properties or the public storm drainage system. Available Best Management Practices for the site should be employed, and include site planning to minimize impervious surface and reduce clearing and re-grading. Best Management Practices may include erosion control and stormwater treatment by means of swales, filters, plantings, roof gardens, native vegetation, and leaching catch basins. Stormwater should be treated at least minimally on the development site; that which cannot be handled on site shall be removed from all roofs, canopies, paved and pooling areas and carried away in an underground drainage system. Surface water in all paved areas shall be collected in intervals so that it will not obstruct the flow of vehicular or pedestrian traffic and will not create puddles in the paved areas. In accordance with Section 3.3.4., the Board may require from any Applicant, after consultation with the Director of Public Works, security satisfactory to the Board to ensure the maintenance of all stormwater facilities such as catch basins, leaching catch basins, detention basins, swales, etc. within the site. The Board may use funds provided by such security to conduct maintenance that the Applicant fails to do. The Board may adjust in its sole discretion the amount and type of financial security such that it is satisfied that the amount is sufficient to provide for any future maintenance needs.

The application materials, drainage summary letter, and site development plan show that surface water drainage will be improved through the installation of pervious pavers and an underground stormwater infiltration system that will receive reduce stormwater runoff from the site. In addition, approximately 1,008 square feet of presently impervious pavement will be replaced with landscaped areas. This is an improvement over the existing conditions. The proposed design complies with the Town's current stormwater bylaw. Final design materials must be submitted for review and approval by the Town Engineer.

6. <u>EDR-6 Utilities Service</u>

Electric, telephone, cable TV, and other such lines of equipment shall be underground. The proposed method of sanitary sewage disposal and solid waste disposal from all buildings shall be indicated.

All utility service will be provided through connections to existing utility lines adjacent to the site. The Board can find that this condition is met.

7. <u>EDR-7 Advertising Features</u>

The size, location, design, color, texture, lighting and materials of all permanent signs and outdoor advertising structures or features shall not detract from the use and enjoyment of proposed buildings and structures and the surrounding properties.

The application materials show a representative sign on one of the commercial storefronts. Any future signage would be subject to review by the Department of

Planning and Community Development, and possibly the Redevelopment Board, prior to the issuance of a sign permit. Additionally, lighting and any other potential outdoor features relative to the building should be provided.

8. **EDR-8 Special Features**

Exposed storage areas, exposed machinery installations, service areas, truck loading areas, utility buildings and structures, and similar accessory areas and structures shall be subject to such setbacks, screen plantings or other screening methods as shall reasonably be required to prevent their being incongruous with the existing or contemplated environment and the surrounding properties.

The roofing plan provided indicates that roof structures are appropriately set back and that a parapet and additional screening will provide screening of said structures. The site plan shows an enclosed dumpster and recycling area located adjacent to the surface parking. The existing businesses currently utilize Park Terrace for truck loading and unloading and for service deliveries. The Board can find that this condition is met.

9. **EDR-9 Safety**

With respect to personal safety, all open and enclosed spaces shall be designed to facilitate building evacuation and maximize accessibility by fire, police and other emergency personnel and equipment. Insofar as practicable, all exterior spaces and interior public and semi-public spaces shall be so designed to minimize the fear and probability of personal harm or injury by increasing the potential surveillance by neighboring residents and passersby of any accident or attempted criminal act.

The proposed building has been designed to meet all relevant health and safety codes. A lighting plan was not provided as part of the plan set. These details are needed to assess safety criteria and compliance.

10. **EDR-10 Heritage**

With respect to Arlington's heritage, removal or disruption of historic, traditional or significant uses, structures or architectural elements shall be minimized insofar as practical whether these exist on the site or on adjacent properties.

While both structures are part of the Arlington Center Historic District area, only 4-14 Medford Street is a contributing historic structure as identified on the *Inventory of Historically or Architecturally Significant Properties in the Town of Arlington*. Both structures are under the jurisdiction of the Arlington Historical Commission, which will need to review the building design.

11. **EDR-11 Microclimate**

With respect to the localized climatic characteristics of a given area, any development which proposes new structures, new hard surface, ground coverage or the installation of machinery which emits heat, vapor or fumes shall endeavor to

minimize insofar as practicable, any adverse impacts on light, air and water resources or on noise and temperature levels of the immediate environment.

Based upon materials provided in the application, there will be no adverse impacts on air and water resources or on temperature levels of the immediate environment. While the applicant states that they intend to minimize any adverse impact, additional plan details are needed regarding lighting and emissions from machinery located on the roof to determine any impacts on the immediate environment.

12. <u>EDR-12 Sustainable Building and Site Design</u>
Projects are encouraged to incorporate best practices related to sustainable sites, water efficiency, energy and atmosphere, materials and resources, and indoor environmental quality. Applicants must submit a current Green Building Council Leadership in Energy and Environmental Design (LEED) checklist, appropriate to the type of development, annotated with narrative description that indicates how the LEED performance objectives will be incorporated into the project.

The proposed building generates a LEED score that demonstrates the building could qualify for LEED platinum certification. The Board can find that this condition is met.

IV. Findings

The following findings are for the Board's consideration:

1. The ARB finds that the project is consistent with Environmental Design Review per Section 3.4 of the Zoning Bylaw.
2. The ARB finds that the setbacks on Lake Street and Chandler Street are appropriate per Section 5.3.16.
3. The ARB finds that the two-story building will not adversely affect the adjacent R1 zoning districts per Section 5.3.19.
4. The ARB finds that the vehicle and bicycle parking improvements justify the parking reduction per Section 6.1.5.

V. Conditions

A. General

1. The final design, sign, exterior material, landscaping, and lighting plans shall be subject to the approval of the Arlington Redevelopment Board or administratively approved by the Department of Planning and Community Development. Any substantial or material deviation during construction from the approved plans and specifications is subject to the written approval of the Arlington Redevelopment Board.

2. Any substantial or material deviation during construction from the approved plans and specifications is subject to the written approval of the Arlington Redevelopment Board.

3. The Board maintains continuing jurisdiction over this permit and may, after a duly advertised public hearing, attach other conditions or modify these conditions as it deems appropriate in order to protect the public interest and welfare.

4. Snow removal from all parts of the site, as well as from any abutting public sidewalks, shall be the responsibility of the owner and shall be accomplished in accordance with Town Bylaws.

5. Trash shall be picked up only on Monday through Friday between the hours of 7:00 am and 6:00 pm. All exterior trash and storage areas on the property, if any, shall be properly screened and maintained in accordance with Article 30 of Town Bylaws.

6. The Applicant shall provide a statement from the Town Engineer that all proposed utility services have adequate capacity to serve the development. The applicant shall provide evidence that a final plan for drainage and surface water removal has been reviewed and approved by the Town Engineer.

7. Upon installation of landscaping materials and other site improvements, the Applicant shall remain responsible for such materials and improvement and shall replace and repair as necessary to remain in compliance with the approved site plan.

8. All utilities serving or traversing the site (including electric, telephone, cable, and other such lines and equipment) shall be underground.

9. Upon the issuance of the building permit the Applicant shall file with the Building Inspector and the Department of Community Safety the names and telephone numbers of contact personnel who may be reached 24 hours each day during the construction period.

10. Building signage will be filed with and reviewed and approved by the Department of Planning and Community Development and Inspectional Services.

B. Special Conditions

1. The owner will work with the Department of Planning and Community Development to comply with all requirements of Section 8.2, Affordable Housing Requirements.

2. The affordable units must be equitably dispersed throughout the building and shall be comparable to market-rate units in terms of location, quality and character, room size, number of rooms, number of bedrooms, and external appearance.

3. An Affordable Housing Deed Restriction shall be executed with the Town prior to issuance of an Occupancy Permit for the two affordable units.

4. No condominium conversion of said affordable rental units shall be permitted without the express permission of this Board. In the case of a proposed condominium conversion, Applicant shall work with the Department of Planning and Community Development to ensure that the units continue to meet the requirements of Section 8.2.

DUPLICATE

TOWN OF ARLINGTON

TOWN CLERK

Date August 30, 20 21

Rec'd from 2-14 Medford Street, LLC

Address 455 Massachusetts Avenue, Suite 1

Arlington, MA 02474

By jaw

		AMOUNT	
	Certificate		
	Marriage Int.		
	Fin. Statement		
	Pole Location		
	Garage Renewal		
	Misc. Books		
	Sporting Licenses		
	Dup. Dog Tags		
	Dog Licenses		
	Citation #		
1	Misc. Licenses / Permits EDR	4,510.	00
Leader Bank check #1222			

29956

RV PRINT SOLUTIONS 603-580-4687 33268277 LB 1/17

Docket 3673

PLANNING & COMMUNITY DEVELOPMENT
2021 AUG 27 A 11:06

TOWN OF ARLINGTON
REDEVELOPMENT BOARD
Application for Special Permit In Accordance with Environmental Design Review Procedures (Section 3.4 of the Zoning Bylaw)

Docket No. 3673

1. Property Address 455-457 Massachusetts Avenue & 2-14 Medford Street
   Name of Record Owner(s) 2-14 Medford Street, LLC Phone 781-654-6306
   Address of Owner 455 Massachusetts Avenue, Ste 1, Arlington, MA 02474
   Street City, State, Zip

2. Name of Applicant(s) (if different than above) Same as above
   Address _____ Phone _____
   Status Relative to Property (occupant, purchaser, etc.) _____

3. Location of Property Map 45, Lots 4-5.a
   Assessor's Block Plan, Block, Lot No.

4. Deed recorded in the Registry of deeds, Book _____, Page _____;
   -or- registered in Land Registration Office, Cert. No. 7802C, in Book 1523, Page 99.

5. Present Use of Property (include # of dwelling units, if any) Retail, Service, Restaurant

6. Proposed Use of Property (include # of dwelling units, if any) Mixed-Use Apartment Units & Retail, Service, Restaurant

7. Permit applied for in accordance with the following Zoning Bylaw section(s)

section(s)	title(s)
	See Exhibit "A" attached

RECEIVED
2021 AUG 30 AM 12:55
TOWN CLERK'S OFFICE ARLINGTON, MA 02174

8. Please attach a statement that describes your project and provide any additional information that may aid the ARB in understanding the permits you request. Include any reasons that you feel you should be granted the requested permission.

   See attached

(In the statement below, strike out the words that do not apply)

The applicant states that 2-14 Medford Street, LLC is the owner -or- occupant -or- purchaser under agreement of the property in Arlington located at 455-457 Massachusetts Avenue & 2-14 Medford Street which is the subject of this application; and that unfavorable action -or- no unfavorable action has been taken by the Zoning Board of Appeals *on a similar application regarding this property within the last two years.* The applicant expressly agrees to comply with any and all conditions and qualifications imposed upon this permission, either by the Zoning Bylaw or by the Redevelopment Board, should the permit be granted.

Signature of Applicant(s)

1171 Massachusetts Avenue, Arlington, MA 02476 781-646-4911
Address Phone

Exhibit A to Application for Special Permit
in Accordance with Environmental Design Review
455-457 Massachusetts Avenue
Arlington, MA

7. Permit applied for in accordance with the following Zoning Bylaw section(s):

3.4	Environmental Design Review
5.5.2	Dimensional and Density Regulations
SP	(Mixed Use <=20,000 SF)
6.12.5	Parking Reduction in a Business Zone
6.1.11	Parking Standards
5.3.21	Screening and Buffer Requirements
5.3.16	Setback Requirements
5.3.19	Reduced Height Buffer Area
5.3.8	Corner Lot Requirements
5.2.4	Multiple Principal Uses

Town of Arlington Redevelopment Board
Application for Special Permit in accordance with
Environmental Design Review (Section 3.4)

Required Submittals Checklist

Two full sets of materials and one electronic copy are required. A model may be requested. Review the ARB's Rules and Regulations, which can be found at arlingtonma.gov/arb, for the full list of required submittals.

_X_ Dimensional and Parking Information Form (see attached)

_X_ Site plan of proposal

N/A Model, if required

_X_ Drawing of existing conditions

_X_ Drawing of proposed structure

_X_ Proposed landscaping. May be incorporated into site plan

_X_ Photographs

_X_ Impact statement

N/A Application and plans for sign permits

_X_ Stormwater management plan (for stormwater management during construction for projects with new construction

FOR OFFICE USE ONLY

_____ Special Permit Granted Date: _____

_____ Received evidence of filing with Registry of Deeds Date: _____

_____ Notified Building Inspector of Special Permit filing Date: _____

TOWN OF ARLINGTON
REDEVELOPMENT BOARD
Petition for Special Permit under Environmental Design Review (see Section 3.4 of the Arlington Zoning Bylaw for Applicability)

For projects subject to Environmental Design Review, (see Section 3.4), please submit a statement that completely describes your proposal, and addresses each of the following standards.

1. Preservation of Landscape. The landscape shall be preserved in its natural state, insofar as practicable, by minimizing tree and soil removal, and any grade changes shall be in keeping with the general appearance of neighboring developed areas.

2. Relation of Buildings to Environment. Proposed development shall be related harmoniously to the terrain and to the use, scale, and architecture of existing buildings in the vicinity that have functional or visual relationship to the proposed buildings. The Arlington Redevelopment Board may require a modification in massing to reduce the effect of shadows on abutting property in an R0, R1 or R2 district or on public open space.

3. Open Space. All open space (landscaped and usable) shall be so designed as to add to the visual amenities of the vicinity by maximizing its visibility for persons passing the site or overlooking it from nearby properties. The location and configuration of usable open space shall be so designed as to encourage social interaction, maximize its utility, and facilitate maintenance.

4. Circulation. With respect to vehicular, pedestrian and bicycle circulation, including entrances, ramps, walkways, drives, and parking, special attention shall be given to location and number of access points to the public streets (especially in relation to existing traffic controls and mass transit facilities), width of interior drives and access points, general interior circulation, separation of pedestrian and vehicular traffic, access to community facilities, and arrangement of vehicle parking and bicycle parking areas, including bicycle parking spaces required by Section 8.13 that are safe and convenient and, insofar as practicable, do not detract from the use and enjoyment of proposed buildings and structures and the neighboring properties.

5. Surface Water Drainage. Special attention shall be given to proper site surface drainage so that removal of surface waters will not adversely affect neighboring properties or the public storm drainage system. Available Best Management Practices for the site should be employed, and include site planning to minimize impervious surface and reduce clearing and re-grading. Best Management Practices may include erosion control and storm water treatment by means of swales, filters, plantings, roof gardens, native vegetation, and leaching catch basins. Storm water should be treated at least minimally on the development site; that which cannot be handled on site shall be removed from all roofs, canopies, paved and pooling areas and carried away in an underground drainage system. Surface water in all paved areas shall be collected at intervals so that it will not obstruct the flow of vehicular or pedestrian traffic, and will not create puddles in the paved areas.

   In accordance with Section 3.3.4, the Board may require from any applicant, after consultation with the Director of Public Works, security satisfactory to the Board to insure the maintenance of all storm water facilities such as catch basins, leaching catch basins, detention basins, swales, etc. within the site. The Board may use funds provided by such security to conduct maintenance that the applicant fails to do. The Board may adjust in its sole discretion the amount and type of financial security such that it is satisfied that the amount is sufficient to provide for the future maintenance needs.

6. Utility Service. Electric, telephone, cable TV and other such lines and equipment shall be underground. The proposed method of sanitary sewage disposal and solid waste disposal from all buildings shall be indicated.

7. Advertising Features. The size, location, design, color, texture, lighting and materials of all permanent signs and outdoor advertising structures or features shall not detract from the use and enjoyment of proposed buildings and structures and the surrounding properties. Advertising features are subject to the provisions of Section 6.2 of the Zoning Bylaw.

8. Special Features. Exposed storage areas, exposed machinery installations, service areas, truck loading areas, utility buildings and structures, and similar accessory areas and structures shall be subject to such setbacks, screen plantings or other screening methods as shall reasonably be required to prevent their being incongruous with the existing or contemplated environment and the surrounding properties.

9. Safety. With respect to personal safety, all open and enclosed spaces shall be designed to facilitate building evacuation and maximize accessibility by fire, police, and other emergency personnel and equipment. Insofar as practicable, all exterior spaces and interior public and semi-public spaces shall be so designed as to minimize the fear and probability of personal harm or injury by increasing the potential surveillance by neighboring residents and passersby of any accident or attempted criminal act.

10. Heritage. With respect to Arlington's heritage, removal or disruption of historic, traditional or significant uses, structures, or architectural elements shall be minimized insofar as practicable, whether these exist on the site or on adjacent properties.

11. Microclimate. With respect to the localized climatic characteristics of a given area, any development which proposes new structures, new hard-surface ground coverage, or the installation of machinery which emits heat, vapor, or fumes, shall endeavor to minimize, insofar as practicable, any adverse impact on light, air, and water resources, or on noise and temperature levels of the immediate environment.

12. Sustainable Building and Site Design. Projects are encouraged to incorporate best practices related to sustainable sites, water efficiency, energy and atmosphere, materials and resources, and indoor environmental quality. Applicants must submit a current Green Building Council Leadership in Energy and Environmental Design (LEED) checklist, appropriate to the type of development, annotated with narrative description that indicates how the LEED performance objectives will be incorporated into the project. [LEED checklists can be found at http://www.usgbc.org/DisplayPage.aspx?CMSPageID=220b]

In addition, projects subject to Environmental Design Review must address and meet the following Special Permit Criteria (see Section 3.3.3 of the Zoning Bylaw):

1. The use requested is listed as a special permit in the use regulations for the applicable district or is so designated elsewhere in this Bylaw.

2. The requested use is essential or desirable to the public convenience or welfare.

3. The requested use will not create undue traffic congestion or unduly impair pedestrian safety.

4. The requested use will not overload any public water, drainage or sewer system or any other municipal system to such an extent that the requested use or any developed use in the immediate area or in any other area of the Town will be unduly subjected to hazards affecting health, safety or the general welfare.

5. Any special regulations for the use as may be provided in this Bylaw are fulfilled.

6. The requested use will not impair the integrity or character of the district or adjoining districts, nor be detrimental to the health, morals, or welfare.

7. The requested use will not, by its addition to a neighborhood, cause an excess of the particular use that could be detrimental to the character of said neighborhood.

TOWN OF ARLINGTON
Dimensional and Parking Information
for Application to
The Arlington Redevelopment Board

Docket No. _____

Property Location 455-457 Massachusetts Ave

Zoning District B3

Owner: 2-14 Medford Street, LLC

Address: 455 Massachusetts Ave, Arlington, MA

Present Use/Occupancy: No. of Dwelling Units:
Retail, Service, Restaurant

Uses and their gross square feet:
2-Story 18,588 GSF

Proposed Use/Occupancy: No. of Dwelling Units:
Mixed-Use, 13 Apartment Units & Retail, Service, Restaurant

Uses and their gross square feet:
2-Story Mixed-Use, 28,373 GSF

	Present Conditions	Proposed Conditions	Min. or Max. Required by Zoning for Proposed Use
Lot Size	18,929 SF	18,929 SF	min. ----
Frontage	124.8 FT	124.8 FT	min. 50 FT
Floor Area Ratio	0.98	1.5	max. 1.5
Lot Coverage (%), where applicable	N/A	N/A	max. ----
Lot Area per Dwelling Unit (square feet)	N/A	N/A	min. ----
Front Yard Depth (feet)	0 FT	0 FT	min. 0 FT
Side Yard Width (feet) right side	-----	-----	min. -----
left side	0 FT	0 FT	min. 0 FT
Rear Yard Depth (feet)	0.6 FT	0.6 FT	min. (H+L)/6
Height	----	-----	min. -----
Stories	2-STORY	2-STORY	stories 5-STORY
Feet	<35 FT	<35 FT	feet 60 FT
Open Space (% of G.F.A.)	-----	-----	min. -----
Landscaped (square feet)	0 %	24.1 %	(s.f.) 10% 1,021 SF
Usable (square feet)	0 %	0 %	(s.f.) 20% 2,042 SF
Parking Spaces (No.)	14*	16	min. 31.9
Parking Area Setbacks (feet), where applicable	N/A	N/A	min. N/A
Loading Spaces (No.)	N/A	N/A	min. N/A
Type of Construction	NEW CONSTRUCTION		
Distance to Nearest Building	0 FT	0 FT	min.

2,457 SF/10,211SF (Res. Floor Area)

10,211 SF (Res. Floor Area) X 0.10 = 1,021 SF

10,211 SF (Res. Floor Area) X 0.20 = 2,042 SF

*8 SPACES FROM THE EXSITING 14 SPACES ARE TANDOM SPACES, BLOCKED IN BY THE OTHER 6.

Updated August 28, 2018

TOWN OF ARLINGTON
REDEVELOPMENT BOARD

Petition for Special Permit under Environmental Design Review (see Section 3.4 of the Arlington Zoning Bylaw for Applicability)

For projects subject to Environmental Design Review, (see Section 3.4), please submit a statement that completely describes your proposal, and addresses each of the following standards.

1. Preservation of Landscape. The landscape shall be preserved in its natural state, insofar as practicable, by minimizing tree and soil removal, and any grade changes shall be in keeping with the general appearance of neighboring developed areas.

There is no existing landscaping on-site and the proposed design will introduce an arborvitae row, shrubs, and Serviceberry tree along Park Terrace, See Allen & Major Associates, Inc planting schedule – trees, shrubs, groundcovers, and perennials dated August 20, 2021, Landscape Plan No. L-101 as part of the Applicant's submission. The existing street trees along Mass Ave will be protected and maintained. Also, the proposed design has been graded to closely match the existing building.

2. Relation of Buildings to Environment. Proposed development shall be related harmoniously to the terrain and to the use, scale, and architecture of existing buildings in the vicinity that have functional or visual relationship to the proposed buildings. The Arlington Redevelopment Board may require a modification in massing to reduce the effect of shadows on abutting property in an RO, R1 or R2 district or on public open space.

The proposed construction has been developed in a manner consistent with the existing building structures at the site and to the use, scale, and architecture of existing building in the vicinity of the property that have functional or visual relationship to the proposed construction. No part of the proposed structure is higher than the existing structure. Shadows from the proposed structure will not have an adverse impact on any Residential (R) use.

3. Open Space. All open space (landscaped and usable) shall be so designed as to add to the visual amenities of the vicinity by maximizing its visibility for persons passing the site or overlooking it from nearby properties. The location and configuration of usable open space shall be so designed as to encourage social interaction, maximize its utility, and facilitate maintenance.

The existing site has no open space. The Applicant proposes to add 2,457 square feet of landscaped open space. This area includes planted areas and pervious

paver systems. This area has been calculated to 24.1% of the residential floor area.

4. Circulation. With respect to vehicular, pedestrian and bicycle circulation, including entrances, ramps, walkways, drives, and parking, special attention shall be given to location and number of access points to the public streets (especially in relation to existing traffic controls and mass transit facilities), width of interior drives and access points, general interior circulation, separation of pedestrian and vehicular traffic, access to community facilities, and arrangement of vehicle parking and bicycle parking areas, including bicycle parking spaces required by Section 8.13 that are safe and convenient and, insofar as practicable, do not detract from the use and enjoyment of proposed buildings and structures and the neighboring properties.

The existing parking situation is not functional. 8 spaces from the existing 14 spaces are tandem spaces blocked in by other 6 spaces. The proposed layout provides a functional 16 spaces.

5. Surface Water Drainage. Special attention shall be given to proper site surface drainage so that removal of surface waters will not adversely affect neighboring properties or the public storm drainage system. Available Best Management Practices for the site should be employed and include site planning to minimize impervious surface and reduce clearing and re-grading. Best Management Practices may include erosion control and storm water treatment by means of swales, filters, plantings, roof gardens, native vegetation, and leaching catch basins. Storm water should be treated at least minimally on the development site; that which cannot be handled on site shall be removed from all roofs, canopies, paved and pooling areas and carried away in an underground drainage system. Surface water in all paved areas shall be collected at intervals so that it will not obstruct the flow of vehicular or pedestrian traffic and will not create puddles in the paved areas.

Surface water drainage at the site will be improved with the installation of pervious paver systems and an underground infiltration system. These systems will receive clean roof runoff for storm water recharge and greatly reduce the quantity of stormwater runoff from the parcel.

The proposed work will also result in approximately 1,008 square feet of impervious material being replaced with landscaped areas. See Allen & Major Associates, Inc. letter to the Director of Planning & Community Development dated August 20, 2021.

In accordance with Section 3.3.4, the Board may require from any applicant, after consultation with the Director of Public Works, security satisfactory to the Board to insure the maintenance of all storm water facilities such as catch basins, leaching catch basins, detention basins, swales, etc. within the site. The Board may use funds provided by such security to conduct maintenance that the applicant fails to do. The Board may

adjust in its sole discretion the amount and type of financial security such that it is satisfied that the amount is sufficient to provide for the future maintenance needs.

6. Utility Service. Electric, telephone, cable TV and other such lines and equipment shall be underground. The proposed method of sanitary sewage disposal and solid waste disposal from all buildings shall be indicated.

Utility service would be through a tie into existing utility lines at the site.

7. Advertising Features. The size, location, design, color, texture, lighting and materials of all permanent signs and outdoor advertising structures or features shall not detract from the use and enjoyment of proposed buildings and structure and the surrounding properties. Advertising features are subject to the provisions of Section 6.2 of the Zoning Bylaw.

Any proposed relief with respect to advertising signage will be initially through the Planning Department for the purpose of determining whether any signage issues can be handled administratively or whether those issues need to go before the ARB.

8. Special Features. Exposed storage areas, exposed machinery installations, service areas, truck loading areas, utility buildings and structures, and similar accessory areas and structures shall be subject to such setbacks, screen plantings or other screening methods as shall reasonably be required to prevent their being incongruous with the existing or contemplated environment and the surrounding properties.

9. Safety. With respect to personal safety, all open and enclosed spaces shall be designed to facilitate building evacuation and maximize accessibility by fire, police, and other emergency personnel and equipment. Insofar as practicable, all exterior spaces and interior public and semi-public spaces shall be so designed as to minimize the fear and probability of personal harm or injury by increasing the potential surveillance by neighboring residents and passersby of any accident or attempted criminal act.

With respect to personal safety all open and enclosed spaces have been designed to facilitate building evacuation and maximize accessibility by fire, police, and other emergency personnel and equipment. All spaces have been designed to comply with applicable codes and ordinances.

10. Heritage. With respect to Arlington's heritage, removal, or disruption of historic, traditional, or significant uses, structures, or architectural elements shall be minimized insofar as practicable, whether these exist on the site or on adjacent properties.

The Applicant understands that it will need to appear before the Arlington Historical Commission with respect to its plans as both structures i.e., the Massachusetts Avenue structure and the 4-14 Medford Street structure are part of

the Arlington Center Historic District, although only the 4-14 Medford Street structure is a contributing historic structure.

11. Microclimate. With respect to the localized climatic characteristics of a given area, any development which proposes new structures, new hard-surface ground coverage, or the installation of machinery which emits heat, vapor, or fumes, shall endeavor to minimize, insofar as practicable, any adverse impact on light, air, and water resources, or on noise and temperature levels of the immediate environment.

Adequate provision has been made for any emission of heat vapor fumes from the property so as to endeavor to minimize, insofar as practicable, any adverse impact on light, air and water resources, or noise and temperature levels of the immediate environment.

12. Sustainable Building and Site Design. Projects are encouraged to incorporate best practices related to sustainable sites, water efficiency, energy and atmosphere, materials and resources, and indoor environmental quality. Applicants must submit a current Green Building Council Leadership in Energy and Environmental Design (LEED) checklist, appropriate to the type of development, annotated with narrative description that indicates how the LEED performance objectives will be incorporated into the project. [LEED checklists can be found at http://www.usgbc.org/DisplayPage.aspx?CMSPageID=220b]

See LEED submission for the project dated August 11, 2021, as part of the Applicant's submission.

In addition, projects subject to Environmental Design Review must address and meet the following Special Permit Criteria (see Section 3.3.3 of the Zoning Bylaw):

1. The use requested is listed as a special permit in the use regulations for the applicable district or is so designated elsewhere in this Bylaw.

The use requested is listed as a Special Permit in the use regulations for the applicable district. See Section 5.5.2 (a) Districts and Uses contained within the Zoning Bylaw.

2. The requested use is essential or desirable to the public convenience or welfare.

The requested use is essential or desirable to the public convenience or welfare because a portion of the property has been vacant for a substantial period of time within Arlington Center and the proposal seeks to add a restaurant use as well as residential use to the site on mixed-use basis which comports with the provisions of the Master Plan.

3. The requested use will not create undue traffic congestion or unduly impair pedestrian safety.

The requested use will not create undue traffic congestion or unduly impair pedestrian safety as indicated within the substance of the Applicant's plans.

4. The requested use will not overload any public water, drainage or sewer system or any other municipal system to such an extent that the requested use or any developed use in the immediate area or in any other area of the Town will be unduly subjected to hazards affecting health, safety, or the general welfare.

The requested use will not overload any public water, drainage or sewer system or any municipal system.

5. Any special regulations for the use as may be provided in this Bylaw are fulfilled.

Any special regulations for the use as may be provided for in the Bylaw are fulfilled.

6. The requested use will not impair the integrity or character of the district or adjoining districts, nor be detrimental to the health, morals, or welfare.

The requested use will not impair the integrity or character of the district or adjoining districts, nor be detrimental to the health, morals, or welfare, but rather it will enhance the integrity and character of the district and adjoining districts because of the addition of a restaurant use as well as more residential use which again comports with the substance Master Plan and the design of the changes to

the existing building do not result in massing issues but rather is compatible with the physical characteristics of other buildings in the neighborhood of the property.

7. The requested use will not, by its addition to a neighborhood, cause an excess of the particular use that could be detrimental to the character of said neighborhood.

The requested use will not, by its addition to a neighborhood, cause an excess of that use which could be detrimental to the character of said neighborhood because the requested use will enhance the character of the neighborhood because of the nature and type of the construction which will fit in nicely with adjoining structures in the district.

455-457 Massachusetts Avenue

Arlington, MA

Environmental Impact Statement

The Petitioner is requesting a Special Permit for a mixed-use development project at 455-457 Massachusetts Avenue and 4-14 Medford Street.

The site is located on the corner of Medford Street and Massachusetts Avenue in a B3 zone and there is an existing curb cut to the parcel located off of Park Terrace to the rear of the parcel.

The lot size is 18,929 square feet and the property has frontage of 124.8 feet on both Massachusetts Avenue and Medford Street while zoning requires 50 feet.

The changes to the building will result in a mixed-use building containing thirteen (13) apartment units with a retail, service and restaurant component with the gross square feet of the mixed-use building containing 28,373 square feet.

The floor area ration is presently 0.98 and the proposed FAR is 1.5 and the requirement for zoning is 1.5.

There is a zero-foot front yard setback and there is zero right and left side setback.

The rear yard depth is 0.6 feet, and the proposed rear yard depth will also be 0.6 feet.

The current height of the building is a two-story building, and the proposed height would also be a two-story building while zoning in the B3 zone would allow a five-story height.

The height in feet of the existing building is 35 feet and the proposed height would also be 35 feet while zoning would allow 60 feet.

There is presently no open space at the site and the Petitioner's plans propose to add 2,457 square feet of landscaped open space including planted areas and pervious paver systems which would result in the landscaped open space having a percentage of 24.1% of the residential floor area.

There is no existing usable open space and there is no ability to create usable open space and it would be the Petitioner's position that the existing building is non-conforming with respect to the usable open space requirement contained in the zoning

bylaw and the proposed changes to the building do not increase the non-conformity to the extent that the non-conformity with respect to usable open space is extinguished.

There are presently fourteen (14) parking spaces at the site and sixteen (16) are proposed while zoning requires 30.19 parking spaces. Petitioner will be seeking zoning relief with respect to Section 6.1.5 (c) of the zoning bylaw with regard to a reduction in the parking requirement in accordance with the transportation demand management provisions of the zoning bylaw.

Waivers are being requested with respect Section 6.1.11 – Parking Standards - Petitioner is requesting that the drive aisle width requirement be reduced from 24 feet to 20 feet as well as a reduction in the amount of parking spaces required and the manner of which parking will occur on the site.

A waiver is also being requested in connection with 5.3.21 – the screening and buffering requirements contained in the bylaw as the Petitioner's proposed plans do not allow for the required buffer along the rear of the lot abutting the R1 zone.

A waiver is also being requested in connection with Section 5.3.16 – Setback Requirements – which section gives the ARB the authority to grant a special permit to adjust the required setbacks contained in the bylaw to account for specific conditions unique to the proposal.

It is Petitioner's position that the existing configuration of the lot and the plans to not change the character of the site significantly would represent conditions unique to the proposal allowing the ARB to grant relief with respect to this section of the bylaw.

Relief is also sought from the provisions of Section 5.3.19, i.e., reduced height buffer. It is the Petitioner's position that if this waiver is required, it may already be complying with its provisions as its plans will have no adverse effect on abutting uses (school, church) in the R1 district.

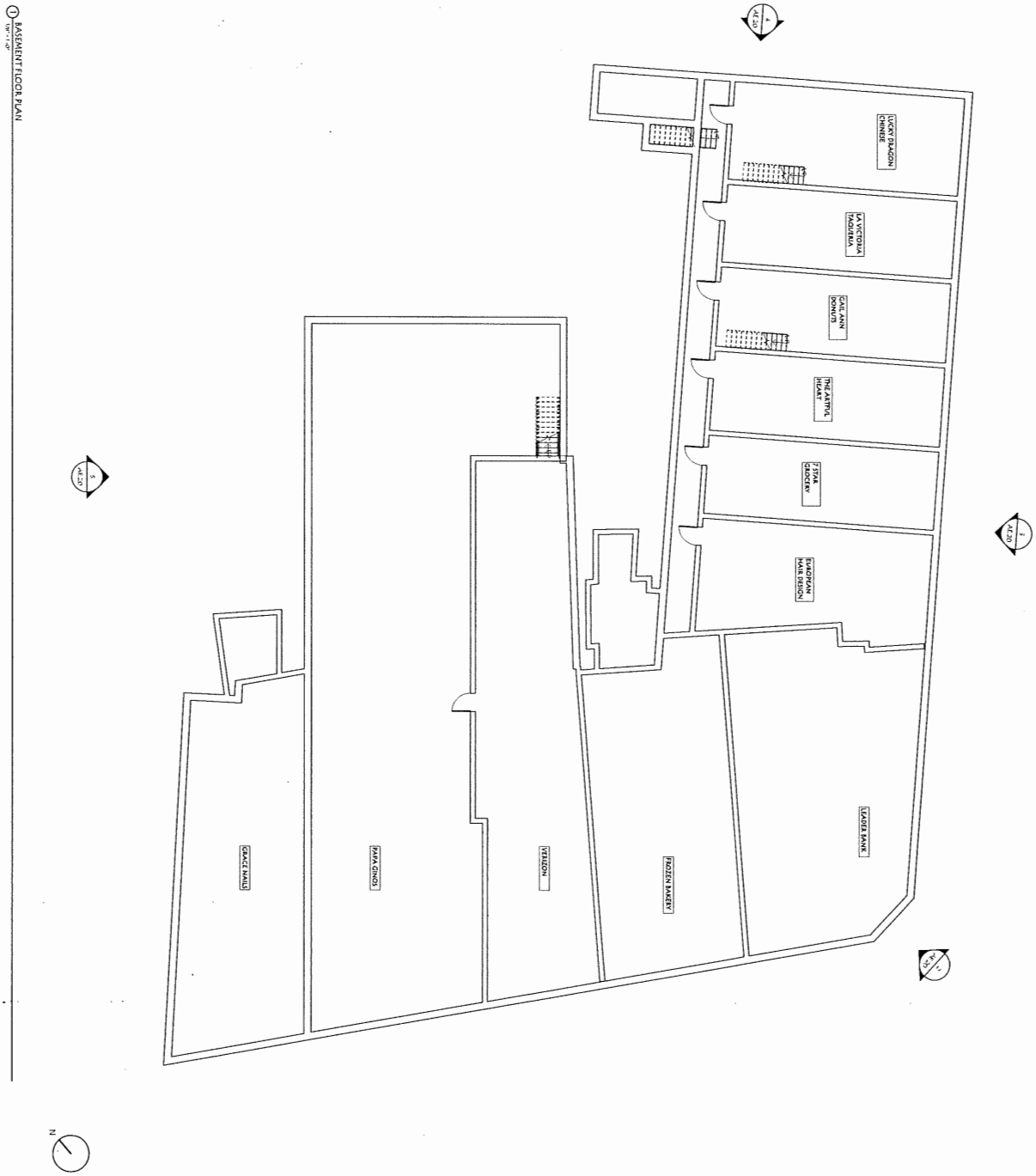
Relief is also sought from Section 3.3.8, the corner lot requirement contained in the bylaw.

Relief would also be sought, if necessary, from the provisions of Section 5.2.4, the multiple principal use section of the zoning bylaw as there will be two uses with respect to the property, i.e., a commercial use as well as a residential use.

Petitioner is providing covered bicycle parking and storage and is prepared to provide bicycle sharing on site and would be open to other means acceptable to the ARB with respect to satisfying the provisions of the transportation demand management provisions of the zoning bylaw.

It is the Petitioner's position that its proposal if approved by the ARB will result in a welcome addition to the heart of Arlington Center, large portions of which have fallen into disuse over many years as the design will add thirteen (13) residential apartments which could be a step toward enlivening retail activity in Arlington Center, an element which has been sorely missed for many years.

It is also the Petitioner's position that its planned design will have no adverse environmental impact upon the neighborhood in which the property is located but rather will result in an improvement to the neighborhood by adding a mix of residential and commercial uses at the site comporting with objectives set forth within the substance of Master Plan.



1 BASEMENT FLOOR PLAN
1/8" = 1'-0"

Title: EXISTING BASEMENT PLAN	Scale: 1/8" = 1'-0"	Revisions: # Description Date	455 MASSACHUSETTS AVENUE	NOT FOR CONSTRUCTION
	Drawn By: VLA, WCB		455 MASSACHUSETTS AVENUE ARLINGTON, MA	MARKET SQUARE ARCHITECTS 104 Congress St., STE 204 Portsmouth, NH 03801 TEL: 603 501 0202
	Checked By: Checker			
AE.00	Project No.: 202034			
	Date: 07/27/20			

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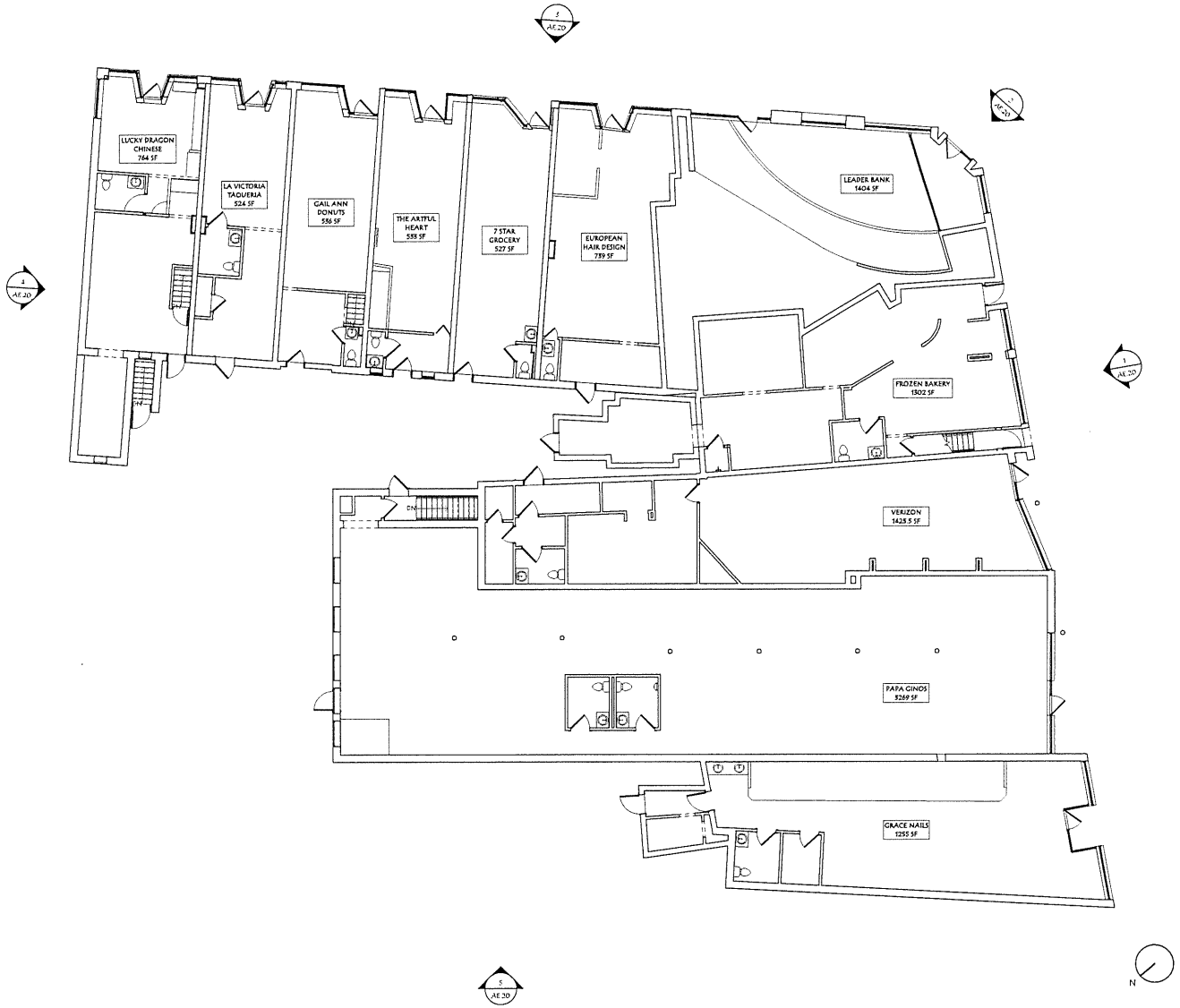


1 EXISTING FIRST FLOOR PLAN
1/8" = 1'-0"

NOT FOR CONSTRUCTION

MARC SQUARE ARCHITECTS
105 Cowgate St., STE 2-F1
Portsmouth, NH 03801
TEL: (603) 433-0001

455 MASSACHUSETTS AVENUE

455 MASSACHUSETTS AVENUE
ARLINGTON, MA

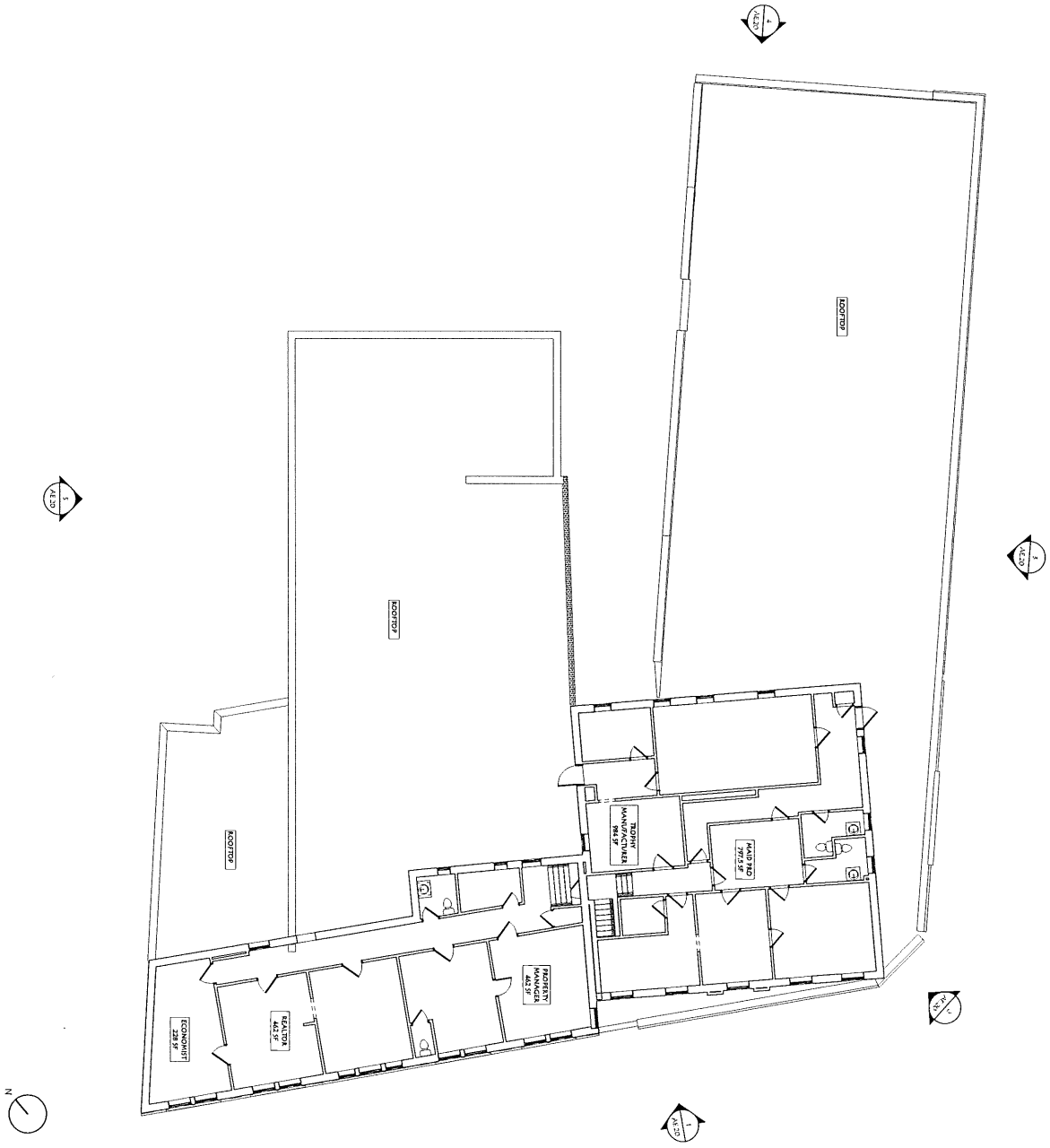
Revisions		
#	Description	Date

Scale:	1/8" = 1'-0"
Drawn By:	VLA, WCB
Checked By:	Checker
Project No.:	202034
Date:	07/27/20

Title: EXISTING FIRST FLOOR PLAN

AE.01

1 EXISTING SECOND FLOOR PLAN
1/8" = 1'-0"



Title:	Scale:	1/8" = 1'-0"	Revisions:		
EXISTING SECOND FLOOR PLAN	Drawn By:	VLA, WCB	#	Description	Date
	Checked By:	Checker			
AE.02	Project No.:	202034			
	Date:	07/27/20			

455 MASSACHUSETTS AVENUE

455 MASSACHUSETTS AVENUE
ARLINGTON, MA

NOT FOR CONSTRUCTION

MARKET SQUARE ARCHITECTS
104 Congress St., Ste 204
Portsmouth, NH 03801
PH: 603.501.0202

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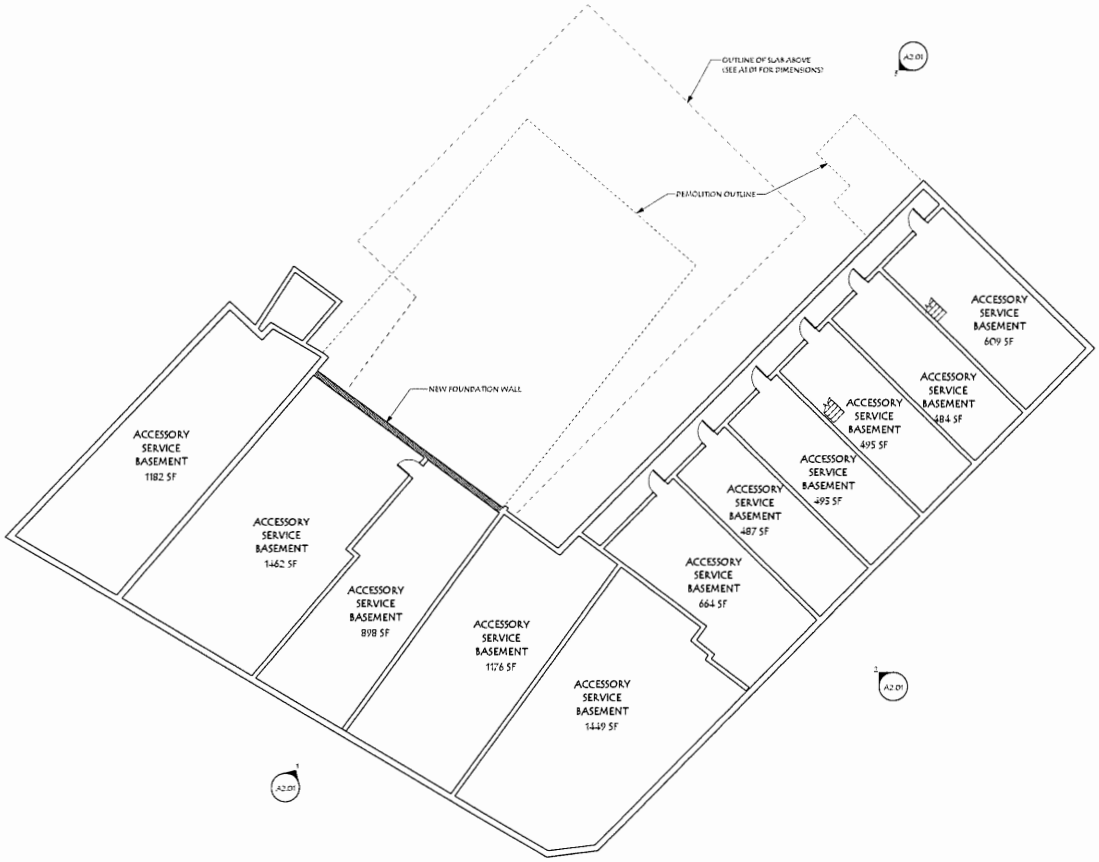

KEY:
EXISTING TO REMAIN
NEW CONSTRUCTION

1 BASEMENT
SCALE: 3/32" = 1'-0"

ARCHITECTS
101 Congress St. Ste. 201
Portsmouth, NH 03801
PH: 603.501.0202

ARLINGTON MIXED USE

455 MASS AVE
ARLINGTON, MA

NOT FOR CONSTRUCTION

Revisions:
#: Description: Date:

Title: OVERALL FOUNDATION FLOOR PLAN
Scale: As indicated
Drawn By: PPS
Checked By: ALW
Project No.: 2020-054
SCHEMATIC DESIGN
Date: 08/20/21

A1.00

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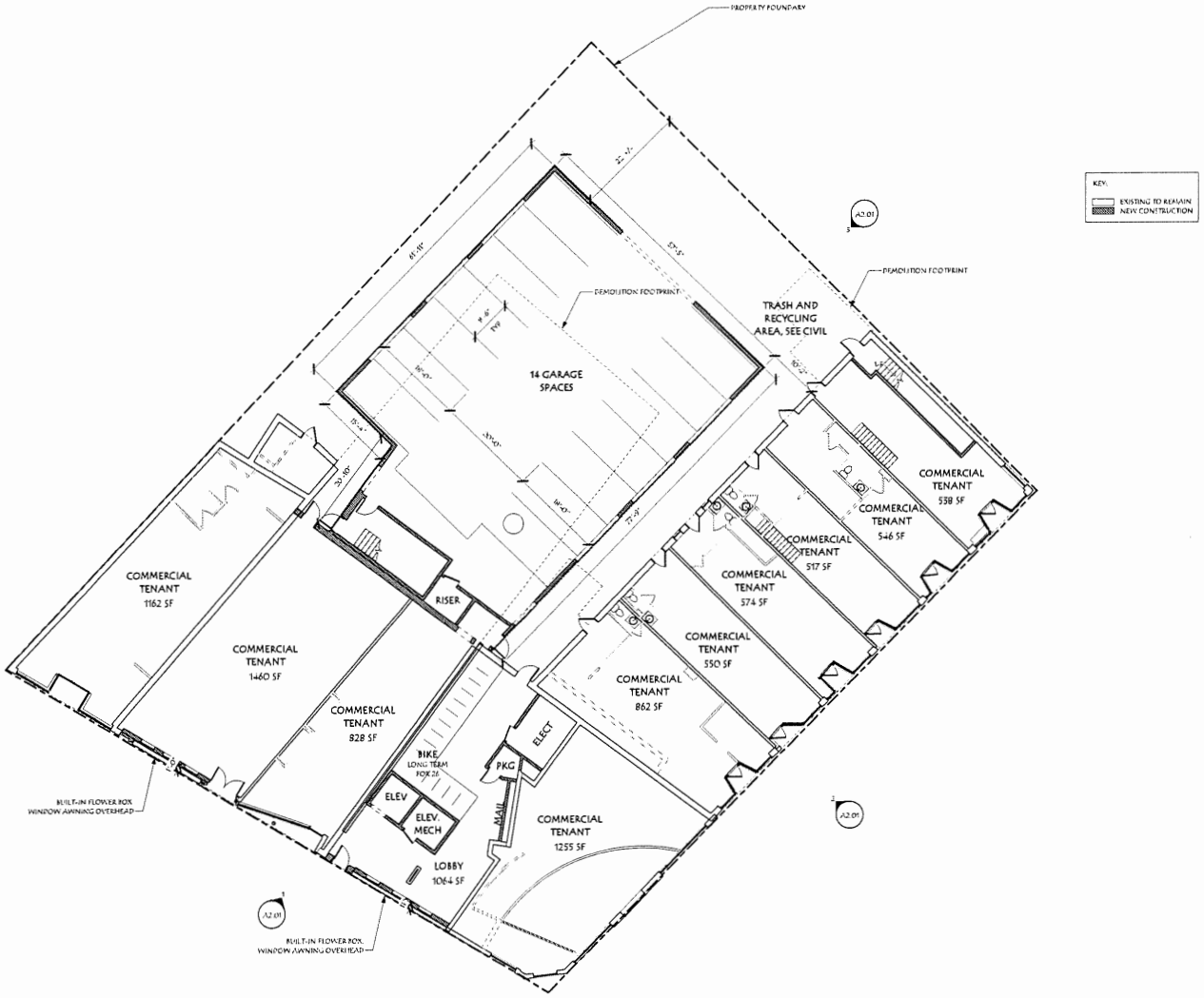


1 LEVEL 1
SCALE: 3/32" = 1'-0"

ARCHITECTS
104 Congress St., Ste. 201
Portsmouth, NH 03801
Ph: 603.501.0022

ARLINGTON MIXED USE

455 MASS AVE
ARLINGTON, MA

NOT FOR CONSTRUCTION

Revisions
No. Description Date

Scale: As indicated
Drawn By: PPS
Checked By: ALW
Project No.: 2020034
SCHEMATIC DESIGN
Date: 08/20/21

Title: OVERALL FIRST FLOOR PLAN

A1.01

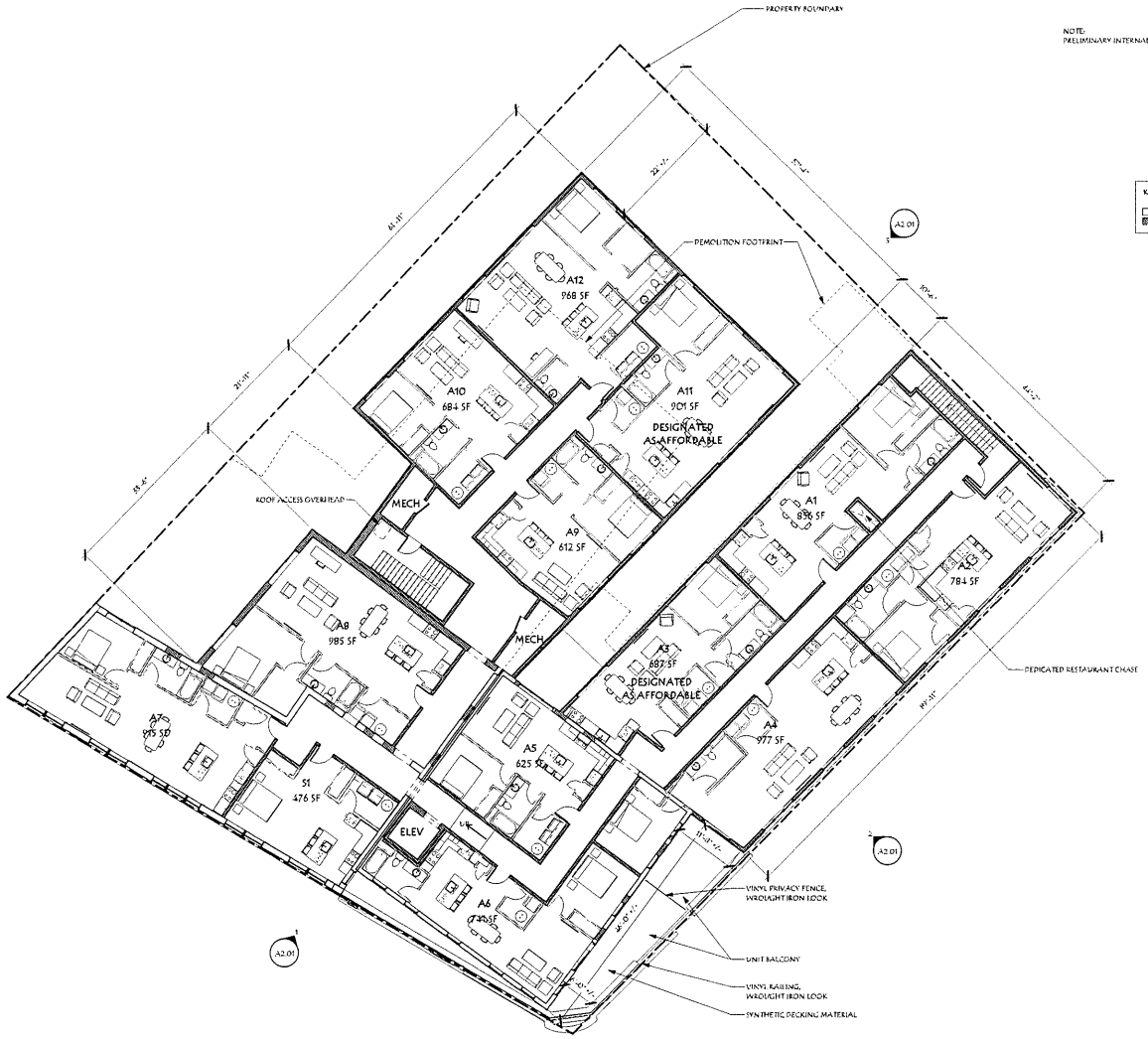
P:\20\2020\10-34\12.AI

UNIT MATRIX:

	#	%
TOTAL:		
STUDIO	1 UNIT	8%
1 BED	12 UNITS	92%
TOTAL	13 UNITS	

NOTE:
PRELIMINARY INTERNAL UNIT LAYOUT SHOWN. FINAL LAYOUT MAY VARY

KEY:
EXISTING TO REMAIN
NEW CONSTRUCTION



1 LEVEL 2
SCALE: 3/32" = 1'-0"

ARCHITECTS
1184 Congress St, STE 201
Portsmouth, NH 03801
PH: 603-501-0202

ARLINGTON MIXED USE

455 MASS AVE
ARLINGTON, MA

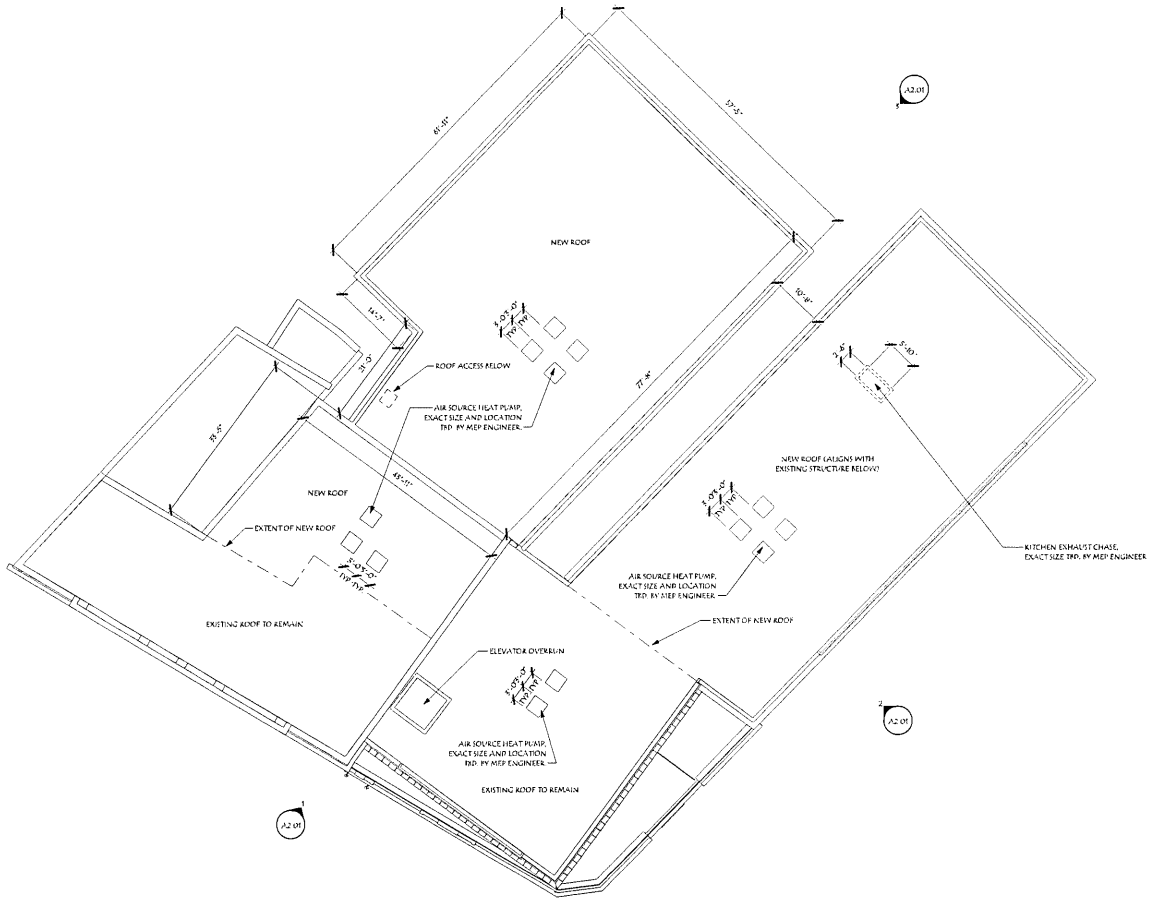
NOT FOR CONSTRUCTION

Revisions
1. Description Date

Title: OVERALL SECOND FLOOR PLAN

Scale: As indicated
Drawn By: PPS
Checked By: ALW
Project No.: 2020094
SCHEMATIC DESIGN
Date: 08/20/21

A1.02



1 ROOF
SCALE: 3/32" = 1'-0"

ARCHITECTS
104 Congress St. 978.210.1
Portsmouth, NH 03801
FAX: 603.501.0320

ARLINGTON MIXED USE

455 MASS AVE
ARLINGTON, MA

NOT FOR CONSTRUCTION

Revisions:	Date
Description	

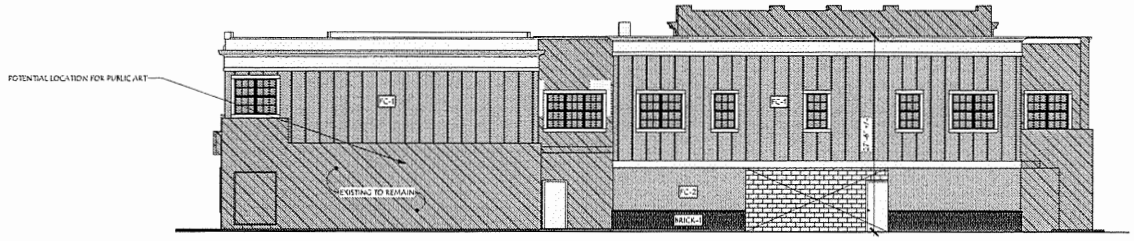
Scale: 3/32" = 1'-0"
Drawn By: PPS
Checked By: ALW
Project No.: 2020034
SCHEMATIC DESIGN
Date: 08/20/21

Title: ROOF PLAN

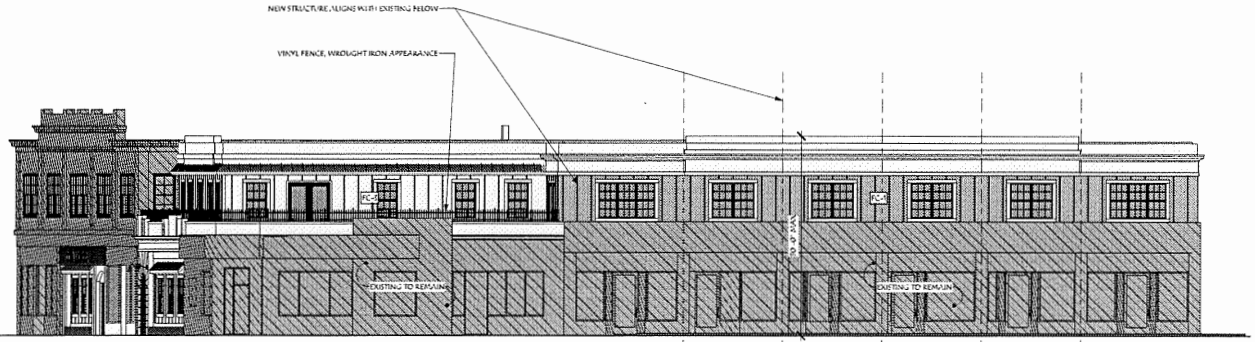
A1.03

FINISH KEY:

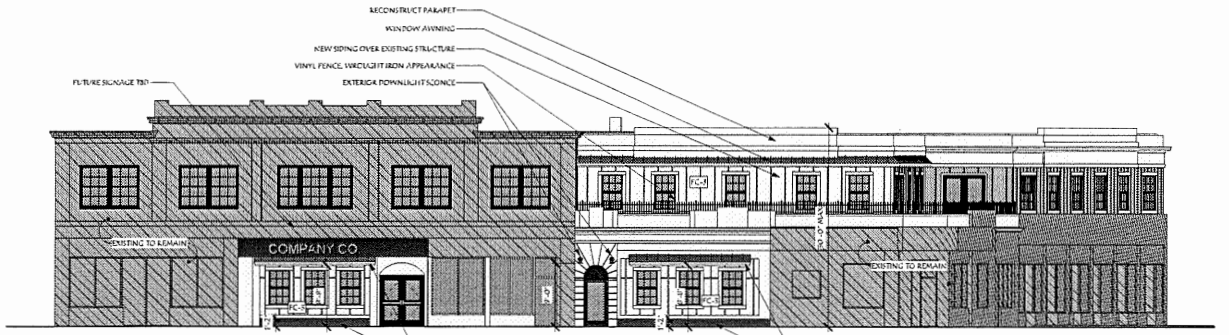
FC-1	BOARD AND BATTEN FIBER CEMENT (LT GREEN)
FC-2	CLAPBOARD FIBER CEMENT (LT GREEN)
FC-3	BOARD AND BATTEN FIBER CEMENT (WHITE)
BRICK-1	BRICK FINISH



3 EXTERIOR ELEVATION - REAR
SCALE: 1/8" = 1'-0"



2 EXTERIOR ELEVATION - MEDFORD ST
SCALE: 1/8" = 1'-0"



1 EXTERIOR ELEVATION - MASS AVE
SCALE: 1/8" = 1'-0"

ARCHITECTS
Tel: 781-319-2311
Fax: 800-501-0302

ARLINGTON MIXED USE
455 MASS AVE
ARLINGTON, MA

NOT FOR CONSTRUCTION

Revisions:
Date:

Scale: 1/8" = 1'-0"
Drawn By: PPS
Checked By: ALW
Project No.: 2020034
SCHEMATIC DESIGN
Date: 08/20/21

Title: EXTERIOR ELEVATIONS

A2.01

Title: CORNER RENDER	Scale		Revisions:	NOT FOR CONSTRUCTION	ARLINGTON MIXED USE 455 MASS AVE ARLINGTON, MA	MARKET SQUARE ARCHITECTS 104 Congress St., Ste 205 Portsmouth, NH 03801 PH: 603 501 0202
	Draw n By: PPS		Date			
	Checked By: ALW					
	Project No.: 202003-1					
	SCHEMATIC DESIGN					
A9.01	Date 08/20/21					

Title MEDFORD ST RENDER	Scale Drawn By: PPS Checked By: ALW Project No.: 202003-1 SCHEMATIC DESIGN Date: 08/20/21	Revisions No. Description Date	NOT FOR CONSTRUCTION	ARLINGTON MIXED USE 455 MASS AVE ARLINGTON, MA	MARKET SQUARE ARCHITECTS 105 Congress St. Ste 205 Portsmouth, NH 03801 Ph: 603 501 0202
A9.02					

LEED v4 for Building Design and Construction: Homes and Multifamily Lowrise

Project Checklist

Project Name: 455 Massachusetts Ave, Arlington, MA 02476
Date: 8/11/2021

Y	?	N			
Y			Credit	Integrative Process	2

14	1	0		Location and Transportation	15
Y			Prereq	Floodplain Avoidance	Required
				PERFORMANCE PATH	
			Credit	LEED for Neighborhood Development Location	15
				PRESCRIPTIVE PATH	
8			Credit	Site Selection	8
2	1		Credit	Compact Development	3
2			Credit	Community Resources	2
2			Credit	Access to Transit	2

4	0	3		Sustainable Sites	7
Y			Prereq	Construction Activity Pollution Prevention	Required
Y			Prereq	No Invasive Plants	Required
2			Credit	Heat Island Reduction	2
		3	Credit	Rainwater Management	3
2			Credit	Non-Toxic Pest Control	2

4	0	6		Water Efficiency	12
Y			Prereq	Water Metering	Required
				PERFORMANCE PATH	
			Credit	Total Water Use	12
				PRESCRIPTIVE PATH	
4		2	Credit	Indoor Water Use	6
		4	Credit	Outdoor Water Use	4

12	19	6		Energy and Atmosphere	38
Y			Prereq	Minimum Energy Performance	Required
Y			Prereq	Energy Metering	Required
Y			Prereq	Education of the Homeowner, Tenant or Building Manager	Required
				PERFORMANCE PATH	
			Credit	Annual Energy Use	29
				BOTH PATHS	
2	3		Credit	Efficient Hot Water Distribution System	5
	1	1	Credit	Advanced Utility Tracking	2
1			Credit	Active Solar Ready Design	1
	1		Credit	HVAC Start-Up Credentialing	1
				PRESCRIPTIVE PATH	
Y			Prereq	Home Size	Required
		3	Credit	Building Orientation for Passive Solar	3
2			Credit	Air Infiltration	2
		2	Credit	Envelope Insulation	2
3			Credit	Windows	3
	4		Credit	Space Heating & Cooling Equipment	4

				EA PRESCRIPTIVE PATH (continued)	
	3		Credit	Heating & Cooling Distribution Systems	3
	3		Credit	Efficient Domestic Hot Water Equipment	3
2			Credit	Lighting	2
2			Credit	High Efficiency Appliances	2
	4		Credit	Renewable Energy	4

3	5	2		Materials and Resources	10
Y			Prereq	Certified Tropical Wood	Required
Y			Prereq	Durability Management	Required
	1		Credit	Durability Management Verification	1
	4		Credit	Environmentally Preferable Products	4
3			Credit	Construction Waste Management	3
		2	Credit	Material Efficient Framing	2

8	6	2		Indoor Environmental Quality	16
Y			Prereq	Ventilation	Required
Y			Prereq	Combustion Venting	Required
Y			Prereq	Garage Pollutant Protection	Required
Y			Prereq	Radon-Resistant Construction	Required
Y			Prereq	Air FIltering	Required
Y			Prereq	Environmental Tobacco Smoke	Required
Y			Prereq	Compartmentalization	Required
1	2		Credit	Enhanced Ventilation	3
2			Credit	Contaminant Control	2
	3		Credit	Balancing of Heating and Cooling Distribution Systems	3
	1		Credit	Enhanced Compartmentalization	1
2			Credit	Enhanced Combustion Venting	2
		2	Credit	Enhanced Garage Pollutant Protection	2
3			Credit	Low Emitting Products	3

0	2	4		Innovation	6
Y			Prereq	Preliminary Rating	Required
	1	4	Credit	Innovation	5
	1		Credit	LEED AP Homes	1

0	4	0		Regional Priority	4
	1		Credit	Regional Priority: Specific Credit	1
	1		Credit	Regional Priority: Specific Credit	1
	1		Credit	Regional Priority: Specific Credit	1
	1		Credit	Regional Priority: Specific Credit	1

45	37	23	TOTALS	Possible Points:	110

Certified: 40 to 49 points, **Silver:** 50 to 59 points, **Gold:** 60 to 79 points, **Platinum:** 80 to 110

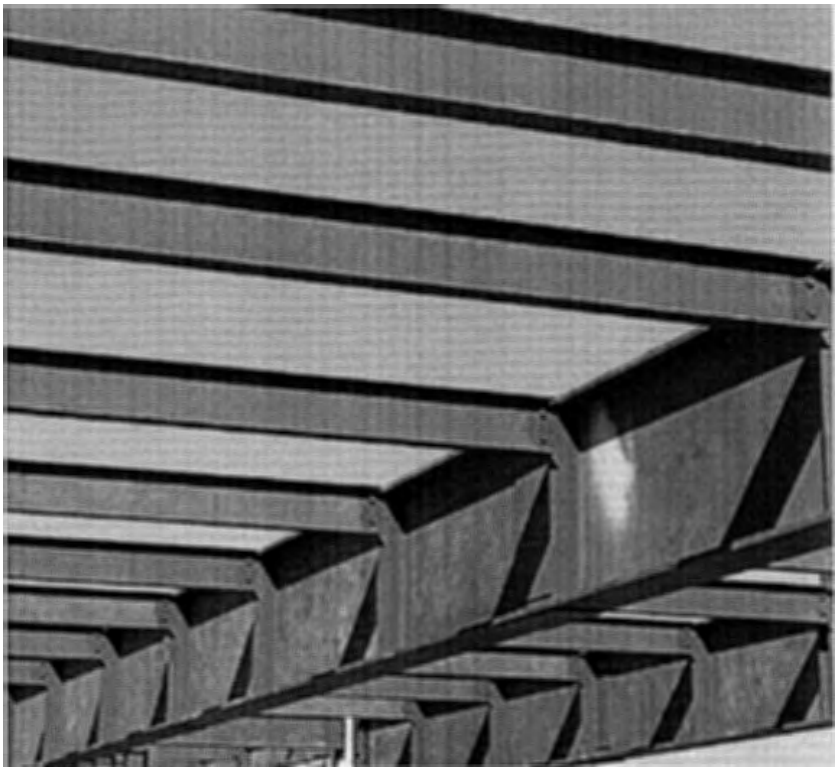
John --

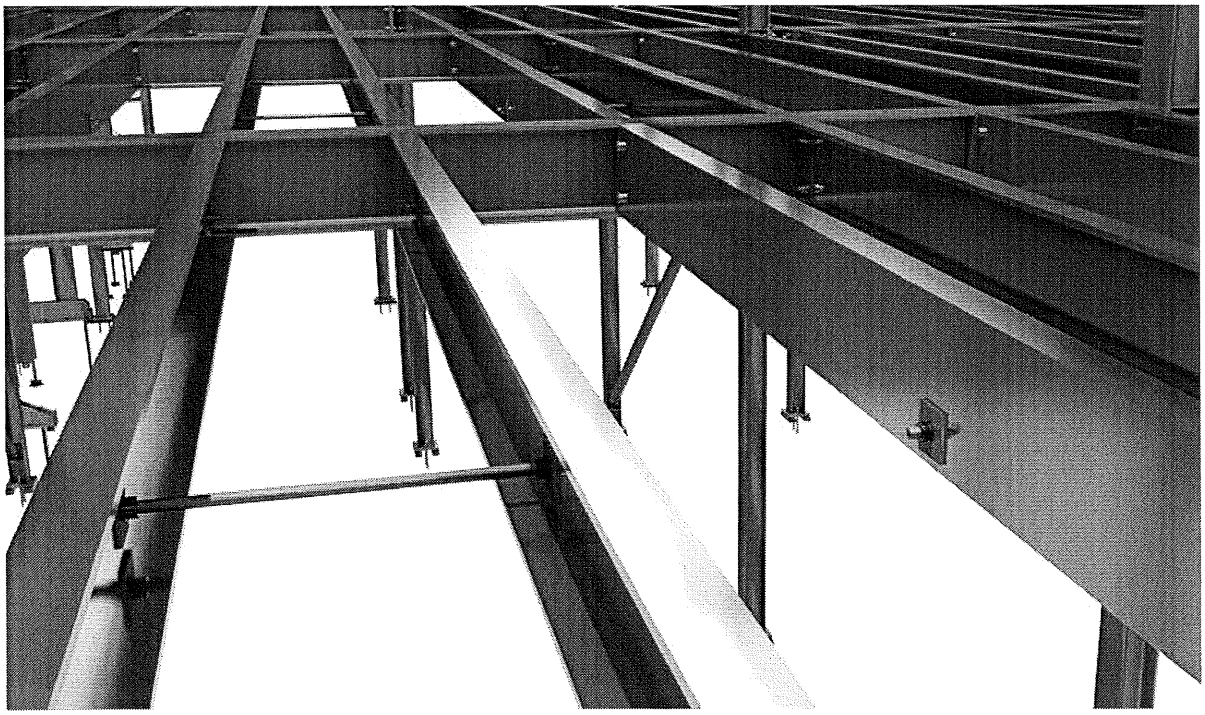
With the 455 Mass Ave. project the process here to develop the upper level residential...

- Prep existing masonry structure to receive new structural supports / beams that we would call "fly over" construction, these beams would run over the existing roof structure allowing a small interstitial space to run MEP mains, branch lines, water & sewer.
- The fly over beams ideally would have intermediate structural steel beams, smaller in size to space from one fly over beam to the next, thereby creating a grid size roughly 8 ft x 8 ft, we would install 4-inch-thick metal decking, we would install t-studs and pour a light-weight concrete slab.
- We would use the new upper-level slab as our starting point to frame in either wood or light gauge metal framing (LGMF) all new walls and roof system for the new upper residential units.
- We would extend all MEP items that run through the roof and continue these through the upper floor and through the new roof, some may be consolidated if possible.
- We would construct the new addition on a standard spread footing below frost extending up above grade with a new 12" thick foundation, this foundation will support the upper level, through a formed concrete or "cast in place" structural wall and in combination support structural steel beams to support the new second floor system, this will match the height of the new fly-over steel/concrete slab so all upper levels are on the same level.
- And same method used for the fly-over slab we would frame in either wood or light gauge metal framing (LGMF) all new walls and roof system for the new upper residential units.
- The new stair tower and the renovated lobby we are recommending to be built out of a masonry CMU (Masonry Block) walls, with proper fire ratings, assume a steel diamond plate stair system with standard railings to the new 2nd floor.
- We need to allow for demolition of the existing 2nd floor building and framing, this would all need to be removed and we can install a temporary rubber roof system installed over the existing wood framed floor system, this system is similar to the existing roof framing that would remain on the first section.
- Provisions need to be made to enhance the existing brick bearing walls to accommodate the new structural flyover construction, this will mean that on selective evenings we will need to enter the retail spaces below to reinforce the masonry wall to accept new beams

above, this can be done relatively quickly and most of the work would be prefabricated 4-inch tube steel columns installed directly to the walls and bolted or welded to the structure, drilling through the wall from the outside and running galvanized threaded rod through the masonry wall and attached to the new steel columns.

I hope this helps explain the process to build this exciting project.





Issued: August 20, 2021

Jennifer Raitt
Director of Planning & Community Development
730 Massachusetts Ave
Arlington, MA 02476

RE: Mixed-Use Redevelopment
Drainage Summary Letter
455-457 Massachusetts Ave
Arlington, MA 02476

Dear Ms. Raitt,

On behalf of our Client, 2-4 Medford Street, LLC, Allen & Major Associates (A&M) is pleased to provide this letter in support of the Special Permit application for the Mixed-Use Redevelopment project at 455-457 Massachusetts Ave. This letter will summarize the changes to the stormwater management system which are proposed as part of the redevelopment efforts.

Existing Conditions

The site is located on the corner of Medford Street and Massachusetts Avenue. There is an existing curb cut to the parcel located off Park Terrace to the rear of the parcel. The existing lot is entirely impervious with exiting building and pavement. Elevations onsite range from elevation 38 along Mass Ave to elevation 36 at the rear along Park Terrace. The majority of the stormwater on-site flows via sheet flow off-site to Park Terrace. A review of the NRCS soil report for Middlesex County indicates that the soil onsite is considered Merrimac-Urban Land which has a Hydrologic Soil Group rating of an "A". A copy of the Existing Watershed Plan is included herewith.

Proposed Conditions

The project, proposes to demolish a portion of the existing structure and construct and addition in a similar location. The project proposes to maintain existing retail, restaurant and service uses on the ground floor, and construct new apartments on the second floor. There are 16 parking stalls proposed on the first level. The stormwater management system will be greatly improved with the installation of pervious paver systems and underground infiltration system. These systems will receive clean roof runoff for stormwater recharge. These systems will greatly reduce the quantity of stormwater runoff from the parcel. The proposed work will also result in approximately 1,008 square feet of impervious material being replaced with landscaped areas.

Runoff flows were estimated for both pre and post development conditions using HydroCAD 10.00 software, at a specific "Study Point" (SP-1). Study Point 1 is the flows that will flow via sheet flow onto Park Terrace. The table below shows that the project causes a reduction in the peak rate of runoff and volume of stormwater leaving the site at the Study Point. Copies of the HydroCAD worksheets and Watershed Plans are included herewith.

STUDY POINT #1 (flow to municipal system)			
	2-Year	10-Year	100-Year
Existing Flow (CFS)	1.41	2.16	3.93
Proposed Flow (CFS)	0.08	0.13	0.50
Decrease (CFS)	**1.33**	**2.03**	**3.43**
Existing Volume (CF)	4,728	7,356	13,644
Proposed Volume (CF)	235	400	975
Decrease (CF)	**4,493**	**6,956**	**12,669**

The surface water drainage requirements of the Town of Arlington Zoning Bylaw Environmental Design Review Standards have been reviewed and met with the proposed design. The proposed project will introduce stormwater infiltration systems and landscaped areas to the site to reduce the impervious area. The Town of Arlington, Article 15 Stormwater Mitigation, shall not apply as the proposed development will introduce a reduction in impervious area. However, with the proposed infiltration systems the project will reduce the runoff rates for all design storms, and comply with this bylaw.

Summary

As shown in the table above, the proposed development will have a positive impact on the stormwater management system by reducing the rate and volume of stormwater runoff from the site.

Very truly yours,

ALLEN & MAJOR ASSOCIATES, INC.

Aaron Mackey, PE
Project Engineer

Attachments:

1. Existing Watershed Plan
2. Proposed Watershed Plan
3. Pre development HydroCAD Calculations
4. Post development HydroCAD Calculations
5. Extreme Precipitation Tables
6. NRCS Soil Report

LEGEND

EXISTING WATERSHED

SUBCATCHMENT BOUNDARY

SUBCATCHMENT LABEL

GRAPHIC SCALE

(IN FEET)
1 inch = 10 ft.

PROGRESS PRINT
ISSUED FOR REVIEW
AUGUST 11, 2021

REV	DATE	DESCRIPTION

APPLICANT\OWNER:

2-14 MEDFORD STREET, LLC
455 MASSACHUSETTS AVE, STE 1
ARLINGTON, MA 02474

PROJECT:

455-457
MASSACHUSETTS AVE
ARLINGTON, MA 02476

PROJECT NO.	2729-03	DATE:	8/11/2021
SCALE:	1" = 10'	DWG. NAME:	C2729-03
DESIGNED BY:	JG/ARM	CHECKED BY:	ARM/BDJ

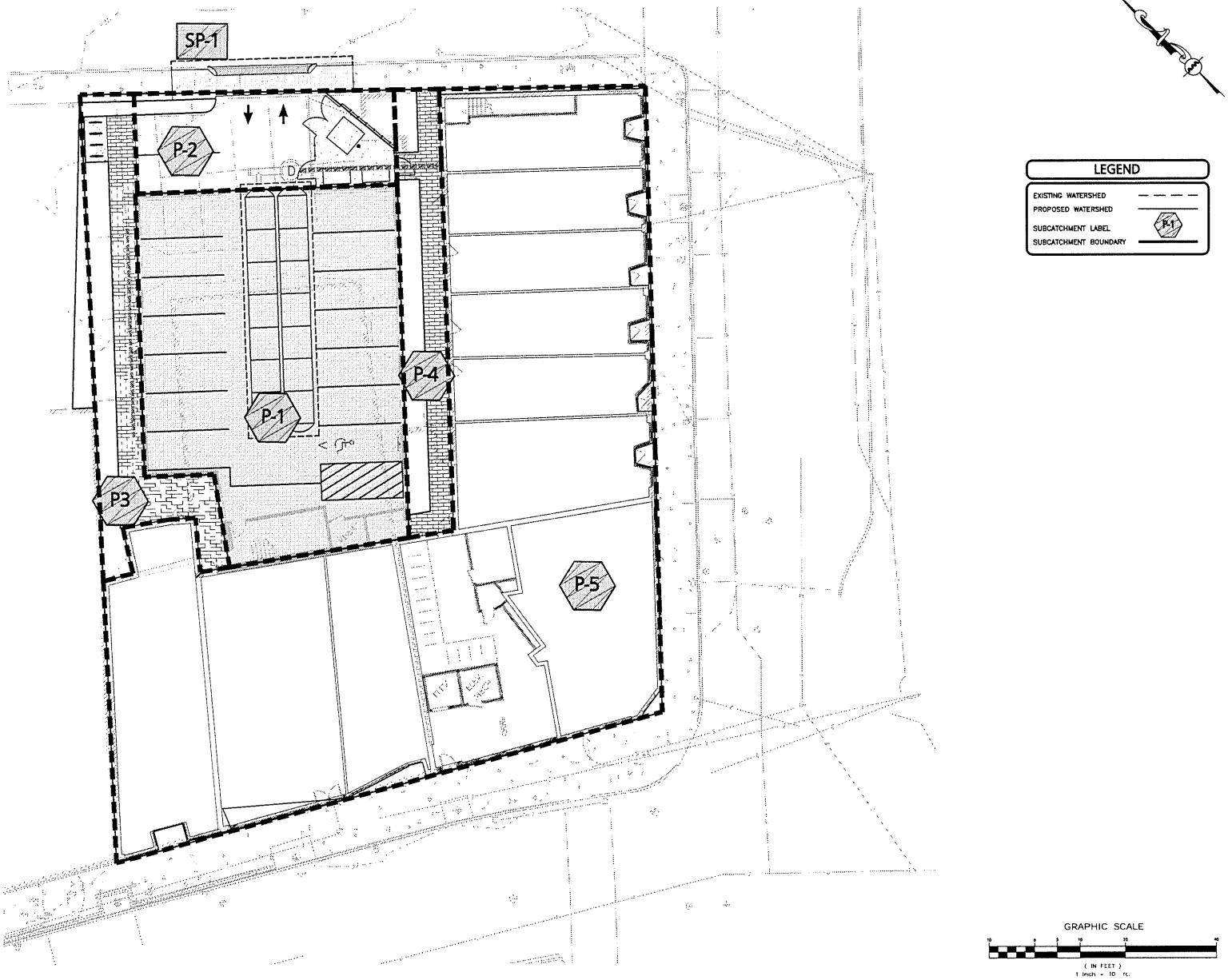
PREPARED BY:

ALLEN & MAJOR ASSOCIATES, INC.
civil engineering • land surveying
environmental consulting • landscape architecture
www.allenmajor.com
100 COMMERCE WAY, SUITE 5
WOBURN MA 01801
TEL: (781) 935-6889
FAX: (781) 935-2896

WOBURN, MA • LAKEVILLE, MA • MANCHESTER, NH

DRAWING TITLE:	SHEET No.
EXISTING WATERSHED PLAN	EWP

R:\PROJECTS\2729-03\CIVIL\DRAWINGS\CURRENT\C-2729-03_WATERSHED-EXISTING.DWG



PROGRESS PRINT
ISSUED FOR REVIEW
AUGUST 11, 2021

REV	DATE	DESCRIPTION

APPLICANT\OWNER:
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ARLINGTON, MA 02474

PROJECT:
455-457
MASSACHUSETTS AVE
ARLINGTON, MA 02476

PROJECT NO.	2729-03	DATE:	8/11/2021
SCALE:	1" = 10'	DWG. NAME:	C2729-03
DESIGNED BY:	JG/ARM	CHECKED BY:	ARM/BDJ

PREPARED BY:
ALLEN & MAJOR ASSOCIATES, INC.
civil engineering • land surveying
environmental consulting • landscape architecture
www.allenmajor.com
100 COMMERCE WAY, SUITE J
WOBURN MA 01801
TEL: (781) 935-6889
FAX: (781) 935-2896
WOBURN, MA • LAKEVILLE, MA • MANCHESTER, NH

DRAWING TITLE: PROPOSED WATERSHED PLAN

SHEET No. PWP

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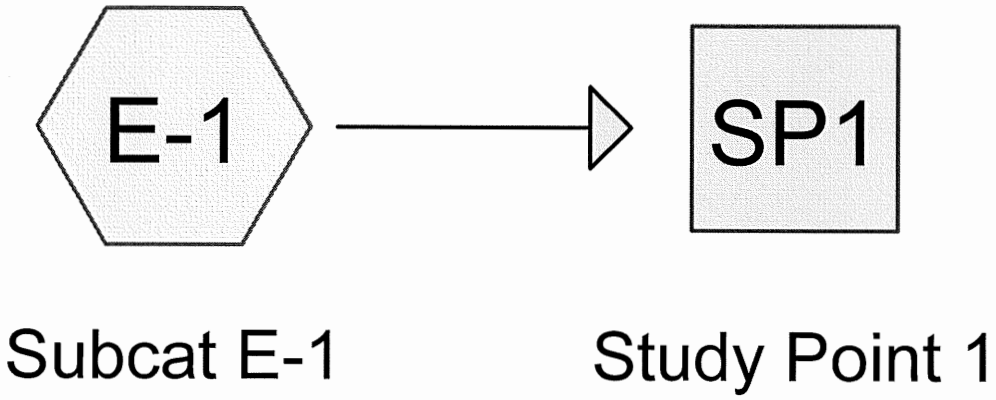
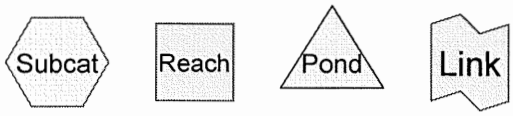




Routing Diagram for 2729-03_Existing-Conditions
Prepared by Allen & Major Associates Inc. , Printed 8/11/2021
HydroCAD® 10.00-24 s/n 02881 © 2018 HydroCAD Software Solutions LLC

Area Listing (all nodes)

Area (sq-ft)	CN	Description (subcatchment-numbers)
18,929	98	Paved parking, HSG A (E-1)
18,929	**98**	**TOTAL AREA**

Soil Listing (all nodes)

Area (sq-ft)	Soil Group	Subcatchment Numbers
18,929	HSG A	E-1
0	HSG B	
0	HSG C	
0	HSG D	
0	Other	
18,929		**TOTAL AREA**

Ground Covers (all nodes)

HSG-A (sq-ft)	HSG-B (sq-ft)	HSG-C (sq-ft)	HSG-D (sq-ft)	Other (sq-ft)	Total (sq-ft)	Ground Cover	Subcatchment Numbers
18,929	0	0	0	0	18,929	Paved parking	E-1
18,929	**0**	**0**	**0**	**0**	**18,929**	**TOTAL AREA**	

Time span=0.00-72.00 hrs, dt=0.01 hrs, 7201 points x 3
Runoff by SCS TR-20 method, UH=SCS, Weighted-CN
Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

SubcatchmentE-1: Subcat E-1 Runoff Area=18,929 sf 100.00% Impervious Runoff Depth=3.00"
Tc=5.0 min CN=98 Runoff=1.41 cfs 4,728 cf

Reach SP1: Study Point 1 Inflow=1.41 cfs 4,728 cf
Outflow=1.41 cfs 4,728 cf

Total Runoff Area = 18,929 sf Runoff Volume = 4,728 cf Average Runoff Depth = 3.00"
0.00% Pervious = 0 sf 100.00% Impervious = 18,929 sf

Summary for Subcatchment E-1: Subcat E-1

Runoff = 1.41 cfs @ 12.07 hrs, Volume= 4,728 cf, Depth= 3.00"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs
Type III 24-hr 2-Year Rainfall=3.23"

Area (sf)	CN	Description
18,929	98	Paved parking, HSG A
18,929		100.00% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					**Direct Entry, Assumed**

Summary for Reach SP1: Study Point 1

Inflow Area = 18,929 sf,100.00% Impervious, Inflow Depth = 3.00" for 2-Year event
Inflow = 1.41 cfs @ 12.07 hrs, Volume= 4,728 cf
Outflow = 1.41 cfs @ 12.07 hrs, Volume= 4,728 cf, Atten= 0%, Lag= 0.0 min

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3

Time span=0.00-72.00 hrs, dt=0.01 hrs, 7201 points x 3
Runoff by SCS TR-20 method, UH=SCS, Weighted-CN
Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

SubcatchmentE-1: Subcat E-1

Runoff Area=18,929 sf 100.00% Impervious Runoff Depth=4.66"
Tc=5.0 min CN=98 Runoff=2.16 cfs 7,356 cf

Reach SP1: Study Point 1

Inflow=2.16 cfs 7,356 cf
Outflow=2.16 cfs 7,356 cf

Total Runoff Area = 18,929 sf Runoff Volume = 7,356 cf Average Runoff Depth = 4.66"
0.00% Pervious = 0 sf 100.00% Impervious = 18,929 sf

Summary for Subcatchment E-1: Subcat E-1

Runoff = 2.16 cfs @ 12.07 hrs, Volume= 7,356 cf, Depth= 4.66"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs
Type III 24-hr 10-Year Rainfall=4.90"

Area (sf)	CN	Description
18,929	98	Paved parking, HSG A
18,929		100.00% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					**Direct Entry, Assumed**

Summary for Reach SP1: Study Point 1

Inflow Area = 18,929 sf,100.00% Impervious, Inflow Depth = 4.66" for 10-Year event
Inflow = 2.16 cfs @ 12.07 hrs, Volume= 7,356 cf
Outflow = 2.16 cfs @ 12.07 hrs, Volume= 7,356 cf, Atten= 0%, Lag= 0.0 min

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3

Time span=0.00-72.00 hrs, dt=0.01 hrs, 7201 points x 3
Runoff by SCS TR-20 method, UH=SCS, Weighted-CN
Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

SubcatchmentE-1: Subcat E-1

Runoff Area=18,929 sf 100.00% Impervious Runoff Depth=8.65"
Tc=5.0 min CN=98 Runoff=3.93 cfs 13,644 cf

Reach SP1: Study Point 1

Inflow=3.93 cfs 13,644 cf
Outflow=3.93 cfs 13,644 cf

Total Runoff Area = 18,929 sf Runoff Volume = 13,644 cf Average Runoff Depth = 8.65"
0.00% Pervious = 0 sf 100.00% Impervious = 18,929 sf

Summary for Subcatchment E-1: Subcat E-1

Runoff = 3.93 cfs @ 12.07 hrs, Volume= 13,644 cf, Depth= 8.65"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs
Type III 24-hr 100-Year Rainfall=8.89"

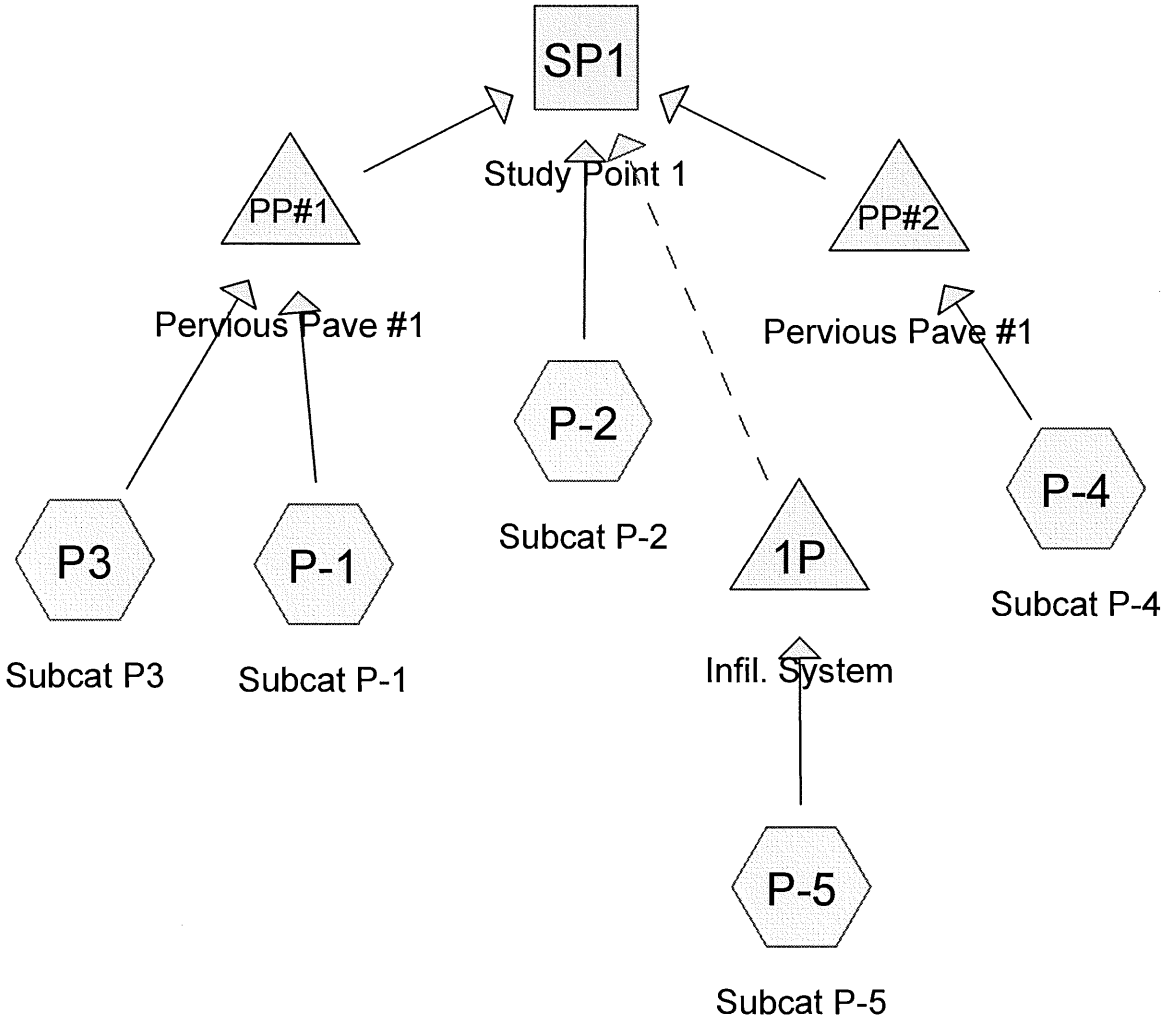
Area (sf)	CN	Description
18,929	98	Paved parking, HSG A
18,929		100.00% Impervious Area

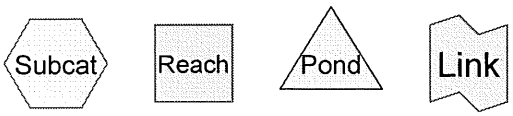
Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					**Direct Entry, Assumed**

Summary for Reach SP1: Study Point 1

Inflow Area = 18,929 sf,100.00% Impervious, Inflow Depth = 8.65" for 100-Year event
Inflow = 3.93 cfs @ 12.07 hrs, Volume= 13,644 cf
Outflow = 3.93 cfs @ 12.07 hrs, Volume= 13,644 cf, Atten= 0%, Lag= 0.0 min

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3





Routing Diagram for 2729-03_Proposed-Conditions
Prepared by Allen & Major Associates Inc. , Printed 8/11/2021
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Area Listing (all nodes)

Area (sq-ft)	CN	Description (subcatchment-numbers)
1,082	39	>75% Grass cover, Good, HSG A (P-1, P-2, P-4, P3)
2,803	98	Paved parking, HSG A (P-1, P-2, P-4, P-5, P3)
15,043	98	Roofs, HSG A (P-1, P-5)
18,929	**95**	**TOTAL AREA**

Soil Listing (all nodes)

Area (sq-ft)	Soil Group	Subcatchment Numbers
18,929	HSG A	P-1, P-2, P-4, P-5, P3
0	HSG B	
0	HSG C	
0	HSG D	
0	Other	
18,929		**TOTAL AREA**

Ground Covers (all nodes)

HSG-A (sq-ft)	HSG-B (sq-ft)	HSG-C (sq-ft)	HSG-D (sq-ft)	Other (sq-ft)	Total (sq-ft)	Ground Cover	Subcatchment Numbers
1,082	0	0	0	0	1,082	>75% Grass cover, Good	P-1, P-2, P-4, P3
2,803	0	0	0	0	2,803	Paved parking	P-1, P-2, P-4, P-5, P3
15,043	0	0	0	0	15,043	Roofs	P-1, P-5
18,929	**0**	**0**	**0**	**0**	**18,929**	**TOTAL AREA**	

Notes Listing (all nodes)

Line#	Node Number	Notes
1	1P	Exfiltration Rate = 7.0 in./hr. (Merrimac-Urban Land = 100 micrometer per second = 14.17 in./hr. / 2 = 7.0 in/hr
2	PP#1	Exfiltration Rate = 7.0 in./hr. (Merrimac-Urban Land = 100 micrometer per second = 14.17 in./hr. / 2 = 7.0 in/hr
3	PP#2	Exfiltration Rate = 7.0 in./hr. (Merrimac-Urban Land = 100 micrometer per second = 14.17 in./hr. / 2 = 7.0 in/hr

Time span=0.00-72.00 hrs, dt=0.01 hrs, 7201 points x 3
Runoff by SCS TR-20 method, UH=SCS, Weighted-CN
Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

SubcatchmentP-1: Subcat P-1 Runoff Area=4,358 sf 99.99% Impervious Runoff Depth=3.00"
Tc=5.0 min CN=98 Runoff=0.33 cfs 1,089 cf

SubcatchmentP-2: Subcat P-2 Runoff Area=1,234 sf 87.57% Impervious Runoff Depth=2.29"
Tc=5.0 min CN=91 Runoff=0.08 cfs 235 cf

SubcatchmentP-4: Subcat P-4 Runoff Area=972 sf 55.84% Impervious Runoff Depth=0.95"
Tc=0.0 min CN=72 Runoff=0.03 cfs 77 cf

SubcatchmentP-5: Subcat P-5 Runoff Area=11,040 sf 100.00% Impervious Runoff Depth=3.00"
Tc=5.0 min CN=98 Runoff=0.82 cfs 2,758 cf

SubcatchmentP3: Subcat P3 Runoff Area=1,324 sf 62.26% Impervious Runoff Depth=1.17"
Tc=5.0 min CN=76 Runoff=0.04 cfs 129 cf

Reach SP1: Study Point 1 Inflow=0.08 cfs 235 cf
Outflow=0.08 cfs 235 cf

Pond 1P: Infil. System Peak Elev=30.70' Storage=674 cf Inflow=0.82 cfs 2,758 cf
Discarded=0.14 cfs 2,759 cf Secondary=0.00 cfs 0 cf Outflow=0.14 cfs 2,759 cf

Pond PP#1: Pervious Pave #1 Peak Elev=34.30' Storage=149 cf Inflow=0.37 cfs 1,218 cf
Discarded=0.13 cfs 1,219 cf Primary=0.00 cfs 0 cf Outflow=0.13 cfs 1,219 cf

Pond PP#2: Pervious Pave #1 Peak Elev=35.30' Storage=0 cf Inflow=0.03 cfs 77 cf
Discarded=0.03 cfs 77 cf Primary=0.00 cfs 0 cf Outflow=0.03 cfs 77 cf

Total Runoff Area = 18,929 sf Runoff Volume = 4,288 cf Average Runoff Depth = 2.72"
5.72% Pervious = 1,082 sf 94.28% Impervious = 17,846 sf

Summary for Subcatchment P-1: Subcat P-1

Runoff = 0.33 cfs @ 12.07 hrs, Volume= 1,089 cf, Depth= 3.00"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs
Type III 24-hr 2-Year Rainfall=3.23"

Area (sf)	CN	Description
0	98	Paved parking, HSG A
0	39	>75% Grass cover, Good, HSG A
4,358	98	Roofs, HSG A
4,358	98	Weighted Average
0		0.01% Pervious Area
4,358		99.99% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					**Direct Entry, Assumed**

Summary for Subcatchment P-2: Subcat P-2

Runoff = 0.08 cfs @ 12.07 hrs, Volume= 235 cf, Depth= 2.29"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs
Type III 24-hr 2-Year Rainfall=3.23"

Area (sf)	CN	Description
1,081	98	Paved parking, HSG A
153	39	>75% Grass cover, Good, HSG A
1,234	91	Weighted Average
153		12.43% Pervious Area
1,081		87.57% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					**Direct Entry, Min tc**

Summary for Subcatchment P-4: Subcat P-4

Runoff = 0.03 cfs @ 12.00 hrs, Volume= 77 cf, Depth= 0.95"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs
Type III 24-hr 2-Year Rainfall=3.23"

Area (sf)	CN	Description
543	98	Paved parking, HSG A
429	39	>75% Grass cover, Good, HSG A
972	72	Weighted Average
429		44.16% Pervious Area
543		55.84% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
0.0					**Direct Entry, min tc**

Summary for Subcatchment P-5: Subcat P-5

Runoff = 0.82 cfs @ 12.07 hrs, Volume= 2,758 cf, Depth= 3.00"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs
Type III 24-hr 2-Year Rainfall=3.23"

Area (sf)	CN	Description
10,685	98	Roofs, HSG A
355	98	Paved parking, HSG A
11,040	98	Weighted Average
11,040		100.00% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					**Direct Entry, min tc**

Summary for Subcatchment P3: Subcat P3

Runoff = 0.04 cfs @ 12.08 hrs, Volume= 129 cf, Depth= 1.17"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs
Type III 24-hr 2-Year Rainfall=3.23"

Area (sf)	CN	Description
500	39	>75% Grass cover, Good, HSG A
825	98	Paved parking, HSG A
1,324	76	Weighted Average
500		37.74% Pervious Area
825		62.26% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					**Direct Entry, min tc**

Summary for Reach SP1: Study Point 1

Inflow Area = 7,889 sf, 86.28% Impervious, Inflow Depth = 0.36" for 2-Year event
Inflow = 0.08 cfs @ 12.07 hrs, Volume= 235 cf
Outflow = 0.08 cfs @ 12.07 hrs, Volume= 235 cf, Atten= 0%, Lag= 0.0 min

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3

Summary for Pond 1P: Infil. System

Inflow Area = 11,040 sf,100.00% Impervious, Inflow Depth = 3.00" for 2-Year event
Inflow = 0.82 cfs @ 12.07 hrs, Volume= 2,758 cf
Outflow = 0.14 cfs @ 11.80 hrs, Volume= 2,759 cf, Atten= 83%, Lag= 0.0 min
Discarded = 0.14 cfs @ 11.80 hrs, Volume= 2,759 cf
Secondary = 0.00 cfs @ 0.00 hrs, Volume= 0 cf

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3
Peak Elev= 30.70' @ 12.51 hrs Surf.Area= 885 sf Storage= 674 cf
Flood Elev= 37.00' Surf.Area= 885 sf Storage= 3,172 cf

Plug-Flow detention time= (not calculated: outflow precedes inflow)
Center-of-Mass det. time= 24.0 min (779.3 - 755.3)

Volume	Invert	Avail.Storage	Storage Description
#1A	29.00'	1,573 cf	**15.83'W x 55.89'L x 6.25'H Field A** 5,531 cf Overall - 1,599 cf Embedded = 3,932 cf x 40.0% Voids
#2A	30.50'	1,599 cf	**ADS_StormTech MC-3500 d +Cap**x 14 Inside #1 Effective Size= 70.4"W x 45.0"H => 15.33 sf x 7.17'L = 110.0 cf Overall Size= 77.0"W x 45.0"H x 7.50'L with 0.33' Overlap 14 Chambers in 2 Rows Cap Storage= +14.9 cf x 2 x 2 rows = 59.6 cf
		3,172 cf	Total Available Storage

Storage Group A created with Chamber Wizard

Device	Routing	Invert	Outlet Devices
#1	Discarded	29.00'	**7.000 in/hr Exfiltration over Surface area**
#2	Secondary	36.00'	**5.0' long x 0.5' breadth Broad-Crested Rectangular Weir** Head (feet) 0.20 0.40 0.60 0.80 1.00 Coef. (English) 2.80 2.92 3.08 3.30 3.32

Discarded OutFlow Max=0.14 cfs @ 11.80 hrs HW=29.09' (Free Discharge)
└**1=Exfiltration** (Exfiltration Controls 0.14 cfs)

Secondary OutFlow Max=0.00 cfs @ 0.00 hrs HW=29.00' TW=0.00' (Dynamic Tailwater)
└**2=Broad-Crested Rectangular Weir**(Controls 0.00 cfs)

Summary for Pond PP#1: Pervious Pave #1

Inflow Area =	5,683 sf,	91.20% Impervious,	Inflow Depth =	2.57"	for 2-Year event
Inflow =	0.37 cfs @	12.07 hrs, Volume=	1,218 cf		
Outflow =	0.13 cfs @	12.32 hrs, Volume=	1,219 cf,	Atten= 65%,	Lag= 14.9 min
Discarded =	0.13 cfs @	12.32 hrs, Volume=	1,219 cf		
Primary =	0.00 cfs @	0.00 hrs, Volume=	0 cf		

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3
Peak Elev= 34.30' @ 12.32 hrs Surf.Area= 753 sf Storage= 149 cf
Flood Elev= 37.00' Surf.Area= 753 sf Storage= 904 cf

Plug-Flow detention time= (not calculated: outflow precedes inflow)
Center-of-Mass det. time= 4.6 min (770.4 - 765.8)

Volume	Invert	Avail.Storage	Storage Description
#1	33.80'	904 cf	**Subbase (Conic)**Listed below (Recalc) 2,259 cf Overall x 40.0% Voids

Elevation (feet)	Surf.Area (sq-ft)	Inc.Store (cubic-feet)	Cum.Store (cubic-feet)	Wet.Area (sq-ft)
33.80	753	0	0	753
36.80	753	2,259	2,259	1,045

Device	Routing	Invert	Outlet Devices
#1	Discarded	33.80'	**7.000 in/hr Exfiltration over Wetted area**
#2	Primary	36.80'	**4.5' long x 3.0' breadth Broad-Crested Rectangular Weir** Head (feet) 0.20 0.40 0.60 0.80 1.00 1.20 1.40 1.60 1.80 2.00 2.50 3.00 3.50 4.00 4.50 Coef. (English) 2.44 2.58 2.68 2.67 2.65 2.64 2.64 2.68 2.68 2.72 2.81 2.92 2.97 3.07 3.32

Discarded OutFlow Max=0.13 cfs @ 12.32 hrs HW=34.30' (Free Discharge)
└**1=Exfiltration** (Exfiltration Controls 0.13 cfs)

Primary OutFlow Max=0.00 cfs @ 0.00 hrs HW=33.80' TW=0.00' (Dynamic Tailwater)
└**2=Broad-Crested Rectangular Weir**(Controls 0.00 cfs)

Summary for Pond PP#2: Pervious Pave #1

Inflow Area =	972 sf,	55.84% Impervious,	Inflow Depth =	0.95"	for 2-Year event
Inflow =	0.03 cfs @	12.00 hrs, Volume=	77 cf		
Outflow =	0.03 cfs @	12.00 hrs, Volume=	77 cf,	Atten= 0%,	Lag= 0.0 min
Discarded =	0.03 cfs @	12.00 hrs, Volume=	77 cf		
Primary =	0.00 cfs @	0.00 hrs, Volume=	0 cf		

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3
Peak Elev= 35.30' @ 0.00 hrs Surf.Area= 546 sf Storage= 0 cf
Flood Elev= 37.00' Surf.Area= 546 sf Storage= 328 cf

Plug-Flow detention time= (not calculated: outflow precedes inflow)
Center-of-Mass det. time= 0.0 min (862.6 - 862.6)

Volume	Invert	Avail.Storage	Storage Description
#1	35.30'	328 cf	**Subbase (Conic)**Listed below (Recalc) 819 cf Overall x 40.0% Voids

Elevation (feet)	Surf.Area (sq-ft)	Inc.Store (cubic-feet)	Cum.Store (cubic-feet)	Wet.Area (sq-ft)
35.30	546	0	0	546
36.80	546	819	819	670

Device	Routing	Invert	Outlet Devices
#1	Discarded	35.30'	**7.000 in/hr Exfiltration over Wetted area**
#2	Primary	36.80'	**4.5' long x 3.0' breadth Broad-Crested Rectangular Weir** Head (feet) 0.20 0.40 0.60 0.80 1.00 1.20 1.40 1.60 1.80 2.00 2.50 3.00 3.50 4.00 4.50 Coef. (English) 2.44 2.58 2.68 2.67 2.65 2.64 2.64 2.68 2.68 2.72 2.81 2.92 2.97 3.07 3.32

Discarded OutFlow Max=0.00 cfs @ 12.00 hrs HW=35.30' (Free Discharge)
└**1=Exfiltration** (Passes 0.00 cfs of 0.09 cfs potential flow)

Primary OutFlow Max=0.00 cfs @ 0.00 hrs HW=35.30' TW=0.00' (Dynamic Tailwater)
└**2=Broad-Crested Rectangular Weir**(Controls 0.00 cfs)

Time span=0.00-72.00 hrs, dt=0.01 hrs, 7201 points x 3
Runoff by SCS TR-20 method, UH=SCS, Weighted-CN
Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

SubcatchmentP-1: Subcat P-1 Runoff Area=4,358 sf 99.99% Impervious Runoff Depth=4.66"
Tc=5.0 min CN=98 Runoff=0.50 cfs 1,694 cf

SubcatchmentP-2: Subcat P-2 Runoff Area=1,234 sf 87.57% Impervious Runoff Depth=3.89"
Tc=5.0 min CN=91 Runoff=0.13 cfs 400 cf

SubcatchmentP-4: Subcat P-4 Runoff Area=972 sf 55.84% Impervious Runoff Depth=2.12"
Tc=0.0 min CN=72 Runoff=0.07 cfs 172 cf

SubcatchmentP-5: Subcat P-5 Runoff Area=11,040 sf 100.00% Impervious Runoff Depth=4.66"
Tc=5.0 min CN=98 Runoff=1.26 cfs 4,290 cf

SubcatchmentP3: Subcat P3 Runoff Area=1,324 sf 62.26% Impervious Runoff Depth=2.45"
Tc=5.0 min CN=76 Runoff=0.09 cfs 271 cf

Reach SP1: Study Point 1 Inflow=0.13 cfs 400 cf
Outflow=0.13 cfs 400 cf

Pond 1P: Infil. System Peak Elev=31.58' Storage=1,292 cf Inflow=1.26 cfs 4,290 cf
Discarded=0.14 cfs 4,291 cf Secondary=0.00 cfs 0 cf Outflow=0.14 cfs 4,291 cf

Pond PP#1: Pervious Pave #1 Peak Elev=35.10' Storage=391 cf Inflow=0.59 cfs 1,964 cf
Discarded=0.14 cfs 1,966 cf Primary=0.00 cfs 0 cf Outflow=0.14 cfs 1,966 cf

Pond PP#2: Pervious Pave #1 Peak Elev=35.30' Storage=0 cf Inflow=0.07 cfs 172 cf
Discarded=0.07 cfs 172 cf Primary=0.00 cfs 0 cf Outflow=0.07 cfs 172 cf

Total Runoff Area = 18,929 sf Runoff Volume = 6,826 cf Average Runoff Depth = 4.33"
5.72% Pervious = 1,082 sf 94.28% Impervious = 17,846 sf

Summary for Subcatchment P-1: Subcat P-1

Runoff = 0.50 cfs @ 12.07 hrs, Volume= 1,694 cf, Depth= 4.66"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs
Type III 24-hr 10-Year Rainfall=4.90"

Area (sf)	CN	Description
0	98	Paved parking, HSG A
0	39	>75% Grass cover, Good, HSG A
4,358	98	Roofs, HSG A
4,358	98	Weighted Average
0		0.01% Pervious Area
4,358		99.99% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					**Direct Entry, Assumed**

Summary for Subcatchment P-2: Subcat P-2

Runoff = 0.13 cfs @ 12.07 hrs, Volume= 400 cf, Depth= 3.89"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs
Type III 24-hr 10-Year Rainfall=4.90"

Area (sf)	CN	Description
1,081	98	Paved parking, HSG A
153	39	>75% Grass cover, Good, HSG A
1,234	91	Weighted Average
153		12.43% Pervious Area
1,081		87.57% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					**Direct Entry, Min tc**

Summary for Subcatchment P-4: Subcat P-4

Runoff = 0.07 cfs @ 12.00 hrs, Volume= 172 cf, Depth= 2.12"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs
Type III 24-hr 10-Year Rainfall=4.90"

Area (sf)	CN	Description
543	98	Paved parking, HSG A
429	39	>75% Grass cover, Good, HSG A
972	72	Weighted Average
429		44.16% Pervious Area
543		55.84% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
0.0					**Direct Entry, min tc**

Summary for Subcatchment P-5: Subcat P-5

Runoff = 1.26 cfs @ 12.07 hrs, Volume= 4,290 cf, Depth= 4.66"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs
Type III 24-hr 10-Year Rainfall=4.90"

Area (sf)	CN	Description
10,685	98	Roofs, HSG A
355	98	Paved parking, HSG A
11,040	98	Weighted Average
11,040		100.00% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					**Direct Entry, min tc**

Summary for Subcatchment P3: Subcat P3

Runoff = 0.09 cfs @ 12.08 hrs, Volume= 271 cf, Depth= 2.45"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs
Type III 24-hr 10-Year Rainfall=4.90"

Area (sf)	CN	Description
500	39	>75% Grass cover, Good, HSG A
825	98	Paved parking, HSG A
1,324	76	Weighted Average
500		37.74% Pervious Area
825		62.26% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					**Direct Entry, min tc**

Summary for Reach SP1: Study Point 1

Inflow Area = 7,889 sf, 86.28% Impervious, Inflow Depth = 0.61" for 10-Year event
Inflow = 0.13 cfs @ 12.07 hrs, Volume= 400 cf
Outflow = 0.13 cfs @ 12.07 hrs, Volume= 400 cf, Atten= 0%, Lag= 0.0 min

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3

Summary for Pond 1P: Infil. System

Inflow Area = 11,040 sf,100.00% Impervious, Inflow Depth = 4.66" for 10-Year event
Inflow = 1.26 cfs @ 12.07 hrs, Volume= 4,290 cf
Outflow = 0.14 cfs @ 11.69 hrs, Volume= 4,291 cf, Atten= 89%, Lag= 0.0 min
Discarded = 0.14 cfs @ 11.69 hrs, Volume= 4,291 cf
Secondary = 0.00 cfs @ 0.00 hrs, Volume= 0 cf

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3
Peak Elev= 31.58' @ 12.64 hrs Surf.Area= 885 sf Storage= 1,292 cf
Flood Elev= 37.00' Surf.Area= 885 sf Storage= 3,172 cf

Plug-Flow detention time= (not calculated: outflow precedes inflow)
Center-of-Mass det. time= 54.6 min (802.0 - 747.4)

Volume	Invert	Avail.Storage	Storage Description
#1A	29.00'	1,573 cf	**15.83'W x 55.89'L x 6.25'H Field A** 5,531 cf Overall - 1,599 cf Embedded = 3,932 cf x 40.0% Voids
#2A	30.50'	1,599 cf	**ADS_StormTech MC-3500 d +Cap** x 14 Inside #1 Effective Size= 70.4"W x 45.0"H => 15.33 sf x 7.17'L = 110.0 cf Overall Size= 77.0"W x 45.0"H x 7.50'L with 0.33' Overlap 14 Chambers in 2 Rows Cap Storage= +14.9 cf x 2 x 2 rows = 59.6 cf
		3,172 cf	Total Available Storage

Storage Group A created with Chamber Wizard

Device	Routing	Invert	Outlet Devices
#1	Discarded	29.00'	**7.000 in/hr Exfiltration over Surface area**
#2	Secondary	36.00'	**5.0' long x 0.5' breadth Broad-Crested Rectangular Weir** Head (feet) 0.20 0.40 0.60 0.80 1.00 Coef. (English) 2.80 2.92 3.08 3.30 3.32

Discarded OutFlow Max=0.14 cfs @ 11.69 hrs HW=29.08' (Free Discharge)
└**1=Exfiltration** (Exfiltration Controls 0.14 cfs)

Secondary OutFlow Max=0.00 cfs @ 0.00 hrs HW=29.00' TW=0.00' (Dynamic Tailwater)
└**2=Broad-Crested Rectangular Weir**(Controls 0.00 cfs)

Summary for Pond PP#1: Pervious Pave #1

Inflow Area = 5,683 sf, 91.20% Impervious, Inflow Depth = 4.15" for 10-Year event
Inflow = 0.59 cfs @ 12.07 hrs, Volume= 1,964 cf
Outflow = 0.14 cfs @ 12.44 hrs, Volume= 1,966 cf, Atten= 76%, Lag= 22.3 min
Discarded = 0.14 cfs @ 12.44 hrs, Volume= 1,966 cf
Primary = 0.00 cfs @ 0.00 hrs, Volume= 0 cf

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3
Peak Elev= 35.10' @ 12.44 hrs Surf.Area= 753 sf Storage= 391 cf
Flood Elev= 37.00' Surf.Area= 753 sf Storage= 904 cf

Plug-Flow detention time= (not calculated: outflow precedes inflow)
Center-of-Mass det. time= 13.3 min (772.4 - 759.1)

Volume	Invert	Avail.Storage	Storage Description
#1	33.80'	904 cf	**Subbase (Conic)**Listed below (Recalc) 2,259 cf Overall x 40.0% Voids

Elevation (feet)	Surf.Area (sq-ft)	Inc.Store (cubic-feet)	Cum.Store (cubic-feet)	Wet.Area (sq-ft)
33.80	753	0	0	753
36.80	753	2,259	2,259	1,045

Device	Routing	Invert	Outlet Devices
#1	Discarded	33.80'	**7.000 in/hr Exfiltration over Wetted area**
#2	Primary	36.80'	**4.5' long x 3.0' breadth Broad-Crested Rectangular Weir** Head (feet) 0.20 0.40 0.60 0.80 1.00 1.20 1.40 1.60 1.80 2.00 2.50 3.00 3.50 4.00 4.50 Coef. (English) 2.44 2.58 2.68 2.67 2.65 2.64 2.64 2.68 2.68 2.72 2.81 2.92 2.97 3.07 3.32

Discarded OutFlow Max=0.14 cfs @ 12.44 hrs HW=35.10' (Free Discharge)
└**1=Exfiltration** (Exfiltration Controls 0.14 cfs)

Primary OutFlow Max=0.00 cfs @ 0.00 hrs HW=33.80' TW=0.00' (Dynamic Tailwater)
└**2=Broad-Crested Rectangular Weir**(Controls 0.00 cfs)

Summary for Pond PP#2: Pervious Pave #1

Inflow Area = 972 sf, 55.84% Impervious, Inflow Depth = 2.12" for 10-Year event
Inflow = 0.07 cfs @ 12.00 hrs, Volume= 172 cf
Outflow = 0.07 cfs @ 12.00 hrs, Volume= 172 cf, Atten= 0%, Lag= 0.0 min
Discarded = 0.07 cfs @ 12.00 hrs, Volume= 172 cf
Primary = 0.00 cfs @ 0.00 hrs, Volume= 0 cf

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3
Peak Elev= 35.30' @ 12.00 hrs Surf.Area= 546 sf Storage= 0 cf
Flood Elev= 37.00' Surf.Area= 546 sf Storage= 328 cf

Plug-Flow detention time= (not calculated: outflow precedes inflow)
Center-of-Mass det. time= 0.0 min (838.0 - 838.0)

Volume	Invert	Avail.Storage	Storage Description
#1	35.30'	328 cf	**Subbase (Conic)** Listed below (Recalc) 819 cf Overall x 40.0% Voids

Elevation (feet)	Surf.Area (sq-ft)	Inc.Store (cubic-feet)	Cum.Store (cubic-feet)	Wet.Area (sq-ft)
35.30	546	0	0	546
36.80	546	819	819	670

Device	Routing	Invert	Outlet Devices
#1	Discarded	35.30'	**7.000 in/hr Exfiltration over Wetted area**
#2	Primary	36.80'	**4.5' long x 3.0' breadth Broad-Crested Rectangular Weir** Head (feet) 0.20 0.40 0.60 0.80 1.00 1.20 1.40 1.60 1.80 2.00 2.50 3.00 3.50 4.00 4.50 Coef. (English) 2.44 2.58 2.68 2.67 2.65 2.64 2.64 2.68 2.68 2.72 2.81 2.92 2.97 3.07 3.32

Discarded OutFlow Max=0.09 cfs @ 12.00 hrs HW=35.30' (Free Discharge)
↑—**1=Exfiltration** (Exfiltration Controls 0.09 cfs)

Primary OutFlow Max=0.00 cfs @ 0.00 hrs HW=35.30' TW=0.00' (Dynamic Tailwater)
↑—**2=Broad-Crested Rectangular Weir**(Controls 0.00 cfs)

Time span=0.00-72.00 hrs, dt=0.01 hrs, 7201 points x 3
Runoff by SCS TR-20 method, UH=SCS, Weighted-CN
Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

SubcatchmentP-1: Subcat P-1 Runoff Area=4,358 sf 99.99% Impervious Runoff Depth=8.65"
Tc=5.0 min CN=98 Runoff=0.91 cfs 3,142 cf

SubcatchmentP-2: Subcat P-2 Runoff Area=1,234 sf 87.57% Impervious Runoff Depth=7.80"
Tc=5.0 min CN=91 Runoff=0.25 cfs 803 cf

SubcatchmentP-4: Subcat P-4 Runoff Area=972 sf 55.84% Impervious Runoff Depth=5.48"
Tc=0.0 min CN=72 Runoff=0.18 cfs 444 cf

SubcatchmentP-5: Subcat P-5 Runoff Area=11,040 sf 100.00% Impervious Runoff Depth=8.65"
Tc=5.0 min CN=98 Runoff=2.29 cfs 7,958 cf

SubcatchmentP3: Subcat P3 Runoff Area=1,324 sf 62.26% Impervious Runoff Depth=5.97"
Tc=5.0 min CN=76 Runoff=0.22 cfs 659 cf

Reach SP1: Study Point 1 Inflow=0.50 cfs 975 cf
Outflow=0.50 cfs 975 cf

Pond 1P: Infil. System Peak Elev=34.98' Storage=3,078 cf Inflow=2.29 cfs 7,958 cf
Discarded=0.14 cfs 7,958 cf Secondary=0.00 cfs 0 cf Outflow=0.14 cfs 7,958 cf

Pond PP#1: Pervious Pave #1 Peak Elev=36.91' Storage=904 cf Inflow=1.12 cfs 3,801 cf
Discarded=0.17 cfs 3,629 cf Primary=0.40 cfs 172 cf Outflow=0.57 cfs 3,801 cf

Pond PP#2: Pervious Pave #1 Peak Elev=35.42' Storage=26 cf Inflow=0.18 cfs 444 cf
Discarded=0.09 cfs 444 cf Primary=0.00 cfs 0 cf Outflow=0.09 cfs 444 cf

Total Runoff Area = 18,929 sf Runoff Volume = 13,005 cf Average Runoff Depth = 8.24"
5.72% Pervious = 1,082 sf 94.28% Impervious = 17,846 sf

Summary for Subcatchment P-1: Subcat P-1

Runoff = 0.91 cfs @ 12.07 hrs, Volume= 3,142 cf, Depth= 8.65"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs
Type III 24-hr 100-Year Rainfall=8.89"

Area (sf)	CN	Description
0	98	Paved parking, HSG A
0	39	>75% Grass cover, Good, HSG A
4,358	98	Roofs, HSG A
4,358	98	Weighted Average
0		0.01% Pervious Area
4,358		99.99% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					**Direct Entry, Assumed**

Summary for Subcatchment P-2: Subcat P-2

Runoff = 0.25 cfs @ 12.07 hrs, Volume= 803 cf, Depth= 7.80"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs
Type III 24-hr 100-Year Rainfall=8.89"

Area (sf)	CN	Description
1,081	98	Paved parking, HSG A
153	39	>75% Grass cover, Good, HSG A
1,234	91	Weighted Average
153		12.43% Pervious Area
1,081		87.57% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					**Direct Entry, Min tc**

Summary for Subcatchment P-4: Subcat P-4

Runoff = 0.18 cfs @ 12.00 hrs, Volume= 444 cf, Depth= 5.48"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs
Type III 24-hr 100-Year Rainfall=8.89"

Area (sf)	CN	Description
543	98	Paved parking, HSG A
429	39	>75% Grass cover, Good, HSG A
972	72	Weighted Average
429		44.16% Pervious Area
543		55.84% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
0.0					**Direct Entry, min tc**

Summary for Subcatchment P-5: Subcat P-5

Runoff = 2.29 cfs @ 12.07 hrs, Volume= 7,958 cf, Depth= 8.65"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs
Type III 24-hr 100-Year Rainfall=8.89"

Area (sf)	CN	Description
10,685	98	Roofs, HSG A
355	98	Paved parking, HSG A
11,040	98	Weighted Average
11,040		100.00% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					**Direct Entry, min tc**

Summary for Subcatchment P3: Subcat P3

Runoff = 0.22 cfs @ 12.07 hrs, Volume= 659 cf, Depth= 5.97"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs
Type III 24-hr 100-Year Rainfall=8.89"

Area (sf)	CN	Description
500	39	>75% Grass cover, Good, HSG A
825	98	Paved parking, HSG A
1,324	76	Weighted Average
500		37.74% Pervious Area
825		62.26% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					**Direct Entry, min tc**

Summary for Reach SP1: Study Point 1

Inflow Area = 7,889 sf, 86.28% Impervious, Inflow Depth = 1.48" for 100-Year event
Inflow = 0.50 cfs @ 12.24 hrs, Volume= 975 cf
Outflow = 0.50 cfs @ 12.24 hrs, Volume= 975 cf, Atten= 0%, Lag= 0.0 min

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3

Summary for Pond 1P: Infil. System

Inflow Area = 11,040 sf,100.00% Impervious, Inflow Depth = 8.65" for 100-Year event
Inflow = 2.29 cfs @ 12.07 hrs, Volume= 7,958 cf
Outflow = 0.14 cfs @ 11.19 hrs, Volume= 7,958 cf, Atten= 94%, Lag= 0.0 min
Discarded = 0.14 cfs @ 11.19 hrs, Volume= 7,958 cf
Secondary = 0.00 cfs @ 0.00 hrs, Volume= 0 cf

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3
Peak Elev= 34.98' @ 13.49 hrs Surf.Area= 885 sf Storage= 3,078 cf
Flood Elev= 37.00' Surf.Area= 885 sf Storage= 3,172 cf

Plug-Flow detention time= (not calculated: outflow precedes inflow)
Center-of-Mass det. time= 159.1 min (898.1 - 739.0)

Volume	Invert	Avail.Storage	Storage Description
#1A	29.00'	1,573 cf	**15.83'W x 55.89'L x 6.25'H Field A** 5,531 cf Overall - 1,599 cf Embedded = 3,932 cf x 40.0% Voids
#2A	30.50'	1,599 cf	**ADS_StormTech MC-3500 d +Cap** x 14 Inside #1 Effective Size= 70.4"W x 45.0"H => 15.33 sf x 7.17'L = 110.0 cf Overall Size= 77.0"W x 45.0"H x 7.50'L with 0.33' Overlap 14 Chambers in 2 Rows Cap Storage= +14.9 cf x 2 x 2 rows = 59.6 cf
		3,172 cf	Total Available Storage

Storage Group A created with Chamber Wizard

Device	Routing	Invert	Outlet Devices
#1	Discarded	29.00'	**7.000 in/hr Exfiltration over Surface area**
#2	Secondary	36.00'	**5.0' long x 0.5' breadth Broad-Crested Rectangular Weir** Head (feet) 0.20 0.40 0.60 0.80 1.00 Coef. (English) 2.80 2.92 3.08 3.30 3.32

Discarded OutFlow Max=0.14 cfs @ 11.19 hrs HW=29.08' (Free Discharge)
└**1=Exfiltration** (Exfiltration Controls 0.14 cfs)

Secondary OutFlow Max=0.00 cfs @ 0.00 hrs HW=29.00' TW=0.00' (Dynamic Tailwater)
└**2=Broad-Crested Rectangular Weir**(Controls 0.00 cfs)

Summary for Pond PP#1: Pervious Pave #1

Inflow Area = 5,683 sf, 91.20% Impervious, Inflow Depth = 8.03" for 100-Year event
Inflow = 1.12 cfs @ 12.07 hrs, Volume= 3,801 cf
Outflow = 0.57 cfs @ 12.24 hrs, Volume= 3,801 cf, Atten= 50%, Lag= 10.3 min
Discarded = 0.17 cfs @ 12.24 hrs, Volume= 3,629 cf
Primary = 0.40 cfs @ 12.24 hrs, Volume= 172 cf

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3
Peak Elev= 36.91' @ 12.24 hrs Surf.Area= 753 sf Storage= 904 cf
Flood Elev= 37.00' Surf.Area= 753 sf Storage= 904 cf

Plug-Flow detention time= (not calculated: outflow precedes inflow)
Center-of-Mass det. time= 30.9 min (781.7 - 750.8)

Volume	Invert	Avail.Storage	Storage Description
#1	33.80'	904 cf	**Subbase (Conic)**Listed below (Recalc) 2,259 cf Overall x 40.0% Voids

Elevation (feet)	Surf.Area (sq-ft)	Inc.Store (cubic-feet)	Cum.Store (cubic-feet)	Wet.Area (sq-ft)
33.80	753	0	0	753
36.80	753	2,259	2,259	1,045

Device	Routing	Invert	Outlet Devices
#1	Discarded	33.80'	**7.000 in/hr Exfiltration over Wetted area**
#2	Primary	36.80'	**4.5' long x 3.0' breadth Broad-Crested Rectangular Weir** Head (feet) 0.20 0.40 0.60 0.80 1.00 1.20 1.40 1.60 1.80 2.00 2.50 3.00 3.50 4.00 4.50 Coef. (English) 2.44 2.58 2.68 2.67 2.65 2.64 2.64 2.68 2.68 2.72 2.81 2.92 2.97 3.07 3.32

Discarded OutFlow Max=0.17 cfs @ 12.24 hrs HW=36.91' (Free Discharge)
└**1=Exfiltration** (Exfiltration Controls 0.17 cfs)

Primary OutFlow Max=0.35 cfs @ 12.24 hrs HW=36.90' TW=0.00' (Dynamic Tailwater)
└**2=Broad-Crested Rectangular Weir**(Weir Controls 0.35 cfs @ 0.77 fps)

Summary for Pond PP#2: Pervious Pave #1

Inflow Area = 972 sf, 55.84% Impervious, Inflow Depth = 5.48" for 100-Year event
Inflow = 0.18 cfs @ 12.00 hrs, Volume= 444 cf
Outflow = 0.09 cfs @ 12.09 hrs, Volume= 444 cf, Atten= 49%, Lag= 5.1 min
Discarded = 0.09 cfs @ 12.09 hrs, Volume= 444 cf
Primary = 0.00 cfs @ 0.00 hrs, Volume= 0 cf

Routing by Dyn-Stor-Ind method, Time Span= 0.00-72.00 hrs, dt= 0.01 hrs / 3
Peak Elev= 35.42' @ 12.09 hrs Surf.Area= 546 sf Storage= 26 cf
Flood Elev= 37.00' Surf.Area= 546 sf Storage= 328 cf

Plug-Flow detention time= (not calculated: outflow precedes inflow)
Center-of-Mass det. time= 0.9 min (811.4 - 810.6)

Volume	Invert	Avail.Storage	Storage Description
#1	35.30'	328 cf	**Subbase (Conic)** Listed below (Recalc) 819 cf Overall x 40.0% Voids

Elevation (feet)	Surf.Area (sq-ft)	Inc.Store (cubic-feet)	Cum.Store (cubic-feet)	Wet.Area (sq-ft)
35.30	546	0	0	546
36.80	546	819	819	670

Device	Routing	Invert	Outlet Devices
#1	Discarded	35.30'	**7.000 in/hr Exfiltration over Wetted area**
#2	Primary	36.80'	**4.5' long x 3.0' breadth Broad-Crested Rectangular Weir** Head (feet) 0.20 0.40 0.60 0.80 1.00 1.20 1.40 1.60 1.80 2.00 2.50 3.00 3.50 4.00 4.50 Coef. (English) 2.44 2.58 2.68 2.67 2.65 2.64 2.64 2.68 2.68 2.72 2.81 2.92 2.97 3.07 3.32

Discarded OutFlow Max=0.09 cfs @ 12.09 hrs HW=35.42' (Free Discharge)
└**1=Exfiltration** (Exfiltration Controls 0.09 cfs)

Primary OutFlow Max=0.00 cfs @ 0.00 hrs HW=35.30' TW=0.00' (Dynamic Tailwater)
└**2=Broad-Crested Rectangular Weir**(Controls 0.00 cfs)

Extreme Precipitation Tables

Northeast Regional Climate Center

Data represents point estimates calculated from partial duration series. All precipitation amounts are displayed in inches.

Smoothing	Yes
State	Massachusetts
Location	
Longitude	71.142 degrees West
Latitude	42.405 degrees North
Elevation	0 feet
Date/Time	Fri, 28 Aug 2020 14:10:00 -0400

Extreme Precipitation Estimates

	5min	10min	15min	30min	60min	120min		1hr	2hr	3hr	6hr	12hr	24hr	48hr		1day	2day	4day	7day	10day	
1yr	0.28	0.43	0.53	0.70	0.87	1.10	**1yr**	0.75	1.04	1.28	1.63	2.09	2.69	2.94	**1yr**	2.38	2.83	3.29	3.98	4.65	**1yr**
2yr	0.35	0.54	0.67	0.88	1.11	1.40	**2yr**	0.96	1.28	1.62	2.04	2.57	3.23	3.59	**2yr**	2.86	3.45	3.95	4.70	5.35	**2yr**
5yr	0.42	0.65	0.81	1.09	1.39	1.77	**5yr**	1.20	1.61	2.06	2.60	3.26	4.09	4.56	**5yr**	3.62	4.38	5.00	5.97	6.69	**5yr**
10yr	0.47	0.74	0.93	1.27	1.65	2.12	**10yr**	1.42	1.91	2.47	3.12	3.92	4.90	5.47	**10yr**	4.33	5.26	5.99	7.15	7.92	**10yr**
25yr	0.56	0.89	1.13	1.56	2.06	2.67	**25yr**	1.78	2.40	3.13	3.96	4.98	6.20	6.96	**25yr**	5.49	6.69	7.59	9.10	9.91	**25yr**
50yr	0.63	1.01	1.30	1.82	2.45	3.21	**50yr**	2.12	2.86	3.77	4.78	5.98	7.43	8.36	**50yr**	6.57	8.03	9.08	10.92	11.75	**50yr**
100yr	0.73	1.18	1.52	2.14	2.92	3.84	**100yr**	2.52	3.40	4.52	5.73	7.17	8.89	10.04	**100yr**	7.87	9.65	10.88	13.10	13.94	**100yr**
200yr	0.83	1.36	1.76	2.52	3.47	4.60	**200yr**	2.99	4.05	5.43	6.89	8.61	10.65	12.07	**200yr**	9.43	11.60	13.03	15.73	16.54	**200yr**
500yr	1.01	1.65	2.16	3.13	4.37	5.83	**500yr**	3.77	5.11	6.90	8.77	10.97	13.54	15.40	**500yr**	11.98	14.81	16.55	20.05	20.75	**500yr**

Lower Confidence Limits

	5min	10min	15min	30min	60min	120min		1hr	2hr	3hr	6hr	12hr	24hr	48hr		1day	2day	4day	7day	10day	
1yr	0.25	0.38	0.46	0.62	0.76	0.85	**1yr**	0.66	0.83	1.15	1.44	1.78	2.44	2.50	**1yr**	2.16	2.41	2.93	3.53	4.05	**1yr**
2yr	0.33	0.51	0.63	0.85	1.05	1.26	**2yr**	0.91	1.23	1.45	1.91	2.48	3.13	3.47	**2yr**	2.77	3.33	3.82	4.53	5.18	**2yr**
5yr	0.39	0.60	0.75	1.02	1.30	1.51	**5yr**	1.12	1.47	1.73	2.24	2.89	3.77	4.18	**5yr**	3.34	4.02	4.59	5.47	6.17	**5yr**
10yr	0.44	0.67	0.83	1.16	1.50	1.73	**10yr**	1.29	1.69	1.95	2.53	3.24	4.35	4.83	**10yr**	3.85	4.65	5.27	6.29	7.01	**10yr**

	5min	10min	15min	30min	60min	120min		1hr	2hr	3hr	6hr	12hr	24hr	48hr		1day	2day	4day	7day	10day	
25yr	0.50	0.77	0.95	1.36	1.79	2.05	**25yr**	1.54	2.00	2.31	2.96	3.78	5.23	5.82	**25yr**	4.63	5.60	6.31	7.52	8.29	**25yr**
50yr	0.56	0.85	1.06	1.52	2.05	2.35	**50yr**	1.77	2.30	2.61	3.34	4.24	5.99	6.70	**50yr**	5.30	6.44	7.22	8.60	9.39	**50yr**
100yr	0.63	0.95	1.18	1.71	2.35	2.68	**100yr**	2.03	2.62	2.96	3.62	4.77	6.89	7.70	**100yr**	6.10	7.41	8.27	9.79	10.65	**100yr**
200yr	0.70	1.06	1.34	1.94	2.71	3.06	**200yr**	2.34	2.99	3.36	4.05	5.37	7.91	8.86	**200yr**	7.00	8.52	9.46	11.12	12.03	**200yr**
500yr	0.82	1.23	1.58	2.29	3.26	3.65	**500yr**	2.81	3.57	3.97	4.70	6.29	9.50	10.64	**500yr**	8.41	10.23	11.30	13.12	14.12	**500yr**

Upper Confidence Limits

	5min	10min	15min	30min	60min	120min		1hr	2hr	3hr	6hr	12hr	24hr	48hr		1day	2day	4day	7day	10day	
1yr	0.31	0.48	0.58	0.79	0.97	1.13	**1yr**	0.83	1.11	1.32	1.77	2.25	2.86	3.17	**1yr**	2.53	3.05	3.51	4.29	5.03	**1yr**
2yr	0.36	0.56	0.69	0.94	1.15	1.36	**2yr**	1.00	1.33	1.57	2.08	2.68	3.35	3.74	**2yr**	2.97	3.59	4.11	4.89	5.55	**2yr**
5yr	0.45	0.70	0.86	1.19	1.51	1.79	**5yr**	1.30	1.75	2.05	2.66	3.39	4.44	5.00	**5yr**	3.93	4.81	5.43	6.48	7.21	**5yr**
10yr	0.55	0.84	1.05	1.46	1.89	2.20	**10yr**	1.63	2.15	2.55	3.22	4.07	5.51	6.25	**10yr**	4.88	6.01	6.72	8.04	8.83	**10yr**
25yr	0.71	1.08	1.35	1.92	2.53	2.90	**25yr**	2.19	2.83	3.39	4.16	5.17	7.32	8.42	**25yr**	6.48	8.09	8.92	10.74	11.56	**25yr**
50yr	0.86	1.31	1.64	2.35	3.17	3.59	**50yr**	2.73	3.51	4.21	5.05	6.22	9.08	10.54	**50yr**	8.04	10.14	11.04	13.40	14.18	**50yr**
100yr	1.06	1.60	2.00	2.89	3.96	4.42	**100yr**	3.42	4.32	5.22	6.37	7.47	11.28	13.22	**100yr**	9.98	12.71	13.68	16.75	17.43	**100yr**
200yr	1.29	1.94	2.45	3.55	4.95	5.46	**200yr**	4.27	5.34	6.49	7.78	8.96	14.02	16.60	**200yr**	12.41	15.96	16.97	20.95	21.46	**200yr**
500yr	1.68	2.50	3.21	4.67	6.63	7.20	**500yr**	5.72	7.04	8.66	10.14	11.41	18.71	22.44	**500yr**	16.56	21.58	22.57	28.20	28.29	**500yr**

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Northeast Regional Climate Center

United States Department of Agriculture

NRCS

Natural Resources Conservation Service

A product of the National Cooperative Soil Survey, a joint effort of the United States Department of Agriculture and other Federal agencies, State agencies including the Agricultural Experiment Stations, and local participants

Custom Soil Resource Report for Middlesex County, Massachusetts

Preface

Soil surveys contain information that affects land use planning in survey areas. They highlight soil limitations that affect various land uses and provide information about the properties of the soils in the survey areas. Soil surveys are designed for many different users, including farmers, ranchers, foresters, agronomists, urban planners, community officials, engineers, developers, builders, and home buyers. Also, conservationists, teachers, students, and specialists in recreation, waste disposal, and pollution control can use the surveys to help them understand, protect, or enhance the environment.

Various land use regulations of Federal, State, and local governments may impose special restrictions on land use or land treatment. Soil surveys identify soil properties that are used in making various land use or land treatment decisions. The information is intended to help the land users identify and reduce the effects of soil limitations on various land uses. The landowner or user is responsible for identifying and complying with existing laws and regulations.

Although soil survey information can be used for general farm, local, and wider area planning, onsite investigation is needed to supplement this information in some cases. Examples include soil quality assessments (http://www.nrcs.usda.gov/wps/portal/nrcs/main/soils/health/) and certain conservation and engineering applications. For more detailed information, contact your local USDA Service Center (https://offices.sc.egov.usda.gov/locator/app?agency=nrcs) or your NRCS State Soil Scientist (http://www.nrcs.usda.gov/wps/portal/nrcs/detail/soils/contactus/?cid=nrcs142p2_053951).

Great differences in soil properties can occur within short distances. Some soils are seasonally wet or subject to flooding. Some are too unstable to be used as a foundation for buildings or roads. Clayey or wet soils are poorly suited to use as septic tank absorption fields. A high water table makes a soil poorly suited to basements or underground installations.

The National Cooperative Soil Survey is a joint effort of the United States Department of Agriculture and other Federal agencies, State agencies including the Agricultural Experiment Stations, and local agencies. The Natural Resources Conservation Service (NRCS) has leadership for the Federal part of the National Cooperative Soil Survey.

Information about soils is updated periodically. Updated information is available through the NRCS Web Soil Survey, the site for official soil survey information.

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Contents

How Soil Surveys Are Made

Soil surveys are made to provide information about the soils and miscellaneous areas in a specific area. They include a description of the soils and miscellaneous areas and their location on the landscape and tables that show soil properties and limitations affecting various uses. Soil scientists observed the steepness, length, and shape of the slopes; the general pattern of drainage; the kinds of crops and native plants; and the kinds of bedrock. They observed and described many soil profiles. A soil profile is the sequence of natural layers, or horizons, in a soil. The profile extends from the surface down into the unconsolidated material in which the soil formed or from the surface down to bedrock. The unconsolidated material is devoid of roots and other living organisms and has not been changed by other biological activity.

Currently, soils are mapped according to the boundaries of major land resource areas (MLRAs). MLRAs are geographically associated land resource units that share common characteristics related to physiography, geology, climate, water resources, soils, biological resources, and land uses (USDA, 2006). Soil survey areas typically consist of parts of one or more MLRA.

The soils and miscellaneous areas in a survey area occur in an orderly pattern that is related to the geology, landforms, relief, climate, and natural vegetation of the area. Each kind of soil and miscellaneous area is associated with a particular kind of landform or with a segment of the landform. By observing the soils and miscellaneous areas in the survey area and relating their position to specific segments of the landform, a soil scientist develops a concept, or model, of how they were formed. Thus, during mapping, this model enables the soil scientist to predict with a considerable degree of accuracy the kind of soil or miscellaneous area at a specific location on the landscape.

Commonly, individual soils on the landscape merge into one another as their characteristics gradually change. To construct an accurate soil map, however, soil scientists must determine the boundaries between the soils. They can observe only a limited number of soil profiles. Nevertheless, these observations, supplemented by an understanding of the soil-vegetation-landscape relationship, are sufficient to verify predictions of the kinds of soil in an area and to determine the boundaries.

Soil scientists recorded the characteristics of the soil profiles that they studied. They noted soil color, texture, size and shape of soil aggregates, kind and amount of rock fragments, distribution of plant roots, reaction, and other features that enable them to identify soils. After describing the soils in the survey area and determining their properties, the soil scientists assigned the soils to taxonomic classes (units). Taxonomic classes are concepts. Each taxonomic class has a set of soil characteristics with precisely defined limits. The classes are used as a basis for comparison to classify soils systematically. Soil taxonomy, the system of taxonomic classification used in the United States, is based mainly on the kind and character of soil properties and the arrangement of horizons within the profile. After the soil

scientists classified and named the soils in the survey area, they compared the individual soils with similar soils in the same taxonomic class in other areas so that they could confirm data and assemble additional data based on experience and research.

The objective of soil mapping is not to delineate pure map unit components; the objective is to separate the landscape into landforms or landform segments that have similar use and management requirements. Each map unit is defined by a unique combination of soil components and/or miscellaneous areas in predictable proportions. Some components may be highly contrasting to the other components of the map unit. The presence of minor components in a map unit in no way diminishes the usefulness or accuracy of the data. The delineation of such landforms and landform segments on the map provides sufficient information for the development of resource plans. If intensive use of small areas is planned, onsite investigation is needed to define and locate the soils and miscellaneous areas.

Soil scientists make many field observations in the process of producing a soil map. The frequency of observation is dependent upon several factors, including scale of mapping, intensity of mapping, design of map units, complexity of the landscape, and experience of the soil scientist. Observations are made to test and refine the soil-landscape model and predictions and to verify the classification of the soils at specific locations. Once the soil-landscape model is refined, a significantly smaller number of measurements of individual soil properties are made and recorded. These measurements may include field measurements, such as those for color, depth to bedrock, and texture, and laboratory measurements, such as those for content of sand, silt, clay, salt, and other components. Properties of each soil typically vary from one point to another across the landscape.

Observations for map unit components are aggregated to develop ranges of characteristics for the components. The aggregated values are presented. Direct measurements do not exist for every property presented for every map unit component. Values for some properties are estimated from combinations of other properties.

While a soil survey is in progress, samples of some of the soils in the area generally are collected for laboratory analyses and for engineering tests. Soil scientists interpret the data from these analyses and tests as well as the field-observed characteristics and the soil properties to determine the expected behavior of the soils under different uses. Interpretations for all of the soils are field tested through observation of the soils in different uses and under different levels of management. Some interpretations are modified to fit local conditions, and some new interpretations are developed to meet local needs. Data are assembled from other sources, such as research information, production records, and field experience of specialists. For example, data on crop yields under defined levels of management are assembled from farm records and from field or plot experiments on the same kinds of soil.

Predictions about soil behavior are based not only on soil properties but also on such variables as climate and biological activity. Soil conditions are predictable over long periods of time, but they are not predictable from year to year. For example, soil scientists can predict with a fairly high degree of accuracy that a given soil will have a high water table within certain depths in most years, but they cannot predict that a high water table will always be at a specific level in the soil on a specific date.

After soil scientists located and identified the significant natural bodies of soil in the survey area, they drew the boundaries of these bodies on aerial photographs and

identified each as a specific map unit. Aerial photographs show trees, buildings, fields, roads, and rivers, all of which help in locating boundaries accurately.

Soil Map

The soil map section includes the soil map for the defined area of interest, a list of soil map units on the map and extent of each map unit, and cartographic symbols displayed on the map. Also presented are various metadata about data used to produce the map, and a description of each soil map unit.

Custom Soil Resource Report
Soil Map

Map Scale: 1:6,510 if printed on A landscape (11" x 8.5") sheet.

N

Meters: 0 50 100 200 300

Feet: 0 300 600 1200 1800

Map projection: Web Mercator Corner coordinates: WGS84 Edge tics: UTM Zone 19N WGS84

MAP LEGEND

Area of Interest (AOI)

- Area of Interest (AOI)

Soils

- Soil Map Unit Polygons
- Soil Map Unit Lines
- Soil Map Unit Points

Special Point Features

- Blowout
- Borrow Pit
- Clay Spot
- Closed Depression
- Gravel Pit
- Gravelly Spot
- Landfill
- Lava Flow
- Marsh or swamp
- Mine or Quarry
- Miscellaneous Water
- Perennial Water
- Rock Outcrop
- Saline Spot
- Sandy Spot
- Severely Eroded Spot
- Sinkhole
- Slide or Slip
- Sodic Spot
- Spoil Area
- Stony Spot
- Very Stony Spot
- Wet Spot
- Other
- Special Line Features

Water Features

- Streams and Canals

Transportation

- Rails
- Interstate Highways
- US Routes
- Major Roads
- Local Roads

Background

- Aerial Photography

MAP INFORMATION

The soil surveys that comprise your AOI were mapped at 1:25,000.

Warning: Soil Map may not be valid at this scale.

Enlargement of maps beyond the scale of mapping can cause misunderstanding of the detail of mapping and accuracy of soil line placement. The maps do not show the small areas of contrasting soils that could have been shown at a more detailed scale.

Please rely on the bar scale on each map sheet for map measurements.

Source of Map: Natural Resources Conservation Service
Web Soil Survey URL:
Coordinate System: Web Mercator (EPSG:3857)

Maps from the Web Soil Survey are based on the Web Mercator projection, which preserves direction and shape but distorts distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more accurate calculations of distance or area are required.

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Middlesex County, Massachusetts
Survey Area Data: Version 20, Jun 9, 2020

Soil map units are labeled (as space allows) for map scales 1:50,000 or larger.

Date(s) aerial images were photographed: Aug 13, 2020—Sep 15, 2020

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.

Map Unit Legend

Map Unit Symbol	Map Unit Name	Acres in AOI	Percent of AOI
253D	Hinckley loamy sand, 15 to 25 percent slopes	0.1	0.0%
420B	Canton fine sandy loam, 3 to 8 percent slopes	2.8	1.4%
602	Urban land	97.4	48.4%
626B	Merrimac-Urban land complex, 0 to 8 percent slopes	80.9	40.2%
631C	Charlton-Urban land-Hollis complex, 3 to 15 percent slopes, rocky	14.2	7.0%
654	Udorthents, loamy	4.7	2.3%
655	Udorthents, wet substratum	1.4	0.7%
Totals for Area of Interest		**201.4**	**100.0%**

Map Unit Descriptions

The map units delineated on the detailed soil maps in a soil survey represent the soils or miscellaneous areas in the survey area. The map unit descriptions, along with the maps, can be used to determine the composition and properties of a unit.

A map unit delineation on a soil map represents an area dominated by one or more major kinds of soil or miscellaneous areas. A map unit is identified and named according to the taxonomic classification of the dominant soils. Within a taxonomic class there are precisely defined limits for the properties of the soils. On the landscape, however, the soils are natural phenomena, and they have the characteristic variability of all natural phenomena. Thus, the range of some observed properties may extend beyond the limits defined for a taxonomic class. Areas of soils of a single taxonomic class rarely, if ever, can be mapped without including areas of other taxonomic classes. Consequently, every map unit is made up of the soils or miscellaneous areas for which it is named and some minor components that belong to taxonomic classes other than those of the major soils.

Most minor soils have properties similar to those of the dominant soil or soils in the map unit, and thus they do not affect use and management. These are called noncontrasting, or similar, components. They may or may not be mentioned in a particular map unit description. Other minor components, however, have properties and behavioral characteristics divergent enough to affect use or to require different management. These are called contrasting, or dissimilar, components. They generally are in small areas and could not be mapped separately because of the scale used. Some small areas of strongly contrasting soils or miscellaneous areas are identified by a special symbol on the maps. If included in the database for a given area, the contrasting minor components are identified in the map unit descriptions along with some characteristics of each. A few areas of minor components may not have been observed, and consequently they are not

mentioned in the descriptions, especially where the pattern was so complex that it was impractical to make enough observations to identify all the soils and miscellaneous areas on the landscape.

The presence of minor components in a map unit in no way diminishes the usefulness or accuracy of the data. The objective of mapping is not to delineate pure taxonomic classes but rather to separate the landscape into landforms or landform segments that have similar use and management requirements. The delineation of such segments on the map provides sufficient information for the development of resource plans. If intensive use of small areas is planned, however, onsite investigation is needed to define and locate the soils and miscellaneous areas.

An identifying symbol precedes the map unit name in the map unit descriptions. Each description includes general facts about the unit and gives important soil properties and qualities.

Soils that have profiles that are almost alike make up a *soil series*. Except for differences in texture of the surface layer, all the soils of a series have major horizons that are similar in composition, thickness, and arrangement.

Soils of one series can differ in texture of the surface layer, slope, stoniness, salinity, degree of erosion, and other characteristics that affect their use. On the basis of such differences, a soil series is divided into *soil phases*. Most of the areas shown on the detailed soil maps are phases of soil series. The name of a soil phase commonly indicates a feature that affects use or management. For example, Alpha silt loam, 0 to 2 percent slopes, is a phase of the Alpha series.

Some map units are made up of two or more major soils or miscellaneous areas. These map units are complexes, associations, or undifferentiated groups.

A *complex* consists of two or more soils or miscellaneous areas in such an intricate pattern or in such small areas that they cannot be shown separately on the maps. The pattern and proportion of the soils or miscellaneous areas are somewhat similar in all areas. Alpha-Beta complex, 0 to 6 percent slopes, is an example.

An *association* is made up of two or more geographically associated soils or miscellaneous areas that are shown as one unit on the maps. Because of present or anticipated uses of the map units in the survey area, it was not considered practical or necessary to map the soils or miscellaneous areas separately. The pattern and relative proportion of the soils or miscellaneous areas are somewhat similar. Alpha-Beta association, 0 to 2 percent slopes, is an example.

An *undifferentiated group* is made up of two or more soils or miscellaneous areas that could be mapped individually but are mapped as one unit because similar interpretations can be made for use and management. The pattern and proportion of the soils or miscellaneous areas in a mapped area are not uniform. An area can be made up of only one of the major soils or miscellaneous areas, or it can be made up of all of them. Alpha and Beta soils, 0 to 2 percent slopes, is an example.

Some surveys include *miscellaneous areas*. Such areas have little or no soil material and support little or no vegetation. Rock outcrop is an example.

Middlesex County, Massachusetts

253D—Hinckley loamy sand, 15 to 25 percent slopes

Map Unit Setting

National map unit symbol: 2svmc
Elevation: 0 to 1,460 feet
Mean annual precipitation: 36 to 71 inches
Mean annual air temperature: 39 to 55 degrees F
Frost-free period: 140 to 240 days
Farmland classification: Not prime farmland

Map Unit Composition

Hinckley and similar soils: 85 percent
Minor components: 15 percent
Estimates are based on observations, descriptions, and transects of the mapunit.

Description of Hinckley

Setting

Landform: Outwash terraces, outwash deltas, kame terraces, kames, outwash plains, eskers, moraines
Landform position (two-dimensional): Backslope
Landform position (three-dimensional): Nose slope, side slope, crest, head slope, riser
Down-slope shape: Linear, concave, convex
Across-slope shape: Convex, linear, concave
Parent material: Sandy and gravelly glaciofluvial deposits derived from gneiss and/or granite and/or schist

Typical profile

Oe - 0 to 1 inches: moderately decomposed plant material
A - 1 to 8 inches: loamy sand
Bw1 - 8 to 11 inches: gravelly loamy sand
Bw2 - 11 to 16 inches: gravelly loamy sand
BC - 16 to 19 inches: very gravelly loamy sand
C - 19 to 65 inches: very gravelly sand

Properties and qualities

Slope: 15 to 25 percent
Depth to restrictive feature: More than 80 inches
Drainage class: Excessively drained
Runoff class: Low
Capacity of the most limiting layer to transmit water (Ksat): Moderately high to very high (1.42 to 99.90 in/hr)
Depth to water table: More than 80 inches
Frequency of flooding: None
Frequency of ponding: None
Maximum salinity: Nonsaline (0.0 to 1.9 mmhos/cm)
Available water capacity: Low (about 3.1 inches)

Interpretive groups

Land capability classification (irrigated): None specified
Land capability classification (nonirrigated): 6e
Hydrologic Soil Group: A

Ecological site: F144AY022MA - Dry Outwash
Hydric soil rating: No

Minor Components

Merrimac
Percent of map unit: 8 percent
Landform: Kames, outwash terraces, eskers, moraines, outwash plains
Landform position (two-dimensional): Backslope
Landform position (three-dimensional): Side slope, crest, head slope, nose slope, riser
Down-slope shape: Convex
Across-slope shape: Convex
Hydric soil rating: No

Windsor
Percent of map unit: 5 percent
Landform: Outwash deltas, eskers, moraines, kame terraces, kames, outwash plains, outwash terraces
Landform position (two-dimensional): Backslope
Landform position (three-dimensional): Side slope, crest, head slope, nose slope, riser
Down-slope shape: Convex, concave, linear
Across-slope shape: Concave, linear, convex
Hydric soil rating: No

Sudbury
Percent of map unit: 2 percent
Landform: Outwash plains, moraines, outwash deltas, kame terraces, eskers, outwash terraces
Landform position (two-dimensional): Backslope, footslope
Landform position (three-dimensional): Base slope, tread
Down-slope shape: Concave, linear, convex
Across-slope shape: Linear, concave, convex
Hydric soil rating: No

420B—Canton fine sandy loam, 3 to 8 percent slopes

Map Unit Setting
National map unit symbol: 2w81b
Elevation: 0 to 1,180 feet
Mean annual precipitation: 36 to 71 inches
Mean annual air temperature: 39 to 55 degrees F
Frost-free period: 140 to 240 days
Farmland classification: All areas are prime farmland

Map Unit Composition
Canton and similar soils: 80 percent
Minor components: 20 percent
Estimates are based on observations, descriptions, and transects of the mapunit.

Description of Canton

Setting

Landform: Ridges, moraines, hills
Landform position (two-dimensional): Backslope, summit, shoulder
Landform position (three-dimensional): Side slope, crest, nose slope
Down-slope shape: Convex, linear
Across-slope shape: Convex
Parent material: Coarse-loamy over sandy melt-out till derived from gneiss, granite, and/or schist

Typical profile

Ap - 0 to 7 inches: fine sandy loam
Bw1 - 7 to 15 inches: fine sandy loam
Bw2 - 15 to 26 inches: gravelly fine sandy loam
2C - 26 to 65 inches: gravelly loamy sand

Properties and qualities

Slope: 3 to 8 percent
Depth to restrictive feature: 19 to 39 inches to strongly contrasting textural stratification
Drainage class: Well drained
Runoff class: Low
Capacity of the most limiting layer to transmit water (Ksat): Moderately low to high (0.14 to 14.17 in/hr)
Depth to water table: More than 80 inches
Frequency of flooding: None
Frequency of ponding: None
Available water capacity: Very low (about 2.7 inches)

Interpretive groups

Land capability classification (irrigated): None specified
Land capability classification (nonirrigated): 2s
Hydrologic Soil Group: B
Ecological site: F144AY034CT - Well Drained Till Uplands
Hydric soil rating: No

Minor Components

Scituate

Percent of map unit: 10 percent
Landform: Drumlins, hills, ground moraines
Landform position (two-dimensional): Footslope, backslope, summit
Landform position (three-dimensional): Crest, side slope
Down-slope shape: Linear, convex
Across-slope shape: Convex
Hydric soil rating: No

Montauk

Percent of map unit: 5 percent
Landform: Drumlins, hills, ground moraines, moraines
Landform position (two-dimensional): Backslope, shoulder, summit
Landform position (three-dimensional): Side slope, crest
Down-slope shape: Linear, convex
Across-slope shape: Convex
Hydric soil rating: No

Charlton

Percent of map unit: 4 percent
Landform: Hills, ground moraines, ridges
Landform position (two-dimensional): Backslope, shoulder, summit
Landform position (three-dimensional): Crest, side slope
Down-slope shape: Linear, convex
Across-slope shape: Convex
Hydric soil rating: No

Swansea

Percent of map unit: 1 percent
Landform: Kettles, swamps, bogs, depressions, marshes
Down-slope shape: Concave
Across-slope shape: Concave
Hydric soil rating: Yes

602—Urban land

Map Unit Setting

National map unit symbol: 9950
Elevation: 0 to 3,000 feet
Mean annual precipitation: 32 to 50 inches
Mean annual air temperature: 45 to 50 degrees F
Frost-free period: 110 to 200 days
Farmland classification: Not prime farmland

Map Unit Composition

Urban land: 85 percent
Minor components: 15 percent
Estimates are based on observations, descriptions, and transects of the mapunit.

Description of Urban Land

Setting

Landform position (two-dimensional): Footslope
Landform position (three-dimensional): Base slope
Down-slope shape: Linear
Across-slope shape: Linear
Parent material: Excavated and filled land

Minor Components

Rock outcrop

Percent of map unit: 5 percent
Landform: Ledges
Landform position (two-dimensional): Summit
Landform position (three-dimensional): Head slope
Down-slope shape: Concave
Across-slope shape: Concave

Udorthents, wet substratum
Percent of map unit: 5 percent
Hydric soil rating: No

Udorthents, loamy
Percent of map unit: 5 percent
Hydric soil rating: No

626B—Merrimac-Urban land complex, 0 to 8 percent slopes

Map Unit Setting
National map unit symbol: 2tyr9
Elevation: 0 to 820 feet
Mean annual precipitation: 36 to 71 inches
Mean annual air temperature: 39 to 55 degrees F
Frost-free period: 140 to 250 days
Farmland classification: Not prime farmland

Map Unit Composition
Merrimac and similar soils: 45 percent
Urban land: 40 percent
Minor components: 15 percent
Estimates are based on observations, descriptions, and transects of the mapunit.

Description of Merrimac

Setting
Landform: Eskers, moraines, outwash terraces, outwash plains, kames
Landform position (two-dimensional): Backslope, footslope, summit, shoulder
Landform position (three-dimensional): Side slope, crest, riser, tread
Down-slope shape: Convex
Across-slope shape: Convex
Parent material: Loamy glaciofluvial deposits derived from granite, schist, and gneiss over sandy and gravelly glaciofluvial deposits derived from granite, schist, and gneiss

Typical profile
Ap - 0 to 10 inches: fine sandy loam
Bw1 - 10 to 22 inches: fine sandy loam
Bw2 - 22 to 26 inches: stratified gravel to gravelly loamy sand
2C - 26 to 65 inches: stratified gravel to very gravelly sand

Properties and qualities
Slope: 0 to 8 percent
Depth to restrictive feature: More than 80 inches
Drainage class: Somewhat excessively drained
Runoff class: Very low
Capacity of the most limiting layer to transmit water (Ksat): Moderately high to very high (1.42 to 99.90 in/hr)
Depth to water table: More than 80 inches

Frequency of flooding: None
Frequency of ponding: None
Calcium carbonate, maximum content: 2 percent
Maximum salinity: Nonsaline (0.0 to 1.4 mmhos/cm)
Sodium adsorption ratio, maximum: 1.0
Available water capacity: Low (about 4.6 inches)

Interpretive groups

Land capability classification (irrigated): None specified
Land capability classification (nonirrigated): 2e
Hydrologic Soil Group: A
Ecological site: F144AY022MA - Dry Outwash
Hydric soil rating: No

Description of Urban Land

Typical profile

M - 0 to 10 inches: cemented material

Properties and qualities

Slope: 0 to 8 percent
Depth to restrictive feature: 0 inches to manufactured layer
Runoff class: Very high
Capacity of the most limiting layer to transmit water (Ksat): Very low (0.00 to 0.00 in/hr)
Available water capacity: Very low (about 0.0 inches)

Interpretive groups

Land capability classification (irrigated): None specified
Land capability classification (nonirrigated): 8
Hydrologic Soil Group: D
Hydric soil rating: Unranked

Minor Components

Windsor

Percent of map unit: 5 percent
Landform: Dunes, outwash terraces, deltas, outwash plains
Landform position (three-dimensional): Tread, riser
Down-slope shape: Convex, linear
Across-slope shape: Convex, linear
Hydric soil rating: No

Sudbury

Percent of map unit: 5 percent
Landform: Outwash plains, terraces, deltas
Landform position (two-dimensional): Footslope
Landform position (three-dimensional): Tread, dip
Down-slope shape: Concave
Across-slope shape: Linear
Hydric soil rating: No

Hinckley

Percent of map unit: 5 percent
Landform: Eskers, kames, deltas, outwash plains
Landform position (two-dimensional): Summit, shoulder, backslope
Landform position (three-dimensional): Nose slope, side slope, crest, head slope, rise

Down-slope shape: Convex
Across-slope shape: Convex, linear
Hydric soil rating: No

631C—Charlton-Urban land-Hollis complex, 3 to 15 percent slopes, rocky

Map Unit Setting

National map unit symbol: vr1g
Elevation: 0 to 1,000 feet
Mean annual precipitation: 32 to 54 inches
Mean annual air temperature: 43 to 54 degrees F
Frost-free period: 110 to 240 days
Farmland classification: Not prime farmland

Map Unit Composition

Charlton and similar soils: 45 percent
Urban land: 35 percent
Hollis and similar soils: 10 percent
Minor components: 10 percent
Estimates are based on observations, descriptions, and transects of the mapunit.

Description of Charlton

Setting

Landform: Drumlins, ground moraines
Landform position (two-dimensional): Backslope
Landform position (three-dimensional): Side slope
Down-slope shape: Linear
Across-slope shape: Convex
Parent material: Friable loamy eolian deposits over friable loamy basal till derived from granite and gneiss

Typical profile

H1 - 0 to 5 inches: fine sandy loam
H2 - 5 to 22 inches: sandy loam
H3 - 22 to 65 inches: gravelly sandy loam

Properties and qualities

Slope: 3 to 15 percent
Depth to restrictive feature: More than 80 inches
Drainage class: Well drained
Capacity of the most limiting layer to transmit water (Ksat): Moderately high to high (0.60 to 6.00 in/hr)
Depth to water table: More than 80 inches
Frequency of flooding: None
Frequency of ponding: None
Available water capacity: Moderate (about 7.3 inches)

Interpretive groups

Land capability classification (irrigated): None specified
Land capability classification (nonirrigated): 3e
Hydrologic Soil Group: A
Ecological site: F144AY034CT - Well Drained Till Uplands
Hydric soil rating: No

Description of Urban Land

Setting

Landform position (two-dimensional): Footslope
Landform position (three-dimensional): Base slope
Down-slope shape: Linear
Across-slope shape: Linear
Parent material: Excavated and filled land

Description of Hollis

Setting

Landform: Ridges, hillslopes
Landform position (two-dimensional): Backslope
Landform position (three-dimensional): Side slope
Down-slope shape: Linear
Across-slope shape: Convex
Parent material: Friable, shallow loamy basal till over granite and gneiss

Typical profile

H1 - 0 to 2 inches: fine sandy loam
H2 - 2 to 14 inches: fine sandy loam
H3 - 14 to 18 inches: unweathered bedrock

Properties and qualities

Slope: 3 to 15 percent
Surface area covered with cobbles, stones or boulders: 9.0 percent
Depth to restrictive feature: 8 to 20 inches to lithic bedrock
Drainage class: Well drained
Capacity of the most limiting layer to transmit water (Ksat): Very low to moderately low (0.00 to 0.14 in/hr)
Depth to water table: More than 80 inches
Frequency of flooding: None
Frequency of ponding: None
Available water capacity: Very low (about 2.0 inches)

Interpretive groups

Land capability classification (irrigated): None specified
Land capability classification (nonirrigated): 6s
Hydrologic Soil Group: D
Ecological site: F144AY033MA - Shallow Dry Till Uplands
Hydric soil rating: No

Minor Components

Canton

Percent of map unit: 4 percent
Landform: Hills
Landform position (two-dimensional): Backslope, toeslope
Landform position (three-dimensional): Side slope, base slope

Down-slope shape: Linear
Across-slope shape: Convex
Hydric soil rating: No

Udorthents, loamy
Percent of map unit: 2 percent
Hydric soil rating: No

Rock outcrop
Percent of map unit: 2 percent
Landform: Ledges
Landform position (two-dimensional): Summit
Landform position (three-dimensional): Head slope
Down-slope shape: Concave
Across-slope shape: Concave

Scituate
Percent of map unit: 1 percent
Landform: Depressions, hillslopes
Landform position (two-dimensional): Toeslope, summit
Landform position (three-dimensional): Base slope, head slope
Down-slope shape: Linear
Across-slope shape: Concave
Hydric soil rating: No

Montauk
Percent of map unit: 1 percent
Landform: Hillslopes
Landform position (two-dimensional): Shoulder, summit
Landform position (three-dimensional): Nose slope, head slope
Down-slope shape: Convex
Across-slope shape: Convex
Hydric soil rating: No

654—Udorthents, loamy

Map Unit Setting
National map unit symbol: vr1l
Elevation: 0 to 3,000 feet
Mean annual precipitation: 32 to 50 inches
Mean annual air temperature: 45 to 50 degrees F
Frost-free period: 110 to 200 days
Farmland classification: Not prime farmland

Map Unit Composition
Udorthents, loamy, and similar soils: 80 percent
Minor components: 20 percent
Estimates are based on observations, descriptions, and transects of the mapunit.

Description of Udorthents, Loamy

Setting

Parent material: Loamy alluvium and/or sandy glaciofluvial deposits and/or loamy glaciolacustrine deposits and/or loamy marine deposits and/or loamy basal till and/or loamy lodgment till

Properties and qualities

Depth to restrictive feature: More than 80 inches
Depth to water table: More than 80 inches
Frequency of flooding: None
Frequency of ponding: None

Minor Components

Udorthents, sandy

Percent of map unit: 10 percent
Hydric soil rating: No

Udorthents, wet substratum

Percent of map unit: 5 percent
Hydric soil rating: Yes

Urban land

Percent of map unit: 5 percent
Landform position (two-dimensional): Footslope
Landform position (three-dimensional): Base slope
Down-slope shape: Linear
Across-slope shape: Linear

655—Udorthents, wet substratum

Map Unit Setting

National map unit symbol: vr1n
Elevation: 0 to 3,000 feet
Mean annual precipitation: 32 to 54 inches
Mean annual air temperature: 43 to 54 degrees F
Frost-free period: 110 to 240 days
Farmland classification: Not prime farmland

Map Unit Composition

Udorthents, wet substratum, and similar soils: 85 percent
Minor components: 15 percent
Estimates are based on observations, descriptions, and transects of the mapunit.

Description of Udorthents, Wet Substratum

Setting

Parent material: Loamy alluvium and/or sandy glaciofluvial deposits and/or loamy glaciolacustrine deposits and/or loamy marine deposits and/or loamy basal till and/or loamy lodgment till

Properties and qualities

Slope: 0 to 8 percent
Depth to restrictive feature: More than 80 inches
Depth to water table: More than 80 inches
Frequency of flooding: None
Frequency of ponding: None

Minor Components

Urban land

Percent of map unit: 8 percent
Landform position (two-dimensional): Footslope
Landform position (three-dimensional): Base slope
Down-slope shape: Linear
Across-slope shape: Linear

Freetown

Percent of map unit: 4 percent
Landform: Depressions, bogs
Landform position (two-dimensional): Toeslope
Landform position (three-dimensional): Dip
Down-slope shape: Concave
Across-slope shape: Concave
Hydric soil rating: Yes

Swansea

Percent of map unit: 3 percent
Landform: Bogs, depressions
Landform position (two-dimensional): Toeslope
Landform position (three-dimensional): Dip
Down-slope shape: Concave
Across-slope shape: Concave
Hydric soil rating: Yes

Soil Information for All Uses

Soil Properties and Qualities

The Soil Properties and Qualities section includes various soil properties and qualities displayed as thematic maps with a summary table for the soil map units in the selected area of interest. A single value or rating for each map unit is generated by aggregating the interpretive ratings of individual map unit components. This aggregation process is defined for each property or quality.

Soil Physical Properties

Soil Physical Properties are measured or inferred from direct observations in the field or laboratory. Examples of soil physical properties include percent clay, organic matter, saturated hydraulic conductivity, available water capacity, and bulk density.

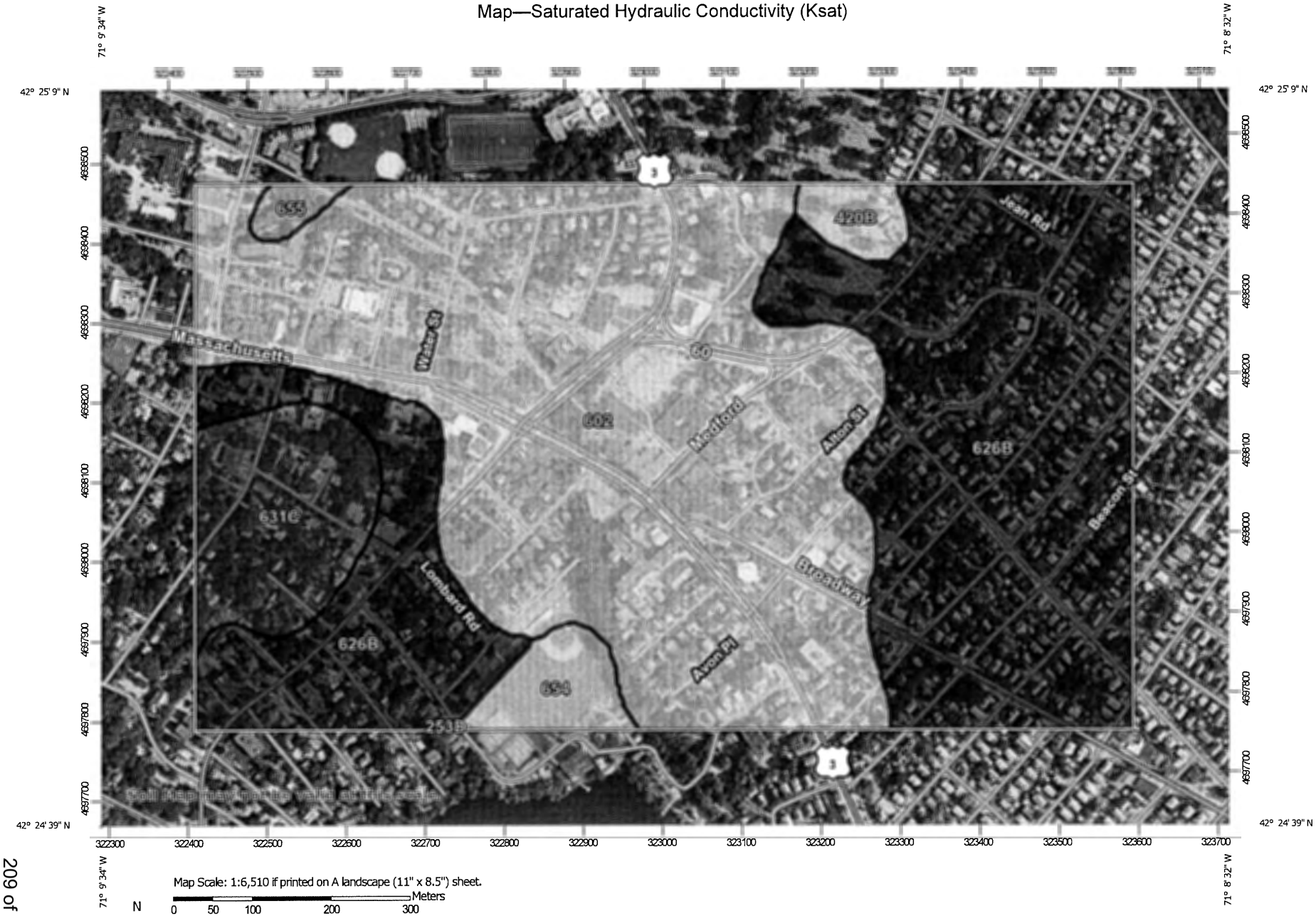
Saturated Hydraulic Conductivity (Ksat)

Saturated hydraulic conductivity (Ksat) refers to the ease with which pores in a saturated soil transmit water. The estimates are expressed in terms of micrometers per second. They are based on soil characteristics observed in the field, particularly structure, porosity, and texture. Saturated hydraulic conductivity is considered in the design of soil drainage systems and septic tank absorption fields.

For each soil layer, this attribute is actually recorded as three separate values in the database. A low value and a high value indicate the range of this attribute for the soil component. A "representative" value indicates the expected value of this attribute for the component. For this soil property, only the representative value is used.

The numeric Ksat values have been grouped according to standard Ksat class limits.

Map—Saturated Hydraulic Conductivity (Ksat)



Map Scale: 1:6,510 if printed on A landscape (11" x 8.5") sheet.

N

Meters: 0 50 100 200 300

Feet: 0 300 600 1200 1800

Map projection: Web Mercator Corner coordinates: WGS84 Edge tics: UTM Zone 19N WGS84

MAP LEGEND

Area of Interest (AOI)

Area of Interest (AOI)

Soils

Soil Rating Polygons

<= 23.2900

> 23.2900 and <= 54.0000

> 54.0000 and <= 100.0000

Not rated or not available

Soil Rating Lines

<= 23.2900

> 23.2900 and <= 54.0000

> 54.0000 and <= 100.0000

Not rated or not available

Soil Rating Points

<= 23.2900

> 23.2900 and <= 54.0000

> 54.0000 and <= 100.0000

Not rated or not available

Water Features

Streams and Canals

Transportation

Rails

Interstate Highways

US Routes

Major Roads

Local Roads

Background

Aerial Photography

MAP INFORMATION

The soil surveys that comprise your AOI were mapped at 1:25,000.

Warning: Soil Map may not be valid at this scale.

Enlargement of maps beyond the scale of mapping can cause misunderstanding of the detail of mapping and accuracy of soil line placement. The maps do not show the small areas of contrasting soils that could have been shown at a more detailed scale.

Please rely on the bar scale on each map sheet for map measurements.

Source of Map: Natural Resources Conservation Service
Web Soil Survey URL:
Coordinate System: Web Mercator (EPSG:3857)

Maps from the Web Soil Survey are based on the Web Mercator projection, which preserves direction and shape but distorts distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more accurate calculations of distance or area are required.

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Middlesex County, Massachusetts
Survey Area Data: Version 20, Jun 9, 2020

Soil map units are labeled (as space allows) for map scales 1:50,000 or larger.

Date(s) aerial images were photographed: Aug 13, 2020—Sep 15, 2020

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.

Table—Saturated Hydraulic Conductivity (Ksat)

Map unit symbol	Map unit name	Rating (micrometers per second)	Acres in AOI	Percent of AOI
253D	Hinckley loamy sand, 15 to 25 percent slopes	100.0000	0.1	0.0%
420B	Canton fine sandy loam, 3 to 8 percent slopes	54.0000	2.8	1.4%
602	Urban land		97.4	48.4%
626B	Merrimac-Urban land complex, 0 to 8 percent slopes	100.0000	80.9	40.2%
631C	Charlton-Urban land-Hollis complex, 3 to 15 percent slopes, rocky	23.2900	14.2	7.0%
654	Udorthents, loamy		4.7	2.3%
655	Udorthents, wet substratum		1.4	0.7%
Totals for Area of Interest			**201.4**	**100.0%**

Rating Options—Saturated Hydraulic Conductivity (Ksat)

Units of Measure: micrometers per second

Aggregation Method: Dominant Component

Component Percent Cutoff: None Specified

Tie-break Rule: Fastest

Interpret Nulls as Zero: No

Layer Options (Horizon Aggregation Method): Depth Range (Weighted Average)

Top Depth: 12

Bottom Depth: 120

Units of Measure: Inches

Soil Qualities and Features

Soil qualities are behavior and performance attributes that are not directly measured, but are inferred from observations of dynamic conditions and from soil properties. Example soil qualities include natural drainage, and frost action. Soil features are attributes that are not directly part of the soil. Example soil features include slope and depth to restrictive layer. These features can greatly impact the use and management of the soil.

Hydrologic Soil Group

Hydrologic soil groups are based on estimates of runoff potential. Soils are assigned to one of four groups according to the rate of water infiltration when the soils are not protected by vegetation, are thoroughly wet, and receive precipitation from long-duration storms.

The soils in the United States are assigned to four groups (A, B, C, and D) and three dual classes (A/D, B/D, and C/D). The groups are defined as follows:

Group A. Soils having a high infiltration rate (low runoff potential) when thoroughly wet. These consist mainly of deep, well drained to excessively drained sands or gravelly sands. These soils have a high rate of water transmission.

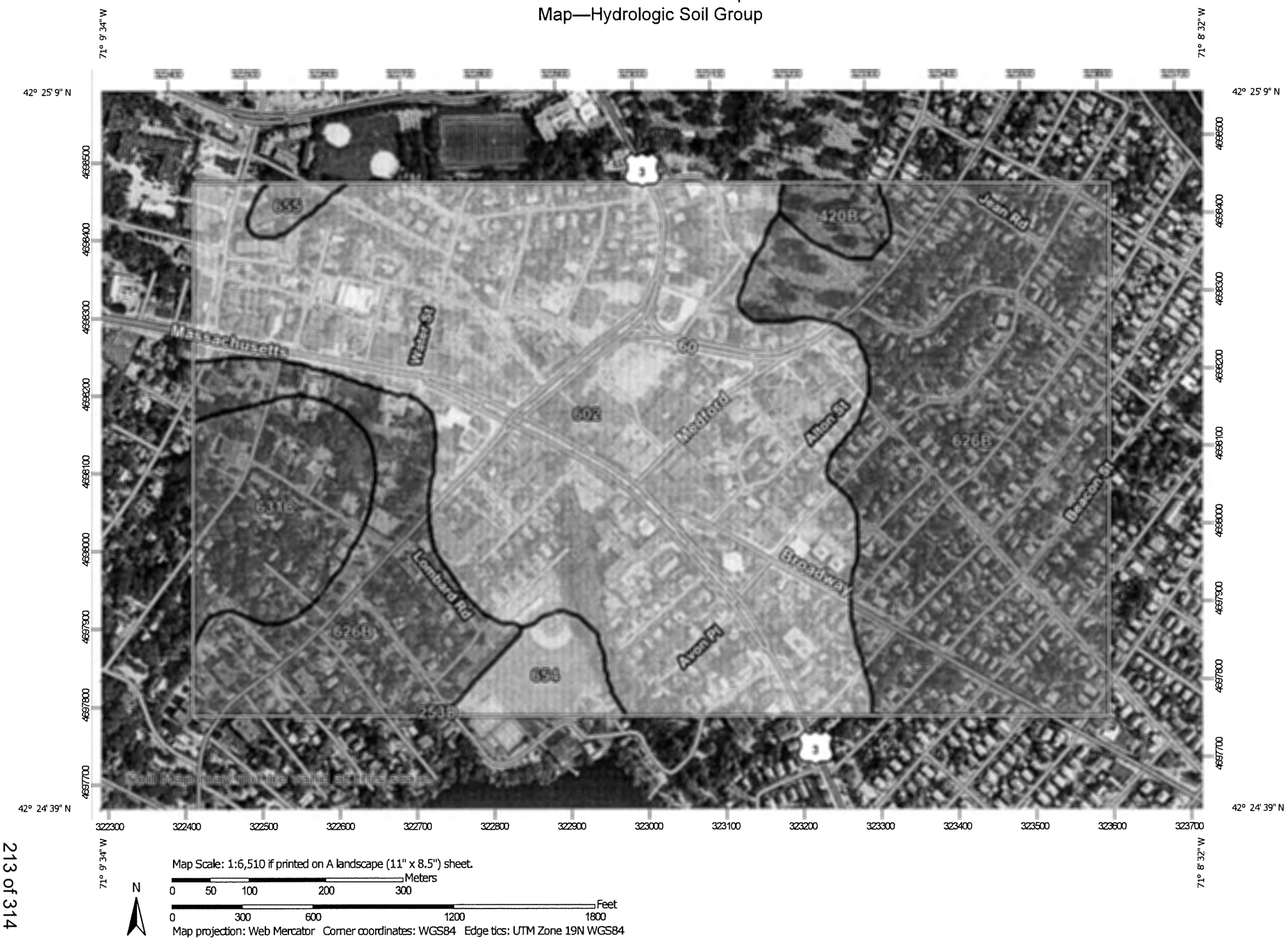
Group B. Soils having a moderate infiltration rate when thoroughly wet. These consist chiefly of moderately deep or deep, moderately well drained or well drained soils that have moderately fine texture to moderately coarse texture. These soils have a moderate rate of water transmission.

Group C. Soils having a slow infiltration rate when thoroughly wet. These consist chiefly of soils having a layer that impedes the downward movement of water or soils of moderately fine texture or fine texture. These soils have a slow rate of water transmission.

Group D. Soils having a very slow infiltration rate (high runoff potential) when thoroughly wet. These consist chiefly of clays that have a high shrink-swell potential, soils that have a high water table, soils that have a claypan or clay layer at or near the surface, and soils that are shallow over nearly impervious material. These soils have a very slow rate of water transmission.

If a soil is assigned to a dual hydrologic group (A/D, B/D, or C/D), the first letter is for drained areas and the second is for undrained areas. Only the soils that in their natural condition are in group D are assigned to dual classes.

Map—Hydrologic Soil Group



MAP LEGEND

Area of Interest (AOI)

Area of Interest (AOI)

Soils

Soil Rating Polygons

- A
- A/D
- B
- B/D
- C
- C/D
- D
- Not rated or not available

Soil Rating Lines

- A
- A/D
- B
- B/D
- C
- C/D
- D
- Not rated or not available

Soil Rating Points

- A
- A/D
- B
- B/D
- C
- C/D
- D
- Not rated or not available

Water Features

Streams and Canals

Transportation

- Rails
- Interstate Highways
- US Routes
- Major Roads
- Local Roads

Background

Aerial Photography

MAP INFORMATION

The soil surveys that comprise your AOI were mapped at 1:25,000.

Warning: Soil Map may not be valid at this scale.

Enlargement of maps beyond the scale of mapping can cause misunderstanding of the detail of mapping and accuracy of soil line placement. The maps do not show the small areas of contrasting soils that could have been shown at a more detailed scale.

Please rely on the bar scale on each map sheet for map measurements.

Source of Map: Natural Resources Conservation Service
Web Soil Survey URL:
Coordinate System: Web Mercator (EPSG:3857)

Maps from the Web Soil Survey are based on the Web Mercator projection, which preserves direction and shape but distorts distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more accurate calculations of distance or area are required.

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Middlesex County, Massachusetts
Survey Area Data: Version 20, Jun 9, 2020

Soil map units are labeled (as space allows) for map scales 1:50,000 or larger.

Date(s) aerial images were photographed: Aug 13, 2020—Sep 15, 2020

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.

Table—Hydrologic Soil Group

Map unit symbol	Map unit name	Rating	Acres in AOI	Percent of AOI
253D	Hinckley loamy sand, 15 to 25 percent slopes	A	0.1	0.0%
420B	Canton fine sandy loam, 3 to 8 percent slopes	B	2.8	1.4%
602	Urban land		97.4	48.4%
626B	Merrimac-Urban land complex, 0 to 8 percent slopes	A	80.9	40.2%
631C	Charlton-Urban land-Hollis complex, 3 to 15 percent slopes, rocky	A	14.2	7.0%
654	Udorthents, loamy		4.7	2.3%
655	Udorthents, wet substratum		1.4	0.7%
Totals for Area of Interest			**201.4**	**100.0%**

Rating Options—Hydrologic Soil Group

Aggregation Method: Dominant Condition

Component Percent Cutoff: None Specified

Tie-break Rule: Higher

References

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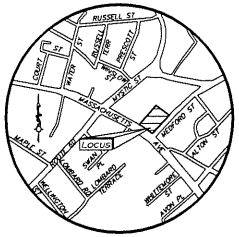
SITE DEVELOPMENT PLAN SET

455-457 MASSACHUSETTS AVENUE

ARLINGTON, MA 02476

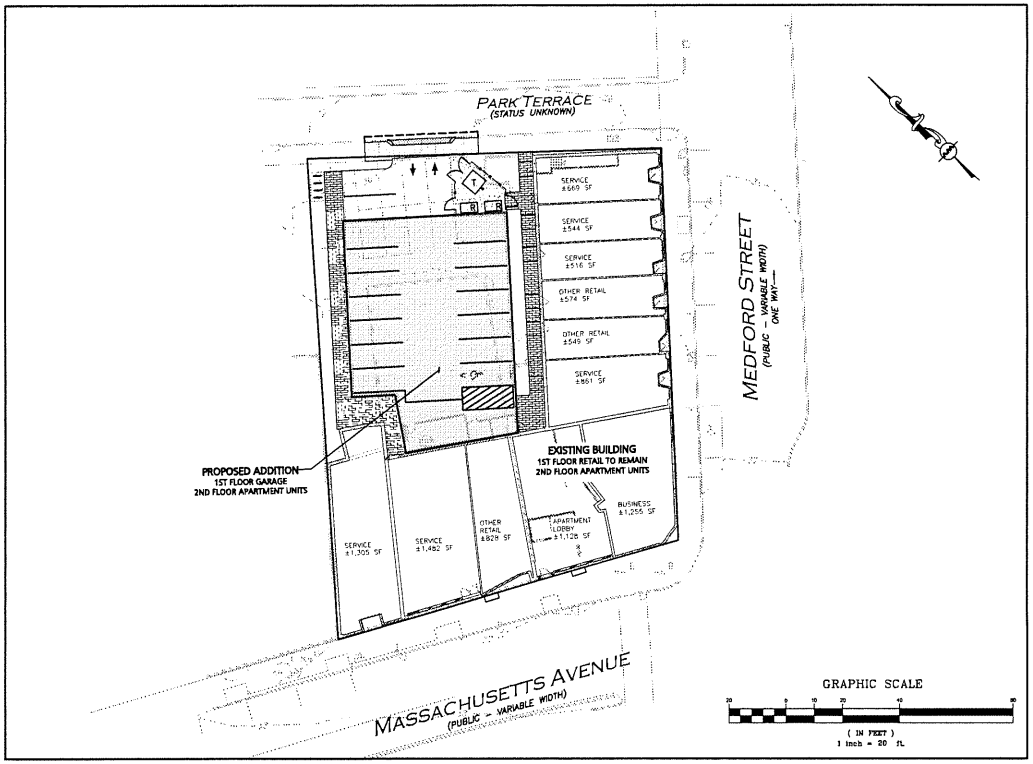


LOCUS MAP
NOT TO SCALE

APPLICANT:
2-14 MEDFORD STREET, LLC
455 MASSACHUSETTS AVE, STE 1
ARLINGTON, MA 02474

ARCHITECT:
MARKET SQUARE ARCHITECTS
104 CONGRESS STREET, STE 203
PORTSMOUTH, NH 03801
(603) 501-0202

CIVIL ENGINEER, LANDSCAPE ARCHITECT & LAND SURVEYOR:
ALLEN & MAJOR ASSOCIATES, INC.
100 COMMERCE WAY, SUITE 5
WOBURN, MA 01801
(781) 985-6889



LIST OF DRAWINGS

DRAWING TITLE	SHEET	ISSUED	REVISED
EXISTING CONDITIONS	1	8/19/2021	-
SITE PREPARATION PLAN	C-101	8/20/2021	-
LAYOUT & MATERIALS PLAN	C-102	8/20/2021	-
GRADING & DRAINAGE PLAN	C-103	8/20/2021	-
DETAILS	C-501	8/20/2021	-
DETAILS	C-502	8/20/2021	-
LANDSCAPE PLAN	L-101	8/20/2021	-
LANDSCAPE DETAILS	L-501	8/20/2021	-

COMMONWEALTH OF MASSACHUSETTS
BRIAN D. JONES
CIVIL
No. 40212
REGISTERED PROFESSIONAL ENGINEER

8.20.21

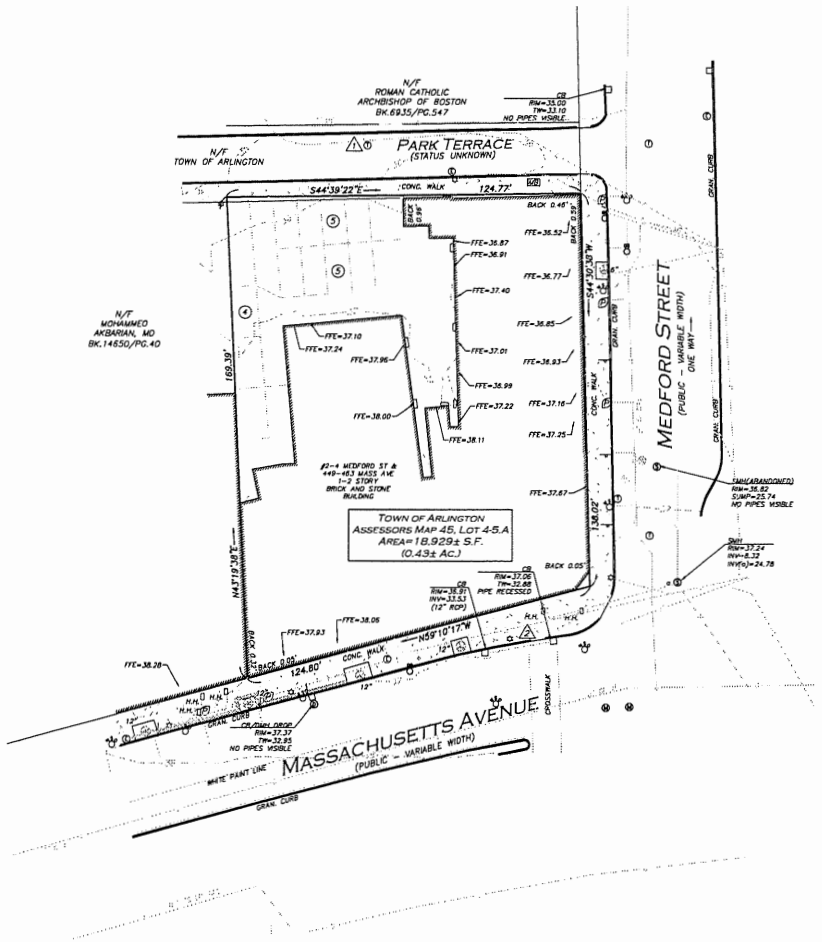
PROFESSIONAL ENGINEER FOR
ALLEN & MAJOR ASSOCIATES, INC.

PREPARED BY:
ALLEN & MAJOR ASSOCIATES, INC.
civil engineering • land surveying
environmental consulting • landscape architecture
www.allenmajor.com
100 COMMERCE WAY, SUITE 5
WOBURN MA 01801
TEL: (781) 935-6889
FAX: (781) 935-2896
WOBURN, MA • LAKEVILLE, MA • MANCHESTER, NH

ISSUED FOR ARB REVIEW: 8/20/2021

R:\PROJECTS\2729-03\CIVIL\DRAWINGS\CURRENT\C-2729-03_COVER.DWG

LOCUS MAP
(NOT TO SCALE)



LEGEND

DRAIN MANHOLE (DMH)	D
SEWER MANHOLE (SMH)	S
ELECTRIC MANHOLE (EMH)	E
MISC. MANHOLE (MH)	M
TELEPHONE MANHOLE (TMH)	T
CATCH BASIN (CB)	
FIRE HYDRANT	
WATER GATE	W
GAS GATE	G
TREE	
SIGN	
DOWNSPOUT	
PARKING METER	P
HAND HOLE	H.H.
LIGHT	
HANDICAPPED PARKING SPACE	
PARKING SPACE COUNT	16
CONCRETE	
BRICK	
1' CONTOUR	
5' CONTOUR	
PROPERTY LINE	
ABUTTERS LINE	
BUILDING	
CURB	
WATER LINE	
SEWER LINE	
DRAIN LINE	
GAS LINE	
ELECTRIC LINE	
TELEPHONE LINE	
FINISHED FLOOR ELEVATION	FFE
CONCRETE	CONC.
GRANITE	GRAN.
BOTTOM CENTER	(BC)
TOP OF WATER	TW
INVERT	INV.
NOW OR FORMERLY	N/F
BOOK	BK.
PAGE	PG.

BENCHMARK SUMMARY

TBM #	DESCRIPTION	ELEV.
1	X-CUT ON RIM OF TELEPHONE MANHOLE	35.76
2	X-CUT ON LIGHT POLE BASE BY #449	37.58

PARKING SUMMARY

STANDARD STALLS	14
HANDICAPPED STALLS	0
TOTAL STALLS	14

UTILITY STATEMENT

THE UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND EXISTING DRAWINGS. ALLEN & MAJOR ASSOCIATES, INC. (A&M) MAKES NO GUARANTEE THAT THE UTILITIES SHOWN HEREON COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. A&M FURTHER DOES NOT WARRANT THAT THE UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED. A&M HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES.

LOCUS REFERENCES

- TOWN OF ARLINGTON ASSESSORS MAP 45, LOT 4-5.A
- RECORD OWNER: 2-14 MEDFORD STREET, LLC
- REGISTERED LAND BOOK 1523, PAGE 99
- L.C.C. NO. 7802C

PLAN REFERENCE

- PLAN 563 OF 1962

NOTES

1. NORTH ARROW IS BASED ON MASSACHUSETTS GRID COORDINATE SYSTEM (MAINLAND ZONE) (NAD 83).
2. BOOK/PAGE AND PLAN REFERENCES ARE TAKEN FROM MIDDLESEX (SOUTH) REGISTRY OF DEEDS IN CAMBRIDGE, MA.
3. VERTICAL DATUM IS NAVD 88.
4. CONTOUR INTERVAL IS ONE FOOT (1').

GRAPHIC SCALE

N:\PROJECTS\2729-03\SURVEY\DRAWINGS\CURRENT\S-2729-03-EC.DWG

WE HEREBY CERTIFY THAT THIS PLAN IS THE RESULT OF AN ACTUAL ON THE GROUND SURVEY PERFORMED ON OR BETWEEN NOVEMBER 16, 2020 AND NOVEMBER 18, 2020.

Aug. 19, 2021

PROFESSIONAL LAND SURVEYOR FOR ALLEN & MAJOR ASSOCIATES, INC.

COMMONWEALTH OF MASSACHUSETTS
NORMAN I. LIPSITZ
No. 28445
PROFESSIONAL LAND SURVEYOR

08/19/21

REV	DATE	DESCRIPTION

APPLICANT/OWNER:
2-14 MEDFORD STREET, LLC
455 MASSACHUSETTS AVENUE, SUITE 1
ARLINGTON, MA 02474

PROJECT:
455-457 MASSACHUSETTS AVENUE
ARLINGTON, MA

PROJECT NO.	2729-03	DATE:	08/19/21
SCALE:	1" = 20'	DWG. NAME:	S-2729-03-EC
DRAFTED BY:	AJR	CHECKED BY:	NIL

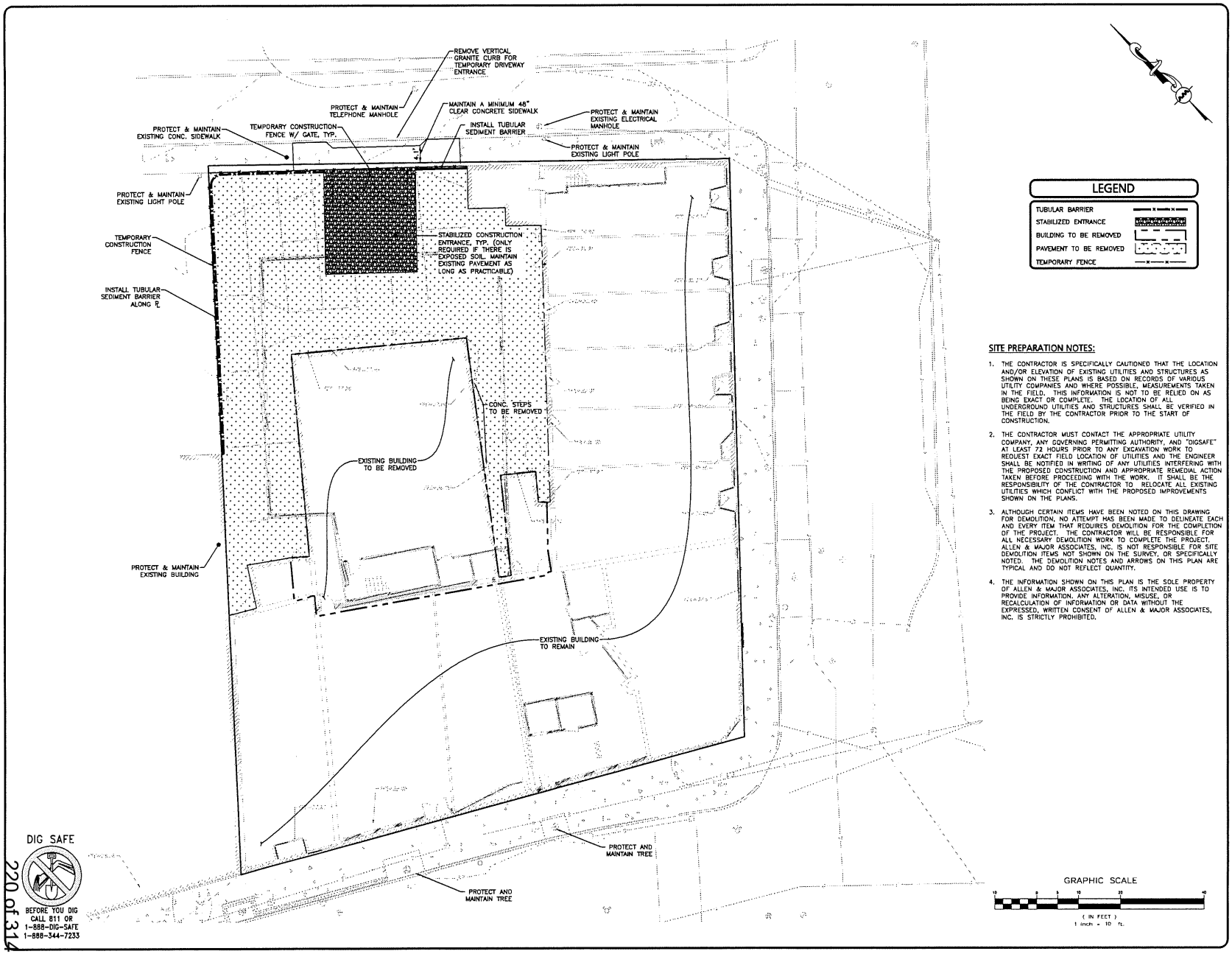
PREPARED BY:

ALLEN & MAJOR ASSOCIATES, INC.
civil engineering • land surveying
environmental consulting • landscape architecture
www.allenmajor.com
100 COMMERCE WAY
WOBURN MA 01801-8201
TEL: (781) 935-6889
FAX: (781) 935-2896

WOBURN, MA • LAKEVILLE, MA • MANCHESTER, NH

DRAWING TITLE: EXISTING CONDITIONS

SHEET No. 1



LEGEND

TUBULAR BARRIER	
STABILIZED ENTRANCE	
BUILDING TO BE REMOVED	
PAVEMENT TO BE REMOVED	
TEMPORARY FENCE	

SITE PREPARATION NOTES:

1. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AND STRUCTURES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF VARIOUS UTILITY COMPANIES AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THIS INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE LOCATION OF ALL UNDERGROUND UTILITIES AND STRUCTURES SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO THE START OF CONSTRUCTION.
2. THE CONTRACTOR MUST CONTACT THE APPROPRIATE UTILITY COMPANY, ANY GOVERNING PERMITTING AUTHORITY, AND "DIGSAFE" AT LEAST 72 HOURS PRIOR TO ANY EXCAVATION WORK TO REQUEST EXACT FIELD LOCATION OF UTILITIES AND THE ENGINEER SHALL BE NOTIFIED IN WRITING OF ANY UTILITIES INTERFERING WITH THE PROPOSED CONSTRUCTION AND APPROPRIATE REMEDIAL ACTION TAKEN BEFORE PROCEEDING WITH THE WORK. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.
3. ALTHOUGH CERTAIN ITEMS HAVE BEEN NOTED ON THIS DRAWING FOR DEMOLITION, NO ATTEMPT HAS BEEN MADE TO DELINEATE EACH AND EVERY ITEM THAT REQUIRES DEMOLITION FOR THE COMPLETION OF THE PROJECT. THE CONTRACTOR WILL BE RESPONSIBLE FOR ALL NECESSARY DEMOLITION WORK TO COMPLETE THE PROJECT. ALLEN & MAJOR ASSOCIATES, INC. IS NOT RESPONSIBLE FOR SITE DEMOLITION ITEMS NOT SHOWN ON THE SURVEY, OR SPECIFICALLY NOTED. THE DEMOLITION NOTES AND ARROWS ON THIS PLAN ARE TYPICAL AND DO NOT REFLECT QUANTITY.
4. THE INFORMATION SHOWN ON THIS PLAN IS THE SOLE PROPERTY OF ALLEN & MAJOR ASSOCIATES, INC. ITS INTENDED USE IS TO PROVIDE INFORMATION. ANY ALTERATION, MISUSE, OR RECALCULATION OF INFORMATION OR DATA WITHOUT THE EXPRESSED, WRITTEN CONSENT OF ALLEN & MAJOR ASSOCIATES, INC. IS STRICTLY PROHIBITED.

COMMONWEALTH OF MASSACHUSETTS BRIAN D. JONES CIVIL No. 42013 REGISTERED PROFESSIONAL ENGINEER

8-20-21

PROFESSIONAL ENGINEER FOR ALLEN & MAJOR ASSOCIATES, INC.

REV	DATE	DESCRIPTION

APPLICANT/OWNER:

2-14 MEDFORD STREET, LLC
455 MASSACHUSETTS AVE, STE 1
ARLINGTON, MA 02474

PROJECT:

455-457
MASSACHUSETTS AVE
ARLINGTON, MA 02476

PROJECT NO.	2729-03	DATE:	8/20/2021
SCALE:	1" = 10'	DWG. NAME:	C2729-03
DESIGNED BY:	JG/ARM	CHECKED BY:	ARM/BDJ

PREPARED BY:

ALLEN & MAJOR ASSOCIATES, INC.
civil engineering • land surveying
environmental consulting • landscape architecture
www.allenmajor.com
100 COMMERCE WAY, SUITE 5
WOBURN, MA 01801
TEL: (781) 935-6880
FAX: (781) 935-2896
WOBURN, MA • LAKEVILLE, MA • MANCHESTER, NH

DRAWING TITLE:	SHEET No.
SITE PREPARATION PLAN	C-101

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SHARED PARKING SUMMARY TABLE

"MIXED-USE" SHALL BE THE SUM OF USES COMPUTED SEPARATELY	REQUIRED PARKING
*FOR MIXED-USE DEVELOPMENT, THE FIRST 3,000 SQUARE FEET OF NON-RESIDENTIAL SPACE IS EXEMPT FROM THE PARKING REQUIREMENTS OF SECTION 6.1 IN THE ARLINGTON ZONING BYLAW	–
SERVICE: 5,377 S.F. 1 PER 300 S.F. → 1 x (5,377-3,000)/300 = 7.9	7.9
BUSINESS: 1,255 S.F. 1 PER 500 S.F. → 1 x 1,255/500 = 2.5	2.5
OTHER RETAIL: 1,951 S.F. 1 PER 300 S.F. → 1 x 1,951/300 = 6.5	6.5
ONE-BEDROOM APARTMENT UNIT (APARTMENT: 13 UNITS) 1.15 PER UNIT → 1.15 x 13 = 15.0	15.0
TOTAL REQUIRED PARKING BASED ON INDIVIDUAL USES:	31.9
TOTAL PARKING PROVIDED:	16*

ADA SPACES REQUIRED:
(1-25) TOTAL PARKING SPACES PROVIDED, 1 SHALL BE THE MINIMUM ADA PARKING PROVIDED, 1 SPACES BEING VAN ACCESSIBLE.

PROVIDED 1 SPACES, 1 BEING VAN ACCESSIBLE.

PARKING TABLE NOTES:
1. SECTION 6.1.10, C: FOR A MIXED-USE DEVELOPMENT THE FIRST 3,000 SF OF NON-RESIDENTIAL SPACE IS EXEMPT FROM THE PARKING REQUIREMENTS OF THIS SECTION 6.1.
2. SECTION 6.1.11, STANDARD PARKING STALLS SHALL BE 8.5'X18', AND DRIVE AISLE WIDTH SHALL BE 24' FOR TWO-WAY TRAFFIC.

* RELIEF REQUESTED FROM THE ARB FOR THE REQUIRED NUMBER OF PARKING STALLS AND THE DRIVE AISLE REDUCED FROM 24' TO 20'.

BICYCLE PARKING SUMMARY TABLE

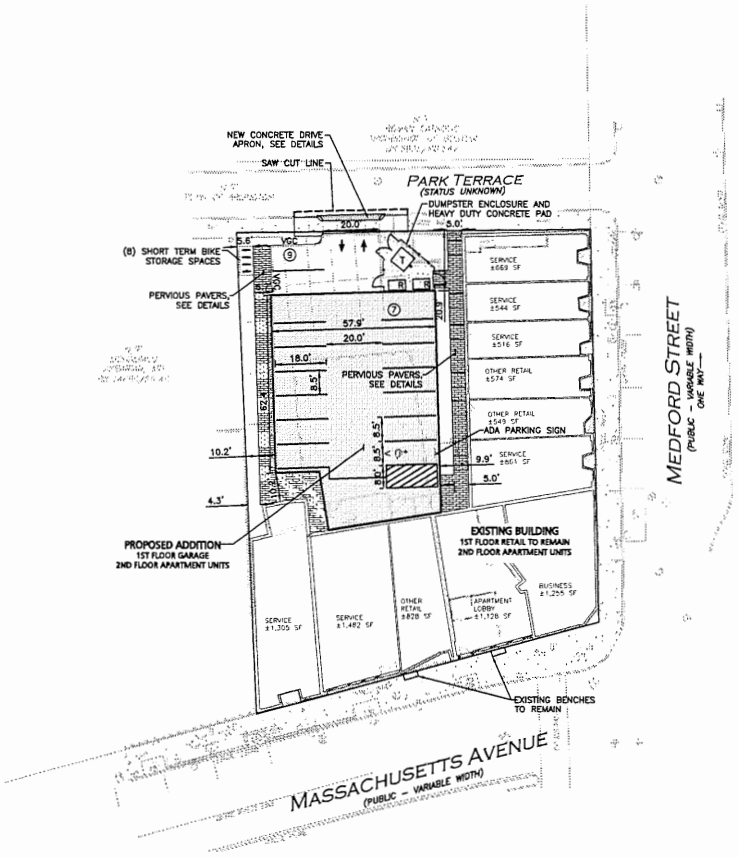
SHORT TERM BICYCLE PARKING (EXTERIOR)

USE	CALCULATION	MIN. REQUIRED	TOTAL PROPOSED
APARTMENT BUILDING	0.1 PER UNIT 13 X 0.1 = 1.3 REQUIRED	1.3	2
OTHER RETAIL OR SERVICE	0.6 PER 1,000 SF (7,328/1,000) X 0.6 = 4.4 REQUIRED	4.4	5
BUSINESS	0.5 PER 1,000 SF (1,255/1,000) X 0.5 = 0.6 REQUIRED	0.6	1
	TOTAL	6.3	8

LONG TERM BICYCLE PARKING (INTERIOR)

USE	CALCULATION	MIN. REQUIRED	TOTAL PROPOSED
APARTMENT BUILDING	1.5 PER UNIT 13 X 1.5 = 19.5 REQUIRED	19.5	20
OTHER RETAIL OR SERVICE	0.1 PER 1,000 SF (7,328/1,000) X 0.1 = 0.7 REQUIRED	0.7	3
BUSINESS	0.3 PER 1,000 SF (1,255/1,000) X 0.3 = 0.4 REQUIRED	0.4	3
	TOTAL	20.6	26

BICYCLE PARKING TABLE NOTES:
1. REQUIRED NUMBER OF SPACES ARE FROM BICYCLE PARKING GUIDELINES, APPENDIX A BIKE PARKING BY-LAW.



LEGEND

PROP. PROPERTY LINE	
SIGN	
BOLLARD	
BUILDING	
BUILDING ARCHITECTURE	
BUILDING INTERIOR WALLS	
CURB	
PARKING STRIPING	
HEAVY DUTY CONCRETE	
SIDEWALK	
ADA ACCESSIBLE RAMP	
PERMEABLE PAVERS	
SAW-CUT LINE	
PARKING COUNT	18
COMPACT PARKING STALL	C

ZONING SUMMARY TABLE

B3-VILLAGE BUSINESS (MIXED-USE <=20,000SF)

ITEM	REQUIRED/ ALLOWED	EXISTING	PROPOSED
MINIMUM LOT AREA	N/A	18,929± SF	18,929± SF
MINIMUM LOT AREA PER UNIT	N/A	N/A	N/A
MINIMUM FRONTAGE	50 FT	124.8± FT MASS. AVE	124.8± FT MASS. AVE
MINIMUM FRONT YARD SETBACK	0 FT	0 FT	0 FT
MINIMUM SIDE YARD SETBACK	0 FT	0 FT	0 FT
MINIMUM REAR YARD SETBACK	(H+L)/6 (1)	0.6 FT	0.6 FT
LANDSCAPED OPEN SPACE	10% (2)	0%	24.1%
USABLE OPEN SPACE	20% (2)	0%	0%
MAXIMUM HEIGHT	60 FT	<35 FT	<35 FT
MAXIMUM HEIGHT STORIES	5	2	2
FLOOR AREA RATIO	1.50	0.98	1.50(2)

ZONING TABLE NOTES:
1. THE REAR SETBACK (H+L)/6, FOR THE NEW ADDITION IS APPROXIMATELY 15.5 FT. (38+57)/6 = 15.5 FT. THE PROPOSED ADDITION HAS BEEN SITUATED OUTSIDE OF THIS SETBACK. THE EXISTING FOUNDATION IS SITUATED 0.6 FT FROM THE REAR PROPERTY LINE. THIS FOUNDATION IS TO BE MAINTAINED.
2. SECTION 5.3.21. SUPPLEMENTAL REQUIREMENTS IN THE BUSINESS AND INDUSTRIAL DISTRICTS, D. FOR MIXED USES AND ANY PERMITTED RESIDENTIAL USE NOT SPECIFICALLY IDENTIFIED IN THE TABLES IN SECTION 5.5.2, THE MINIMUM OPEN SPACE REQUIREMENTS (COMPUTED FROM THE RESIDENTIAL FLOOR AREA ONLY) SHALL BE 10% LANDSCAPED AND 20% USABLE IN THE B1, B2, B2A, B3, AND B4 DISTRICTS, AND 15 PERCENT USABLE IN THE B5 DISTRICT.
3. PROPOSED BUILDING GROSS SQUARE FEET = 28,373 GSF. THE EXISTING LOT AREA IS 18,929 SQUARE FEET. THE FAR = 28,373 GSF / 18,929 SF = 1.50

NOTES

1. WRITTEN DIMENSIONS ON THIS PLAN TAKE PRECEDENCE OVER SCALED DIMENSIONS. THE CONTRACTOR SHALL USE CAUTION WHEN SCALING REPRODUCED PLANS. IN THE EVENT OF A CONFLICT BETWEEN THIS PLAN SET AND ANY OTHER DRAWINGS AND/OR SPECIFICATIONS OR CONDITIONS, THE ENGINEER SHALL BE NOTIFIED BY THE CONTRACTOR. ALL SITE ITEMS SHALL BE LAID OUT AND AS BUILT BY A LICENSED LAND SURVEYOR.
2. THE INFORMATION SHOWN ON THIS PLAN IS THE SOLE PROPERTY OF ALLEN & MAJOR ASSOCIATES, INC. ITS INTENDED USE IS TO PROVIDE INFORMATION. ANY ALTERATION, MISUSE, OR RECALCULATION OF INFORMATION OR DATA WITHOUT THE EXPRESSED, WRITTEN CONSENT OF ALLEN & MAJOR ASSOCIATES, INC. IS STRICTLY PROHIBITED.

GRAPHIC SCALE

(IN FEET)
1 inch = 20 ft.

DIG SAFE

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COMMONWEALTH OF MASSACHUSETTS BRIAN D. JONES CIVIL No. 49212 REGISTERED PROFESSIONAL ENGINEER

8.20.21

PROFESSIONAL ENGINEER FOR ALLEN & MAJOR ASSOCIATES, INC.

REV	DATE	DESCRIPTION

APPLICANT\OWNER:
2-14 MEDFORD STREET, LLC
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ARLINGTON, MA 02474

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DRAWING TITLE: LAYOUT & MATERIALS PLAN

SHEET No. C-102

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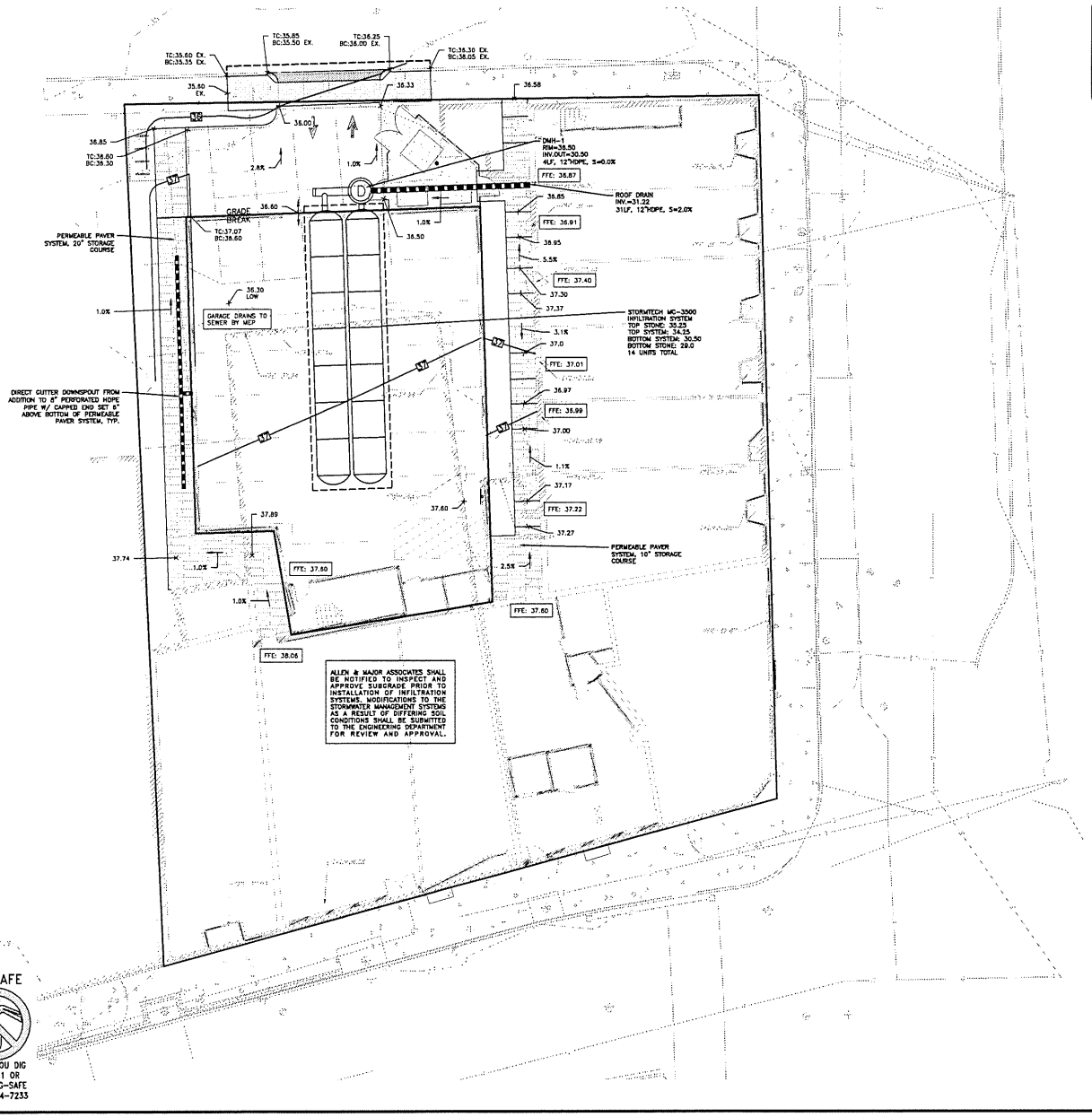

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LEGEND

DRAIN MANHOLE	
CATCH BASIN	
DRAIN LINE	
10' CONTOUR	100
2' CONTOUR	102
SPOT GRADE	78.50
INFILTRATION SYSTEM	

PLAN NOTES:

1. THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR IT'S REPRESENTATIVE. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.
2. CONTRACTOR IS RESPONSIBLE FOR DEMOLITION OF EXISTING STRUCTURES INCLUDING REMOVAL OF ANY EXISTING UTILITIES SERVING THE STRUCTURE. UTILITY CONNECTIONS SHOULD BE COORDINATED WITH THE MEP PRIOR TO CONSTRUCTION.
3. EXISTING SEWER AND WATER SERVICES SHALL BE MAINTAINED AND SERVICE TO ADDITION AND ADDITIONAL UNITS SHALL BE DETERMINED BY MEP. THE CONTRACTOR SHALL BE RESPONSIBLE TO DETERMINE LOCATION AND CAPACITY OF EXISTING SERVICE IS ADEQUATE.
4. EXISTING DRAINAGE STRUCTURES TO REMAIN ARE TO BE INSPECTED AND REPAIRED AS NEEDED, AND EXISTING PIPES TO BE CLEANED OUT TO REMOVE ALL SILT AND DEBRIS.
5. IF ANY EXISTING STRUCTURES TO REMAIN ARE DAMAGED DURING CONSTRUCTION IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO REPAIR AND/OR REPLACE THE EXISTING STRUCTURE AS NECESSARY TO RETURN IT TO EXISTING CONDITIONS OR BETTER.
6. CONTRACTOR SHALL ADJUST AND/OR CUT EXISTING PAVEMENT AS NECESSARY TO ENSURE A SMOOTH FIT AND CONTINUOUS GRADE.
7. CONTRACTOR SHALL ENSURE POSITIVE DRAINAGE AWAY FROM BUILDINGS FOR ALL NATURAL AND PAVED AREAS.
8. THE CONTRACTOR SHALL COORDINATE WITH THE ARCHITECT FOR THE FINAL LOCATIONS OF PROPOSED ROOF DRAINS. LOCATIONS ARE SHOWN HEREON FOR COORDINATION PURPOSES ONLY.
9. WRITTEN DIMENSIONS ON THIS PLAN TAKE PRECEDENCE OVER SCALED DIMENSIONS. THE CONTRACTOR SHALL USE CAUTION WHEN SCALING REPRODUCED PLANS. IN THE EVENT OF A CONFLICT BETWEEN THIS PLAN SET AND ANY OTHER DRAWINGS AND/OR SPECIFICATIONS OR CONDITIONS, THE ENGINEER SHALL BE NOTIFIED BY THE CONTRACTOR.
10. ANY DAMAGE TO PRIVATE OR PUBLIC PROPERTIES DUE TO THE CONTRACTOR'S ACTIVITIES SHALL BE REPAIRED AND RESTORED BY THE CONTRACTOR AT THEIR OWN EXPENSE.
11. ALL PROPERTY MARKERS AND STREET LINE MONUMENTS SHALL BE PROPERLY PROTECTED DURING CONSTRUCTION. ANY DAMAGE TO THESE ITEMS SHALL BE REPAIRED AND RESTORED BY A LAND SURVEYOR LICENSED IN THE COMMONWEALTH OF MASSACHUSETTS AT THE CONTRACTOR'S EXPENSE.
12. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ADDITIONAL BENCHMARK INFORMATION IF REQUIRED. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING AND PROTECTING ALL EXISTING BENCHMARKS. IF IT IS NECESSARY TO RELOCATE A BENCHMARK, IT SHALL BE RELOCATED BY A MASSACHUSETTS LAND SURVEYOR AND DONE SO AT THE CONTRACTOR'S EXPENSE.
13. ALL PERMITS AND APPROVALS NECESSARY FROM AGENCIES GOVERNING THE WORK SHALL BE OBTAINED BY THE CONTRACTOR PRIOR TO THE COMMENCEMENT OF WORK.
14. CONSTRUCTION DURING WET WEATHER OR WINTER CONDITIONS IS TO BE ANTICIPATED AND PROVISIONS TO ADEQUATELY ADDRESS THESE CONDITIONS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
15. ALL CONSTRUCTION SHALL CONFORM TO THE APPLICABLE REGULATIONS AND STANDARDS INCLUDING THE TOWN OF ARLINGTON, MADOT, MADEP, MWRA, MUTCD, AND AASHTO.
16. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR THE MEANS AND METHODS OF CONSTRUCTION AND FOR CONDITIONS AT THE SITE. THESE PLANS, PREPARED BY ALLEN & MAJOR ASSOCIATES DO NOT EXTEND TO OR INCLUDE SYSTEMS PERTAINING TO THE SAFETY OF THE CONSTRUCTION CONTRACTOR OR THEIR EMPLOYEES, AGENTS OR REPRESENTATIVES IN THE PERFORMANCE OF THE WORK, OR THE OWNER'S EMPLOYEES, CUSTOMERS, OR THE GENERAL PUBLIC. THE SEAL OF THE ENGINEER AS INCLUDED IN THE PLAN SET DOES NOT EXTEND TO ANY SUCH SAFETY SYSTEMS THAT MAY NOW OR HEREAFTER BE INCORPORATED INTO THESE PLANS. THE CONSTRUCTION CONTRACTOR SHALL PROVIDE THE APPROPRIATE SAFETY SYSTEMS WHICH MAY BE REQUIRED BY THE U.S. OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA), STATE, AND LOCAL REGULATIONS.
17. THE INFORMATION SHOWN ON THIS PLAN IS THE SOLE PROPERTY OF ALLEN & MAJOR ASSOCIATES, INC. ITS INTENDED USE IS TO PROVIDE INFORMATION. ANY ALTERATION, MISUSE, OR RECALCULATION OF INFORMATION OR DATA WITHOUT THE EXPRESSED, WRITTEN CONSENT OF ALLEN & MAJOR ASSOCIATES, INC. IS STRICTLY PROHIBITED.

GRAPHIC SCALE

10 0 5 10 20 40

(IN FEET)
1 inch = 10 ft.

COMMONWEALTH OF MASSACHUSETTS
BRIAN D. JONES
CIVIL
No. 42212
REGISTERED PROFESSIONAL ENGINEER

8-20-21

PROFESSIONAL ENGINEER FOR
ALLEN & MAJOR ASSOCIATES, INC.

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DRAWING TITLE:	SHEET No.
GRADING & DRAINAGE PLAN	C-103

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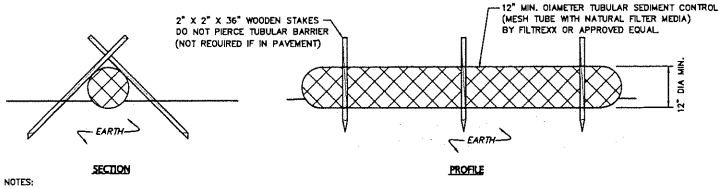
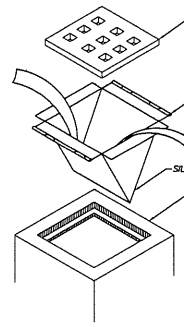


NOTES:

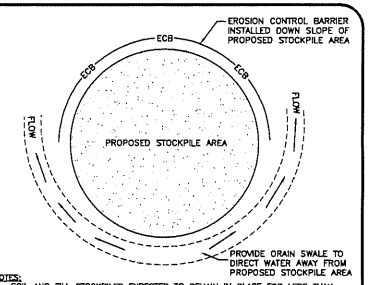
1. ALL MATERIALS TO MEET MANUFACTURERS SPECIFICATIONS.
2. INSTALL WOODEN STAKES IN A CRISS-CROSS PATTERN EVERY 8' ON CENTER.
2. OVERLAP TUBULAR BARRIER SEGMENTS A MINIMUM OF 12".
3. THE CONTRACTOR SHALL MAINTAIN THE TUBULAR BARRIERS IN A FUNCTIONAL CONDITION AT ALL TIMES. THE CONTROLS SHALL BE ROUTINELY INSPECTED BY THE CONTRACTOR.
4. WHERE THE TUBULAR BARRIERS REQUIRE REPAIR OR SEDIMENT REMOVAL, IT WILL BE COMPLETED BY THE CONTRACTOR AT NO ADDITIONAL COST.
5. AT A MINIMUM, THE CONTRACTOR SHALL REMOVE SEDIMENTS COLLECTED AT THE BASE WHEN THEY REACH 1/3 THE EXPOSED HEIGHT OF THE BARRIER.

TUBULAR SEDIMENT BARRIER (1)
NOT TO SCALE



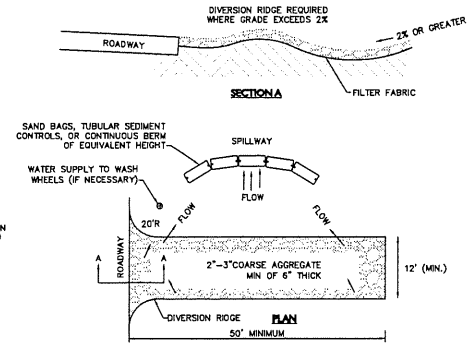
NOTE:
1. INSTALL SILTSACK PER MANUFACTURER'S INSTRUCTIONS AND RECOMMENDATIONS. EMPTY OR REMOVE SEDIMENT FROM SILTSACK WHEN RESTRAINT CORD IS NO LONGER VISIBLE. CLEAN, RINSE, AND REPLACE AS NEEDED.
2. SILT SACKS TO BE INSTALLED DURING CONSTRUCTION OPERATIONS WHEN THE POTENTIAL FOR SEDIMENT TO ENTER EXISTING & PROPOSED BASINS EXISTS.

SILTSACK INLET DETAIL (2)
NOT TO SCALE



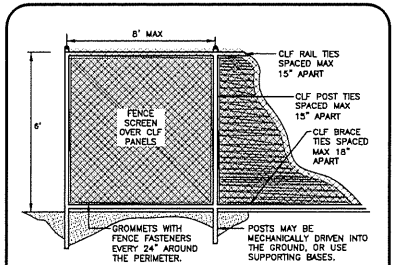
NOTES:
1. SOIL AND FILL STOCKPILES EXPECTED TO REMAIN IN PLACE FOR LESS THAN 90 DAYS SHALL BE COVERED WITH STRAW AND MULCH (AT 100LBS/1,000 SF), OR WITH AN ANCHORED TARP WITHIN 7 DAYS OR PRIOR TO ANY RAINFALL.
2. SOIL AND FILL STOCKPILES EXPECTED TO REMAIN IN PLACE FOR 90 DAYS OR MORE SHALL BE SEEDED WITH WINTER RYE (FOR FALL SEEDING AT 1LB/1,000 SF) OR OATS (FOR SUMMER SEEDING AT 2LB/1,000 SF) AND THEN COVERED WITH STRAW MULCH (AT 100LB/1,000 SF) OR AN ANCHORED TARP WITHIN 7 DAYS OR PRIOR TO ANY RAINFALL.

STOCKPILE PROTECTION DETAIL (3)
NOT TO SCALE



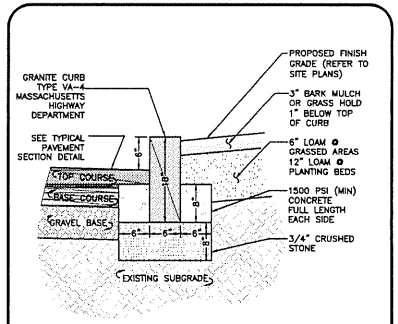
NOTES:
1. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHT-OF-WAYS. THIS MAY REQUIRE TOP DRESSING, REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT.
2. WHEN NECESSARY, WHEELS SHALL BE CLEANED PRIOR TO ENTERING THE PUBLIC RIGHT-OF-WAY.
3. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH CRUSHED STONE THAT DRAINS INTO AN APPROVED SEDIMENT TRAP OR SEDIMENT BASIN.
4. USE SANDBAGS, TUBULAR SEDIMENT CONTROLS, OR OTHER APPROVED METHODS TO CHANNELIZE RUNOFF TO BASIN AS REQUIRED.

TEMPORARY CONSTRUCTION ENTRANCE/EXIT (4)
NOT TO SCALE

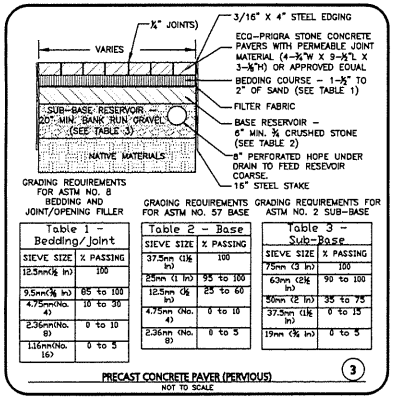


NOTES:
1. SCREEN MATERIAL SHALL BE MADE FROM KNITTED HIGH DENSITY POLYETHYLENE WITH UV ADDITIVES.
2. SCREEN FILAMENT STRENGTH SHALL BE A MINIMUM OF 50LBS/FT.
3. SCREEN MATERIAL BREAK STRENGTH SHALL BE A MINIMUM OF 500 LBS/FT.
4. SCREEN SHADE / WIND BLOCKAGE SHALL BE A MINIMUM OF 85%.
5. SCREEN COLOR SHALL BE GREEN OR BLACK.

TEMPORARY CONSTRUCTION FENCE w/ SCREEN (5)
NOT TO SCALE



VERTICAL GRANITE CURB (6)
NOT TO SCALE



GRADING REQUIREMENTS FOR ASTM NO. 8 BEDDING AND JOINT/OPENING FILLER

Table 1 - Bedding/Joint	
SIEVE SIZE	% PASSING
12.5mm(½ in)	100
9.5mm(⅜ in)	85 to 100
4.75mm(No. 4)	10 to 30
2.36mm(No. 8)	0 to 10
1.16mm(No. 16)	0 to 5

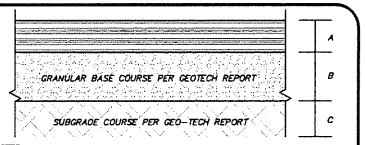
GRADING REQUIREMENTS FOR ASTM NO. 57 BASE

Table 2 - Base	
SIEVE SIZE	% PASSING
37.5mm (1½ in)	100
25mm (1 in)	95 to 100
12.5mm (½ in)	25 to 60
4.75mm (No. 4)	0 to 10
2.36mm (No. 8)	0 to 5

GRADING REQUIREMENTS FOR ASTM NO. 2 SUB-BASE

Table 3 - Sub-Base	
SIEVE SIZE	% PASSING
75mm (3 in)	100
63mm (2½ in)	90 to 100
50mm (2 in)	35 to 75
37.5mm (1½ in)	0 to 15
19mm (¾ in)	0 to 5

PRECAST CONCRETE PAVER (PERVIOUS) (3)
NOT TO SCALE

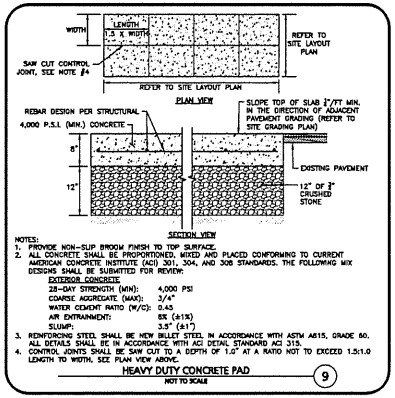


NOTES:
1. SURFACE COURSE, BASE, AND SUBGRADE SHALL BE PER GEOTECHNICAL ENGINEER'S RECOMMENDATIONS FOR LOCAL SOILS OR DRAINAGE CONDITIONS AND/OR METHODS.
2. THIS DETAIL IS APPLICABLE TO ON-SITE PAVING.
3. SUBGRADE SHALL BE COMPACTED TO 95% OF MAXIMUM DRY DENSITY (AS DETERMINED BY ASTM D1557).
4. COORDINATE PAVEMENT DETAIL WITH GEOTECHNICAL ENGINEER.

STANDARD DUTY PAVING
A= 1.5" ASPHALT CONCRETE - SURFACE COURSE (M.3.11.0, TABLE A, TOP COURSE)
1.5" ASPHALT CONCRETE - BINDER COURSE (M.3.11.0, TABLE A, BINDER COURSE)

B= 4" BASE COURSE (MHD 2.01.7)
C= 12" SUBBASE COURSE (MHD 1.03.0, TYPE B)

STANDARDDUTY BITUMINOUS PAVEMENT (7)
NOT TO SCALE



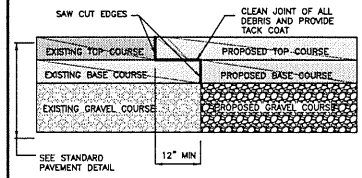
NOTES:
1. PROVIDE NON-SLIP BROOM FINISH TO TOP SURFACE.
2. ALL CONCRETE SHALL BE PROPORTIONED, MIXED AND PLACED CONFORMING TO CURRENT AMERICAN CONCRETE INSTITUTE (ACI) 301, 304, AND 308 STANDARDS. THE FOLLOWING MIX DESIGNS SHALL BE SUBMITTED FOR REVIEW:

EXTERIOR CONCRETE	
28-DAY STRENGTH (MIN):	4,000 PSI
COARSE AGGREGATE (MAX):	3/4"
WATER CEMENT RATIO (W/C):	0.45
AIR ENTRAINMENT:	5% (±1%)
SLUMP:	3.5" (±1")

3. REINFORCING STEEL SHALL BE NEW BILLET STEEL IN ACCORDANCE WITH ASTM A615, GRADE 60. ALL DETAILS SHALL BE IN ACCORDANCE WITH ACI DETAIL STANDARD ACI 315.
4. CONTROL JOINTS SHALL BE SAW CUT TO A DEPTH OF 1.0" AT A RATIO NOT TO EXCEED 1.5:1.0 LENGTH TO WIDTH, SEE PLAN VIEW ABOVE.

HEAVY DUTY CONCRETE PAD (9)
NOT TO SCALE

NOTE:
TACK COAT - PROVIDE EMULSIFIED ASPHALT WHICH CONFORMS TO THE REQUIREMENTS OF THE STATE SPECIFICATIONS, DILUTED WITH ONE PART WATER TO ONE ONE PART ASPHALT FOLLOWING AASHTO M140/ASTM D997, OR AASHTO M208/ASTM D2397, SS-1H, CSS-1, OR CSS-1H.



PAVEMENT KEY CUT DETAIL (10)
NOT TO SCALE

COMMONWEALTH OF MASSACHUSETTS
BRIAN D. JONES
CIVIL
No. 42012
REGISTERED PROFESSIONAL ENGINEER

8.20.21

PROFESSIONAL ENGINEER FOR
ALLEN & MAJOR ASSOCIATES, INC.

REV	DATE	DESCRIPTION

APPLICANT/OWNER:
2-14 MEDFORD STREET, LLC
455 MASSACHUSETTS AVE, STE 1
ARLINGTON, MA 02474

PROJECT:
455-457
MASSACHUSETTS AVE
ARLINGTON, MA 02476

PROJECT NO.	2729-03	DATE:	8/20/2021
SCALE:	AS SHOWN	DWG. NAME:	C2729-03
DESIGNED BY:	JG/ARM	CHECKED BY:	ARM/BDJ

PREPARED BY:

ALLEN & MAJOR ASSOCIATES, INC.
civil engineering • land surveying
environmental consulting • landscape architecture
www.allenmajor.com
100 COMMERCE WAY, SUITE 5
WOBURN, MA 01801
TEL: (781) 933-6860
FAX: (781) 933-2896
WOBURN, MA ◆ LAFAYETTE, NJ ◆ MANCHESTER, NH

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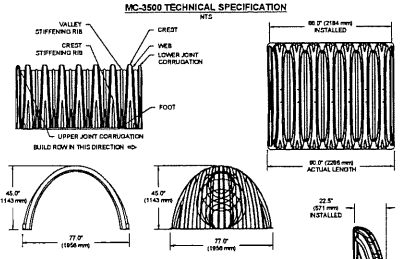
DRAWING TITLE: DETAILS

SHEET No. C-501

R:\PROJECTS\2729-03\CIVIL\DRAWINGS\CURRENT\C-2729-03_DETAILS.DWG

MC-3500 TECHNICAL SPECIFICATION

NTS



NOMINAL CHAMBER SPECIFICATIONS

SIZE (W x H x INSTALLED LENGTH)	77.0" X 45.0" X 86.0"	(1956 mm X 1143 mm X 2184 mm)
CHAMBER STORAGE	109.9 CUBIC FEET	---- - ------
MINIMUM INSTALLED STORAGE*	179.9 CUBIC FEET	---- - ------
WEIGHT	135.0 lbs.	(61.2 kg)

NOMINAL END CAP SPECIFICATIONS

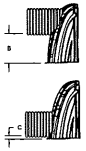
SIZE (W x H x INSTALLED LENGTH)	77.0" X 45.0" X 22.5"	(1956 mm X 1143 mm X 571 mm)
END CAP STORAGE	14.9 CUBIC FEET	---- - ------
MINIMUM INSTALLED STORAGE*	46.0 CUBIC FEET	---- - ------
WEIGHT	50.0 lbs.	(22.7 kg)

*ASSUMES 12" (305 mm) STONE ABOVE, 9" (229 mm) STONE FOUNDATION AND BETWEEN CHAMBERS, 12" (305 mm) STONE PERIMETER IN FRONT OF END CAPS AND 40% STONE POROSITY.

STUBS AT BOTTOM OF END CAP FOR PART NUMBERS ENDING WITH "B"
STUBS AT TOP OF END CAP FOR PART NUMBERS ENDING WITH "T"

PART #	STUB	B	C
MC3500EPP06T	6" (150 mm)	33.21" (844 mm)	—
MC3500EPP06B		—	0.69" (17 mm)
MC3500EPP06T	6" (200 mm)	31.16" (791 mm)	—
MC3500EPP06B			0.81" (21 mm)
MC3500EPP10T	10" (250 mm)	29.94" (759 mm)	—
MC3500EPP10B			0.93" (24 mm)
MC3500EPP12T	12" (300 mm)	26.36" (670 mm)	—
MC3500EPP12B			1.35" (34 mm)
MC3500EPP15T	15" (375 mm)	23.38" (594 mm)	—
MC3500EPP15B		—	1.59" (34 mm)
MC3500EPP18TC	18" (450 mm)	20.03" (509 mm)	—
MC3500EPP18BC		—	1.77" (45 mm)
MC3500EPP24TC	24" (600 mm)	14.48" (368 mm)	—
MC3500EPP24BC		—	2.00" (52 mm)
MC3500EPP30BC	30" (750 mm)	—	—

NOTE: ALL DIMENSIONS ARE NOMINAL



CUSTOM PRECORED INVERTS ARE AVAILABLE UPON REQUEST. INVENTORIED MANIFOLDS INCLUDE 12-24" (305-600 mm) SIZE ON SEIZE AND 15-48" (375-1200 mm) ECCENTRIC MANIFOLDS
CUSTOM INVERT LOCATIONS ON THE MC-3500 END CAP CUT IN THE FIELD ARE NOT RECOMMENDED FOR PIPE SIZES GREATER THAN 10" (250 mm)
THE INVERT LOCATION IN COLUMN "B" ARE THE HIGHTEST POSSIBLE FOR THE PIPE SIZE.

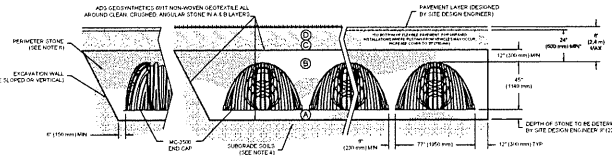
CHAMBER SYSTEM TECHNICAL SPECIFICATIONS (1)

NOT TO SCALE

ACCEPTABLE FILL MATERIALS: STORMTECH MC-3500 CHAMBER SYSTEMS

MATERIAL LOCATION	DESCRIPTION	AASHTO MATERIAL CLASSIFICATIONS	COMPACTION / DENSITY REQUIREMENT
D	**FINAL FILL:** FILL MATERIAL FOR LAYER 'D' STARTS FROM THE TOP OF THE 'C' LAYER TO THE BOTTOM OF FLEXIBLE PAVEMENT OR UNPAVED FINISHED GRADE ABOVE. NOTE THAT PAVEMENT SUBBASE MAY BE PART OF THE 'D' LAYER	N/A	PREPARE PER SITE DESIGN ENGINEER'S PLANS. PAVED INSTALLATIONS MAY HAVE STRINGENT MATERIAL AND PREPARATION REQUIREMENTS
C	**INITIAL FILL:** FILL MATERIAL FOR LAYER 'C' STARTS FROM THE TOP OF THE EMBEDMENT STONE (IF LAYER 'B') TO 24" (600 mm) ABOVE THE TOP OF THE CHAMBER. NOTE THAT PAVEMENT SUBBASE MAY BE A PART OF THE 'C' LAYER	GRANULAR WELL-GRADED SOIL/AGGREGATE MIXTURES, <35% FINES OR PROCESSED AGGREGATE	BEGIN COMPACTING AFTER 24" (600 mm) OF MATERIAL OVER THE CHAMBERS IS REACHED. COMPACT ADDITIONAL LAYERS IN 12" (300 mm) MAX LIFTS TO A MIN. 95% PROCTOR DENSITY FOR WELL GRADED MATERIAL AND 95% RELATIVE DENSITY FOR PROCESSED AGGREGATE MATERIALS
	MOST PAVEMENT SUBBASE MATERIALS CAN BE USED IN LIEU OF THIS LAYER	AASHTO M145: 1, 2, 357, 4, 467, 5, 56, 57, 6, 68, 6a, 7, 78, 8, 89, 9, 10	
B	**EMBEDMENT STONE:** FILL SURROUNDING THE CHAMBERS FROM THE FOUNDATION STONE (IF LAYER 'E') TO THE 'C' LAYER ABOVE	CLEAN, CRUSHED, ANGULAR STONE	AASHTO M43: 3, 4	NO COMPACTION REQUIRED
A	**FOUNDATION STONE:** FILL BELOW CHAMBERS FROM THE SUBGRADE UP TO THE FOOT (BOTTOM) OF THE CHAMBER	CLEAN, CRUSHED, ANGULAR STONE	AASHTO M43: 3, 4	PLATE COMPACTOR ROLL TO ACHIEVE A FLAT ------

PLEASE NOTE:
1. THE LISTED AASHTO DESIGNATIONS ARE FOR GRADATIONS ONLY. THE STONE MUST ALSO BE CLEAN, CRUSHED, ANGULAR. FOR EXAMPLE, A SPECIFICATION FOR #4 STONE WOULD STATE "CLEAN, CRUSHED, ANGULAR NO. 4 (AASHTO M43) STONE".
2. STORMTECH COMPACTION REQUIREMENTS ARE MET FOR 'A' LOCATION MATERIALS WHEN PLACED AND COMPACTED IN 9" (230 mm) MAX LIFTS USING TWO FULL COVERAGES WITH A VIBRATORY COMPACTOR.
3. WHERE INFILTRATION SURFACES MAY BE COMPROMISED BY COMPACTION, FOR STANDARD DESIGN LOAD CONDITIONS, A FLAT SURFACE MAY BE ACHIEVED BY RAKING OR DRAGGING WITHOUT COMPACTION EQUIPMENT. FOR SPECIAL LOAD DESIGNS, CONTACT STORMTECH FOR COMPACTION REQUIREMENTS.



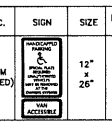
NOTES:
1. MC-3500 CHAMBERS SHALL CONFORM TO THE REQUIREMENTS OF ASTM F2418 "STANDARD SPECIFICATION FOR POLYPROPYLENE (PP) CORRUGATED WALL STORMWATER COLLECTION CHAMBERS".
2. MC-3500 CHAMBERS SHALL BE DESIGNED IN ACCORDANCE WITH ASTM F2787 "STANDARD PRACTICE FOR STRUCTURAL DESIGN OF THERMOPLASTIC CORRUGATED WALL STORMWATER COLLECTION CHAMBERS".
3. "ACCEPTABLE FILL" MATERIAL IN TABLE ABOVE PROVIDES MATERIAL LOCATIONS, DESCRIPTIONS, GRADATIONS, AND COMPACTION REQUIREMENTS FOR FOUNDATION, EMBEDMENT, AND FILL MATERIALS.
4. THE SITE DESIGN ENGINEER IS RESPONSIBLE FOR ASSESSING THE BEARING RESISTANCE (ALLOWABLE BEARING CAPACITY) OF THE SUBGRADE SOILS, AND THE DEPTH OF FOUNDATION STONE WITH CONSIDERATION FOR THE RANGE OF EXPECTED SOIL MOISTURE CONDITIONS.
PERIMETER STONE MUST BE EXTENDED HORIZONTALLY TO THE EXCAVATION WALL FOR BOTH VERTICAL AND SLOPED EXCAVATION WALLS.
5. ONCE LAYER 'C' IS PLACED, ANY SOIL/MATERIAL CAN BE PLACED IN LAYER 'D' UP TO THE FINISHED GRADE. MOST PAVEMENT SUBBASE SOILS CAN BE USED TO REPLACE THE MATERIAL REQUIREMENTS OF LAYER 'C' OR 'D' AT THE SITE DESIGN ENGINEER'S DISCRETION.

CHAMBER SYSTEM TYPICAL CROSS SECTION DETAIL (2)

NOT TO SCALE

STORMWATER CHAMBER SPECIFICATIONS

1. CHAMBERS SHALL BE STORMTECH MC-3500 OR APPROVED EQUAL.
2. CHAMBERS SHALL BE MANUFACTURED FROM VIRGIN POLYPROPYLENE OR POLYETHYLENE RESINS TESTED USING ASTM STANDARDS.
3. CHAMBERS SHALL BE DESIGNED IN ACCORDANCE WITH ASTM F2787, "STANDARD PRACTICE FOR STRUCTURAL DESIGN OF THERMOPLASTIC CORRUGATED WALL STORMWATER COLLECTION CHAMBERS".
4. CHAMBER ROWS SHALL PROVIDE CONTINUOUS, UNOBSTRUCTED INTERNAL SPACE WITH NO INTERNAL SUPPORT PANELS.
5. ONLY CHAMBERS THAT ARE APPROVED BY THE ENGINEER WILL BE ALLOWED.
6. CHAMBERS SHALL BE PRODUCED AT AN ISO 9001 CERTIFIED MANUFACTURING FACILITY.
7. ALL DESIGN SPECIFICATIONS FOR CHAMBERS SHALL BE IN ACCORDANCE WITH THE MANUFACTURER'S LATEST DESIGN MANUAL.
8. THE INSTALLATION OF CHAMBERS SHALL BE IN ACCORDANCE WITH THE MANUFACTURER'S LATEST INSTALLATION INSTRUCTIONS.

DESC.	SIGN	SIZE	MOUNTING HEIGHT	DESCRIPTION	REFLEC-TORIZED
R7-8M (MODIFIED)		12" x 26"	7' – 0"	WHITE TEXT ON BLUE FIELD WITH WHITE BORDER	YES

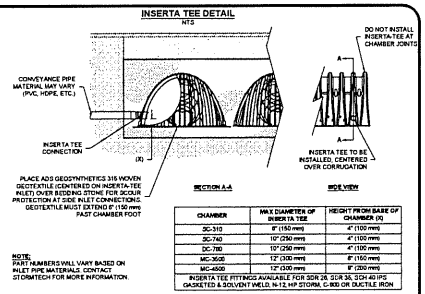
1. TRAFFIC AND SAFETY SIGNAGE SHALL COMPLY WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) STANDARDS.
2. MOUNTING HEIGHT IS DEFINED AS THE DISTANCE FROM THE BOTTOM OF THE SIGN TO THE NEAR EDGE OF THE PAVEMENT.

SIGN TABLE (3)

NOT TO SCALE

INSERTA TEE DETAIL

NTS



CHAMBER	MAX DIAMETER OF INSERTA TEE	HEIGHT FROM BASE OF CHAMBER (R)
SC-310	6" (150 mm)	4" (100 mm)
SC-740	10" (250 mm)	4" (100 mm)
DC-780	10" (250 mm)	4" (100 mm)
MC-3500	12" (300 mm)	6" (150 mm)
MC-4500	12" (300 mm)	6" (150 mm)

INSERTA TEE FITTINGS AVAILABLE FOR SDR 26, SDR 35, SCH 40 IPS GASKETED & SOLVENT WELD, N-12, HP STORM, C-900 OR DUCTILE IRON

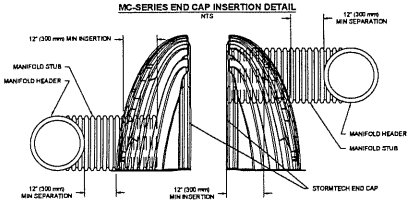
NOTE:
PART NUMBERS WILL VARY BASED ON INLET PIPE MATERIALS. CONTACT STORMTECH FOR MORE INFORMATION.

INSERTA TEE CONNECTION DETAIL (4)

NOT TO SCALE

MC-SERIES END CAP INSERTION DETAIL

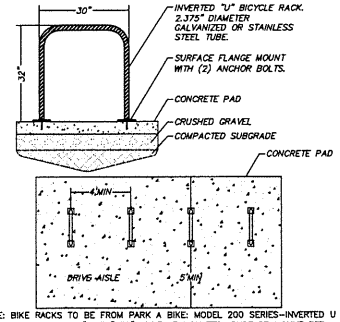
NTS



NOTE: MANIFOLD STUB MUST BE LAID HORIZONTAL FOR A PROPER FIT IN END CAP OPENING.

CHAMBER SYSTEM TYPICAL END CAP DETAIL (5)

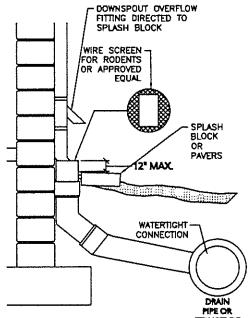
NOT TO SCALE



NOTE: BIKE RACKS TO BE FROM PARK A BIKE: MODEL 200 SERIES–INVERTED U BIKE RACKS HR202 30"X2.38"X32" SURFACE MOUNTED. SURFACE MOUNT PER MANUFACTURER'S RECOMMENDATIONS. FINISH SELECTED BY OWNER. PROVIDE SHOP DRAWINGS FOR APPROVAL PRIOR TO ORDERING.

BICYCLE RACK DETAIL (6)

NOT TO SCALE



NOTES:

1) A WATERTIGHT CONNECTION SHALL BE MAINTAINED FROM ANY TRANSITION FROM SCH. 40 PVC PIPE TO ANY OTHER PIPE TYPE.

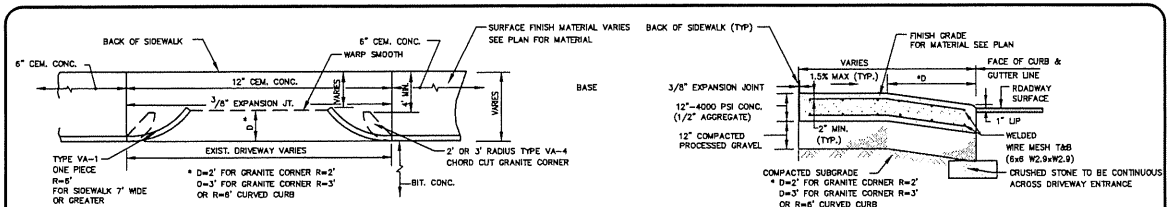
2) SITEWORK CONTRACTOR SHALL BE RESPONSIBLE FOR ALL WORK TO AND INCLUDING THE RODENT SCREEN. BUILDING CONTRACTOR SHALL BE RESPONSIBLE FOR THE CONNECTION AT THE POINT OF THE RODENT SCREEN.

3) DOWNSPOUTS SHALL BE CONNECTED IN THE MANNER SHOWN AND CONVEYED TO THE NEAREST STORM DRAIN.

4) SUBMIT SHOP DRAWINGS FOR DOWNSPOUT OVERFLOW, SPLASH BACK, RODENT SCREEN, FITTINGS AND PIPE.

DOWNSPOUT (7)

NOT TO SCALE



TYPICAL COMMERCIAL DRIVEWAY APRON (8)

NOT TO SCALE

8.20.21

PROFESSIONAL ENGINEER FOR
ALLEN & MAJOR ASSOCIATES, INC.

REV	DATE	DESCRIPTION

APPLICANT\OWNER:
2-14 MEDFORD STREET, LLC
455 MASSACHUSETTS AVE, STE 1
ARLINGTON, MA 02474

PROJECT:
455-457
MASSACHUSETTS AVE
ARLINGTON, MA 02476

PROJECT NO.	2729-03	DATE:	8/20/2021
SCALE:	AS SHOWN	DWG. NAME:	C2729-03
DESIGNED BY:	JG/ARM	CHECKED BY:	ARM/BDJ

PREPARED BY:

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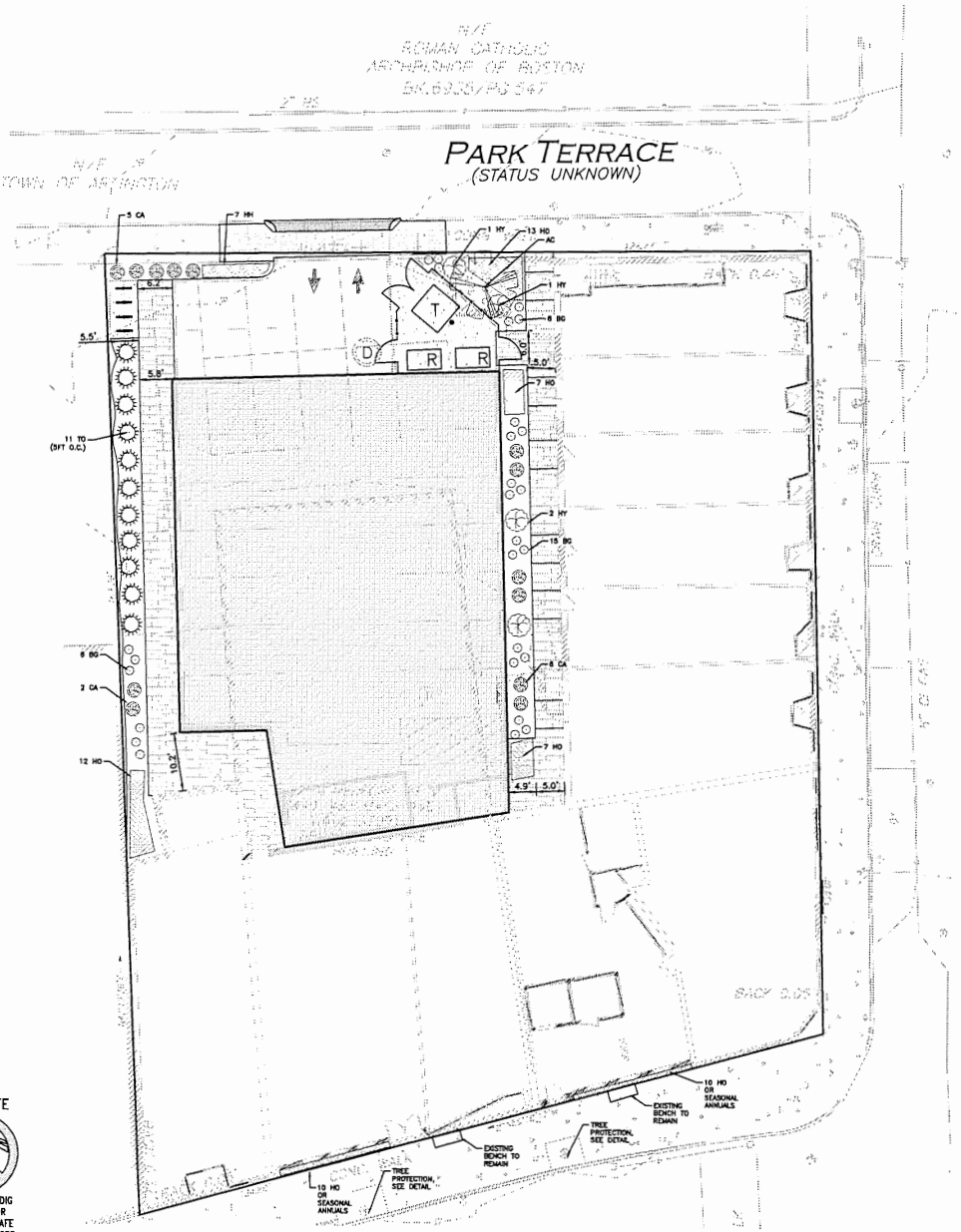
civil engineering • land surveying
environmental consulting • landscape architecture
www.allenmajor.com
100 COMMERCE WAY, SUITE 5
WOBURN, MA 01801
TEL: (781) 935-6889
FAX: (781) 935-2896
WOBURN, MA • LAKEVILLE, MA • MANCHESTER, NH

DRAWING TITLE: DETAILS

SHEET No. C-502



DIG SAFE

BEFORE YOU DIG
CALL 811 OR
1-888-DIG-SAFE
1-888-344-7233

LEGEND

- DECIDUOUS TREE
- EVERGREEN TREE
- FLOWERING TREE
- SHRUBS
- MULCH BED
- PERENNIALS/GROUNDCOVER

PLANTING SCHEDULE-TREES, SHRUBS, GROUNDCOVERS & PERENNIALS

KEY	QUANTITY	BOTANICAL NAME	COMMON NAME	MIN. SIZE	SPACING	COMMENTS
EVERGREEN TREES						
TO	11	THUJA O. 'WINTERGREEN'	WINTERGREEN ARBORVITAE	6–7' HT	AS SHOWN	B&B
FLOWERING TREES						
AC	1	AMELANCHIER CANADENSIS	SHADBLOW SERVICEBERRY	2.5–3" CAL.	AS SHOWN	B&B
SHRUBS/PERENNIALS						
CA	13	CORNUS ALBA 'IVORY HALO'	IVORY HALO DOGWOOD	#3	AS SHOWN	POT
BG	27	BUXUS 'GREEN MOUNTAIN'	GREEN MOUNTAIN BOXWOOD	#2	AS SHOWN	POT
HY	4	HYDRANGEA ARBORESCENS INCREDIBALL	INCREDIBALL HYDRANGEA	#5	AS SHOWN	POT
HO	46	HOSTA 'JUNE'	JUNE HOSTA	#1	24" O.C	STAGGERED
HH	7	HEMEROCALLIS 'BIG TIME HAPPY'	BIG TIME HAPPY DAYLILLY	#1	24" O.C	STAGGERED

NOTES:

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2. ALL LANDSCAPED AREAS WITH SHRUBS, TREES, AND PERENNIALS TO HAVE 18" MINIMUM DEPTH OF TOPSOIL. EIGHTEEN INCHES OF TOPSOIL AROUND TREES AND SHRUBS DOES NOT INCLUDE AMENDED PLANTING SOIL WITHIN TREE / SHRUB PIT FOR FULL DEPTH OF ROOTBALLS. SEE PLANTING DETAILS FOR PLANTING DEPTH AT SHRUBS AND TREES. ALL AREAS OF LOAM AND SEED OR LOAM & SOD TO HAVE 6" MINIMUM DEPTH OF TOPSOIL. TOPSOIL TO BE TESTED BY CONTRACTOR, AND APPROVED BY A&M PRIOR TO PURCHASE AND OR PLACEMENT. GENERAL CONTRACTOR, DEMOLITION CONTRACTOR, AND LANDSCAPE CONTRACTOR TO COORDINATE PROPER DEPTH OF EXISTING MATERIAL REMOVAL ACROSS SITE SO THAT 18" MINIMUM AND 6" MINIMUM DEPTHS OF PROPOSED TOPSOIL NOTED ABOVE ARE MET AT NO ADDITIONAL COST TO OWNER. SEE TOPSOIL DETAIL.

GRAPHIC SCALE

10 0 5 10 20 40

(IN FEET)
1 inch = 10 ft.

MASSACHUSETTS BETHANY C. DUNN REGISTERED LANDSCAPE ARCHITECT No. 4571 3-30-22

REGISTERED LANDSCAPE ARCHITECT FOR ALLEN & MAJOR ASSOCIATES, INC.

REV	DATE	DESCRIPTION

APPLICANT\OWNER:

2-14 MEDFORD STREET, LLC
455 MASSACHUSETTS AVE, STE 1
ARLINGTON, MA 02474

PROJECT:

455-457
MASSACHUSETTS AVE
ARLINGTON, MA 02476

PROJECT NO.	2729-03	DATE:	8/20/2021
SCALE:	1" = 10'	DWG. NAME:	C2729-03
DESIGNED BY:	JG, BCD	CHECKED BY:	ARM/BDJ

PREPARED BY:

ALLEN & MAJOR ASSOCIATES, INC.
civil engineering • land surveying
environmental consulting • landscape architecture
www.allenmajor.com
100 COMMERCE WAY, SUITE 5
WOBURN, MA 01801
TEL: (781) 933-0880
FAX: (781) 933-8936

WOBURN, MA • LAKEVILLE, MA • MANCHESTER, NH

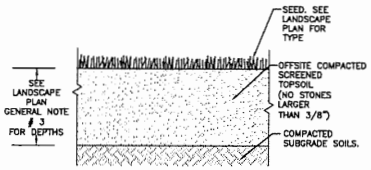
DRAWING TITLE: LANDSCAPE PLAN

SHEET No. L-101

R:\PROJECTS\2729-03\CIVIL\DRAWINGS\CURRENT\C-2729-03_LANDSCAPING.DWG

LANDSCAPE NOTES

1. ALL WORK SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE TOWN OF ARLINGTON, MA.
2. PLANTING PLAN IS DIAGRAMMATIC IN NATURE. FINAL PLACEMENT OF PLANTS TO BE APPROVED BY THE LANDSCAPE ARCHITECT IN THE FIELD.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING ALL UTILITY COMPANIES, ANY PERMITTING AGENCIES, AND "DIG-SAFE" (1-888-344-7233) AT LEAST 72 HOURS IN ADVANCE OF ANY WORK THAT WILL REQUIRE EXCAVATION. CONTRACTOR SHALL NOTIFY THE OWNERS REPRESENTATIVE OF NAY CONFLICTS IN WRITING.
4. NO PLANT MATERIAL SHALL BE INSTALLED UNTIL ALL GRADING AND CONSTRUCTION HAS BEEN COMPLETED IN THE IMMEDIATE AREA, ANY TREES NOTED AS "SEAL OR SELECTED SPECIMEN" SHALL BE TAGGED AND SEALED BY THE LANDSCAPE ARCHITECT.
5. ALL TREES SHALL BE BALLED AND BURLAPPED (B&B) UNLESS OTHERWISE NOTED OR APPROVED BY THE OWNER'S REPRESENTATIVE AND LANDSCAPE ARCHITECT.
6. CONTRACTOR SHALL VERIFY QUANTITIES SHOWN ON PLANT LIST. QUANTITIES SHOWN ON PLANS SHALL GOVERN OVER PLANT LIST.
7. ANY PROPOSED PLANT SUBSTITUTIONS MUST BE APPROVED IN WRITING BY OWNER'S REPRESENTATIVE AND LANDSCAPE ARCHITECT.
8. ALL PLANT MATERIALS INSTALLED SHALL MEET THE GUIDELINES ESTABLISHED BY THE STANDARDS FOR NURSERY STOCK PUBLISHED BY THE AMERICAN ASSOCIATION OF NURSERYMEN.
9. ALL PLANT MATERIALS SHALL BE GUARANTEED FOR ONE YEAR FOLLOWING DATE OF ACCEPTANCE.
10. ALL DISTURBED AREAS NOT OTHERWISE NOTED SHALL RECEIVE 6" OF SUITABLE LOAM & SEED LAWNS WITH 3:1 OR GREATER SLOPES SHALL BE PROTECTED WITH AN EROSION CONTROL BLANKET.
11. ANY FALL TRANSPLANTING HAZARD PLANTS SHALL BE OUG IN THE SPRING AND STORED FOR FALL PLANTING.
12. TREES SHALL HAVE A MINIMUM CALIPER AS INDICATED ON THE PLANTING SCHEDULE TAKEN ONE FOOT ABOVE THE ROOT CROWN.
13. ALL PLANT BEDS AND TREE SAUCERS TO RECEIVE 3" OF PINE BARK MULCH. GROUND COVER AREAS SHALL RECEIVE 1" OF PINE BARK MULCH
14. ALL DECIDUOUS TREES ADJACENT TO WALKWAYS AND ROADWAYS SHALL HAVE A BRANCHING PATTERN TO ALLOW FOR A MINIMUM OF 7' OF CLEARANCE BETWEEN THE GROUND AND THE LOWEST BRANCH.
15. ALL TREE STAKES SHALL BE STAINED DARK BROWN.
16. CONTRACTOR RESPONSIBLE FOR WATERING, AND RESEEDING OF BARE SPOTS UNTIL A UNIFORM STAND OF VEGETATION IS ESTABLISHED AND ACCEPTED.
17. ALL PARKING ISLANDS PLANTED WITH SHRUBS SHALL HAVE 24" OF TOP SOIL. FINISH GRADE SHALL BE EQUAL TO THE TOP OF CURB.
18. SOIL SAMPLES, TESTS, AND SHOP DRAWINGS SHALL BE PROVIDED TO THE LANDSCAPE ARCHITECT OR THE OWNER FOR APPROVAL PRIOR TO CONSTRUCTION.
19. AN MINIMUM 18" WIDE BARRIER OF 1" GRAY OR TAN PEASTONE SHALL BE INSTALLED IN ALL PLANT BEDS WHICH ABUT THE BUILDINGS WITH FLAMMABLE EXTERIOR MATERIAL. NO MULCH IS ALLOWED WITHIN 18" OF THIS BUILDING MATERIAL PER THE LATEST EXECUTIVE OFFICE OF PUBLIC SAFETY AND SECURITY DEPARTMENT OF FIRE SERVICES REGULATION (527 CMR 17.00). INSTALL 6" DEEP OF PEASTONE WITH MIRAFI WEED FABRIC BENEATH AND STEEL EDGING BETWEEN THE PEASTONE AND ADJACENT MULCH BED.
20. ALL PROPOSED LANDSCAPE AREAS INCLUDING MOWED LAWNS, TREES, SHRUB BEDS, AND PERENNIALS SHALL BE PROVIDED WITH WATER EFFICIENT UNDERGROUND IRRIGATION. DESIGN AND INSTALLATION OF IRRIGATION SYSTEM TO BE PERFORMED BY AN APPROVED IRRIGATION DESIGN BUILD CONTRACTOR OR BY AN APPROVED EQUAL, TO BE DETERMINED BY THE OWNERS REPRESENTATIVE AND LANDSCAPE ARCHITECT. IRRIGATION SYSTEM IS TO BE DESIGNED FOR EFFICIENT WATER USAGE INCLUDING: USE OF DRIP IRRIGATION FOR SHRUBS AND PERENNIALS, IRRIGATION SYSTEM WITH HEAD-TO-HEAD COVERAGE, A CENTRAL SHUT-OFF VALVE, AND A RAIN SENSOR TO SHUT OFF IRRIGATION DURING RAIN EVENTS.

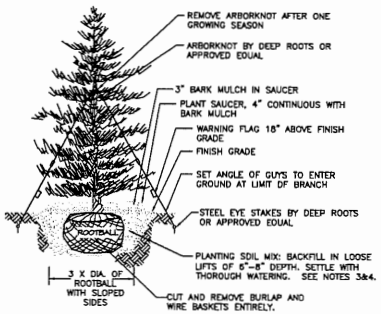


TEXTURE CLASS	% OF TOTAL WEIGHT
SAND	45% – 65%
SILT	15% – 35%
CLAY	5% – 20%

SIEVE	% PASSING
3/8"	100
NO. 4	85–100
NO. 40	60–85
NO. 100	38–60
NO. 200	10–35
20 um	LESS THAN 5%

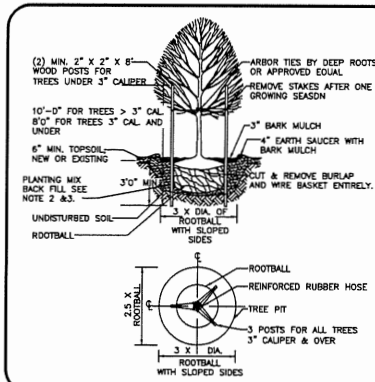
NOTES:
1. TOP OF LOAM (TOPSOIL) IS FINISH GRADE.
2. ALL TOPSOIL (BOTH ONSITE AND OFFSITE SOURCES) SHALL BE COMPOSED OF A NATURAL, FERTILE, FRIABLE SOIL TYPICAL OF CULTIVATED TOPSOILS OF THE LOCALITY. OFFSITE SOIL SHALL BE SUITABLE FOR THE GERMINATION OF SEEDS AND SUPPORT OF VEGETATIVE GROWTH, WITH ADDITIVES, IF REQUIRED, TO ACHIEVE PARTICLE DISTRIBUTION AND ORGANIC CONTENT BELOW. TOPSOIL SHALL BE TAKEN FROM A WELL-DRAINED, ARIABLE SITE, FREE OF SUBSOIL, LARGE STONES, EARTH CLODS, STICKS, STUMPS, CLAY LUMPS, ROOTS, OTHER OBJECTIONABLE, EXTRANEOUS MATTER OR DEBRIS NOR CONTAIN TOXIC SUBSTANCES.
3. THE CONTRACTOR SHALL PROVIDE THE OWNER / LANDSCAPE ARCHITECT WITH TOPSOIL TEST RESULTS (RECOMMEND UMASS AMHERST SOIL TESTING LAB) FOR APPROVAL PRIOR TO OBTAINING AND PLACING THE SOIL. IF ANY TOPSOIL IS PURCHASED OR PLACED PRIOR TO APPROVAL BY OWNER / LANDSCAPE ARCHITECT, IT IS AT CONTRACTORS RISK, AND IT CAN BE REMOVED AT NO ADDITIONAL COST TO THE OWNER. IF THE PLANTING SOIL (BOTH ONSITE AND OFFSITE SOURCES) DOES NOT FALL WITHIN THE REQUIRED SIEVE ANALYSIS, TEXTURAL CLASS, ORGANIC CONTENT, OR PH RANGE, IT SHALL BE ADJUSTED TO MEET THE SPECIFICATIONS THROUGH THE ADDITION OF SAND, COMPOST, LIMESTONE, OR ALUMINUM SULFATE TO BRING IT WITHIN THE SPECIFIED LIMITS AT NO ADDITIONAL COST TO THE OWNER.
4. TOPSOIL SHALL HAVE A PH VALUE BETWEEN 5.5 AND 6.5. TOPSOIL SHALL CONTAIN BETWEEN 4% AND 8% ORGANIC MATTER OF TOTAL DRY WEIGHT AND SHALL CONFORM TO THE FOLLOWING GRADATION AND TEXTURE CLASS ABOVE.

TOPSOIL FOR LAWN, TREES, SHRUBS, & PERENNIALS (2)
NOT TO SCALE



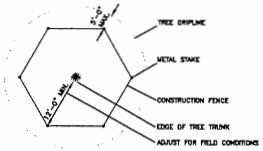
NOTES
1. TREES SHALL BEAR SAME RELATIONSHIP TO FINISH GRADE AS IT BORE TO NURSERY OR FIELD GRADE. ROOT FLARE SHALL BE 2" ABOVE FINISH GRADE. REMOVE SOIL FROM TRUNK FLARE OF TREE TO DETERMINE ACTUAL TOP OF ROOTBALL AREA.
2. INSTALL THREE GUYS PER TREE; EQUALLY SPACED AROUND BALL.
3. ATTACH GUYS AT 2/3 HEIGHT OF TREE.
4. BACKFILL WITH PLANTING MIX. PLANT MIX TO BE: 50% NATIVE TOPSOIL, 20% COMPOST (LEAVES & ORGANIC MATERIAL, NO ASH) 20% PEAT MOSS, 10% SAND.
5. ADD MYCORRHIZA SOIL ADDITIVES AND SLOW RELEASE FERTILIZER WHEN PLANT HOLES ARE 50% FILLED AND WATER THOROUGHLY AT COMPLETION.

EVERGREEN TREE DETAIL (1)
NOT TO SCALE

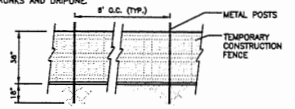


NOTES:
1. ALL TREES SHALL HAVE THE SAME RELATIONSHIP TO FINISH GRADE AFTER PLANTING AS THEY HAD AT THE ORIGINAL NURSERY SETTING. ROOT FLARE SHALL BE 2" ABOVE FINISH GRADE. REMOVE SOIL FROM TRUNK FLARE OF TREE TO DETERMINE ACTUAL ROOTBALL AREA.
2. BACKFILL WITH PLANTING MIX. PLANT MIX TO BE: 50% NATIVE TOPSOIL, 20% COMPOST (LEAVES & ORGANIC MATERIAL, NO ASH) 20% PEAT MOSS, 10% SAND.
3. ADD MYCORRHIZA SOIL ADDITIVES AND SLOW RELEASE FERTILIZER WHEN PLANT HOLES ARE 50% FILLED AND WATER THOROUGHLY AT COMPLETION.

DECIDUOUS TREE PLANTING DETAIL (3)
NOT TO SCALE

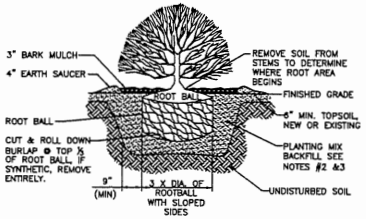


NOTE:
1. CONSTRUCTION FENCE TO BE "VISUAL BARRIER FENCE" AS MANUFACTURED BY EXXON CHEMICAL COMPANY ATLANTA, GA; "KONTROL SAFETY FENCE" AS MANUFACTURED BY MIRAFI, CHARLOTTE, N.C. OR APPROVED EQUAL.
2. IF GROUPS OF TREES ARE TO BE PROTECTED, EXTEND FENCE AROUND PERIMETER TO CONFORM TO MINIMUM DIMENSIONS FOR TREE TRUNKS AND DRIPLINE.



TEMP. CONST. FENCE / TREE PROTECTION (4)
NOT TO SCALE

NOTES:
1. ALL SHRUBS SHALL HAVE THE SAME RELATIONSHIP TO FINISH GRADE AFTER PLANTING AS THEY HAD AT THE ORIGINAL NURSERY SETTING. SET SHRUB 1"-2" ABOVE FINISH GRADE.
2. BACKFILL WITH PLANTING MIX. PLANT MIX TO BE: 50% NATIVE TOPSOIL, 20% COMPOST (LEAVES & ORGANIC MATERIAL, NO ASH) 20% PEAT MOSS, 10% SAND.
3. ADD MYCORRHIZA SOIL ADDITIVES AND SLOW RELEASE FERTILIZER WHEN PLANT HOLES ARE 50% FILLED AND WATER THOROUGHLY AT COMPLETION.
4. SHRUB BEDS TO HAVE 24" MIN. OF CONTINUOUS PLANTING SOIL.



SHRUB PLANTING DETAIL (5)
NOT TO SCALE

DIG SAFE
BEFORE YOU DIG CALL 811 OR 1-888-DIG-SAFE -888-344-7233

R:\PROJECTS\2729-03\CIVIL\DRAWINGS\CURRENT\C-2729-03_LANDSCAPING.DWG

REGISTERED LANDSCAPE ARCHITECT FOR ALLEN & MAJOR ASSOCIATES, INC.

REV	DATE	DESCRIPTION

APPLICANT\OWNER:
2-14 MEDFORD STREET, LLC
455 MASSACHUSETTS AVE, STE 1
ARLINGTON, MA 02474

PROJECT:
455-457
MASSACHUSETTS AVE
ARLINGTON, MA 02476

PROJECT NO.	2729-03	DATE:	8/20/2021
SCALE:	NTS	DWG. NAME:	C2729-03
DESIGNED BY:	JG, BCD	CHECKED BY:	ARM/BDJ

PREPARED BY:

ALLEN & MAJOR ASSOCIATES, INC.
civil engineering • land surveying
environmental consulting • landscape architecture
www.allenmajor.com
100 COMMERCE WAY, SUITE 5
WOBURN MA 01801
TEL: (781) 935-6893
FAX: (781) 935-2896
WOBURN, MA • LAKEVILLE, MA • MANCHESTER, NH

DRAWING TITLE: LANDSCAPE DETAILS

SHEET No. L-501

Robert Annese

From: John H. Murphy III <john@summit-res.net>
Sent: Monday, August 30, 2021 11:03 AM
To: Robert Annese
Subject: Fwd: 455 FAR

John Murphy

Vice President
SUMMIT Real Estate Strategies LLC
60 Summer Street. Manchester. MA 01944
PH: 978.704.9022
FX: 978.704.9728
Cell: 207.776.1751

Begin forwarded message:

> **From:** Peter Slowik <pslowik@marketsquarearchitects.com>
> **Date:** August 30, 2021 at 11:00:02 AM EDT
> **To:** John Murphy <john@summit-res.net>
> **Subject: Re: 455 FAR**
>
> In regards to the previous email-
>
> basement = 10873 sf
> first floor = 15012 sf
> second floor = 13567 sf
> total = 39452 sf
>
> minus basement and 206 sf of riser, electrical, elevator mech. = 28,373 gsf
>
> divide this by site area (18,929) = 1.5 FAR
>
> **Peter Slowik**
> **Market Square Architects, PLLC**
> Project Coordinator
> P: (603) 501-0202
> www.MarketSquareArchitects.com
>
>> On Aug 30, 2021, at 8:49 AM, Peter Slowik <pslowik@marketsquarearchitects.com> wrote:
>>
>> John,
>>
>> We digitally measured Gross Floor Area (GFA) in accordance with the Arlington Bylaws. Total area was measured along the exterior surface of the exterior walls of each floor.

From this we subtracted the areas which are allowed to be excluded, in this case mechanical spaces and service basements. This GFA was then divided by total site area provided by Civil.

Best,

Peter Slowik
Market Square Architects, PLLC
Project Coordinator
P: (603) 501-0202
www.MarketSquareArchitects.com

Robert Annese

From: John H. Murphy III <john@summit-res.net>
Sent: Monday, August 30, 2021 9:54 AM
To: Robert Annese
Subject: Fwd: 455 FAR

John Murphy

Vice President
SUMMIT Real Estate Strategies LLC
60 Summer Street. Manchester. MA 01944
PH: 978.704.9022
FX: 978.704.9728
Cell: 207.776.1751

Begin forwarded message:

From: Peter Slowik <pslowik@marketsquarearchitects.com>
Date: August 30, 2021 at 9:50:00 AM EDT
To: John Murphy <john@summit-res.net>
Subject: 455 FAR

John,

We digitally measured Gross Floor Area (GFA) in accordance with the Arlington Bylaws. Total area was measured along the exterior surface of the exterior walls of each floor. From this we subtracted the areas which are allowed to be excluded, in this case mechanical spaces and service basements. This GFA was then divided by total site area provided by Civil.

Best,

Peter Slowik
Market Square Architects, PLLC
Project Coordinator
P: (603) 501-0202
www.MarketSquareArchitects.com

The Atwood House

Conceptual Design Schemes

821 Mass. Ave, Arlington MA

Concept 01

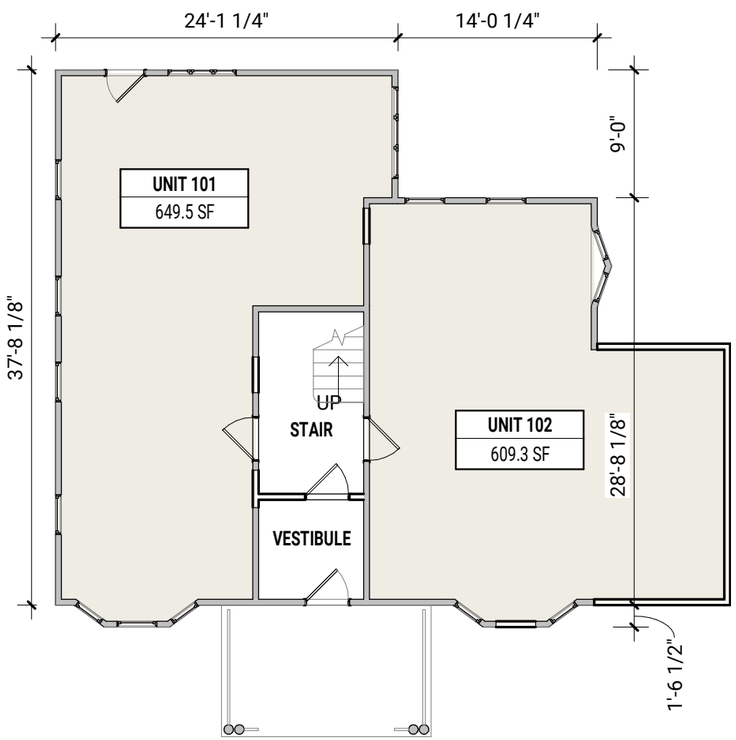
821 Mass. Ave, Arlington MA

PROPOSED SITE PLAN

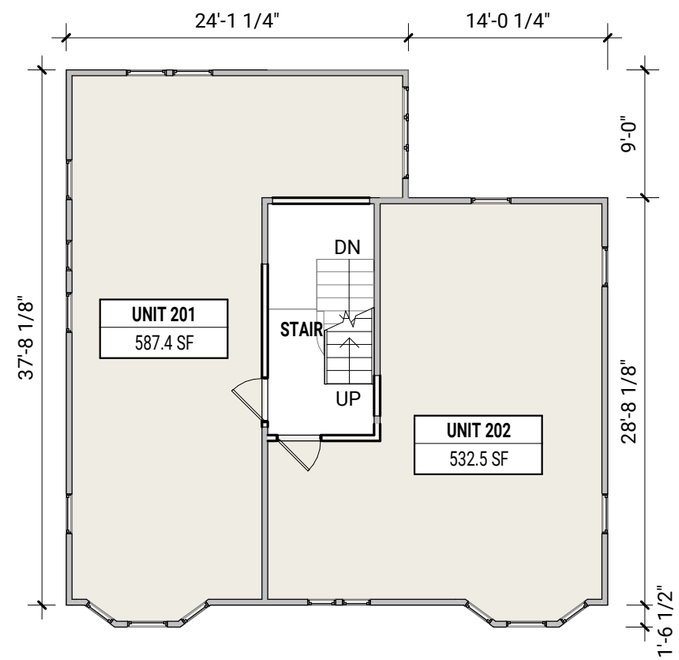
10.21.2021

MONTE FRENCH
DESIGN STUDIO
ARCHITECTURE + PLANNING

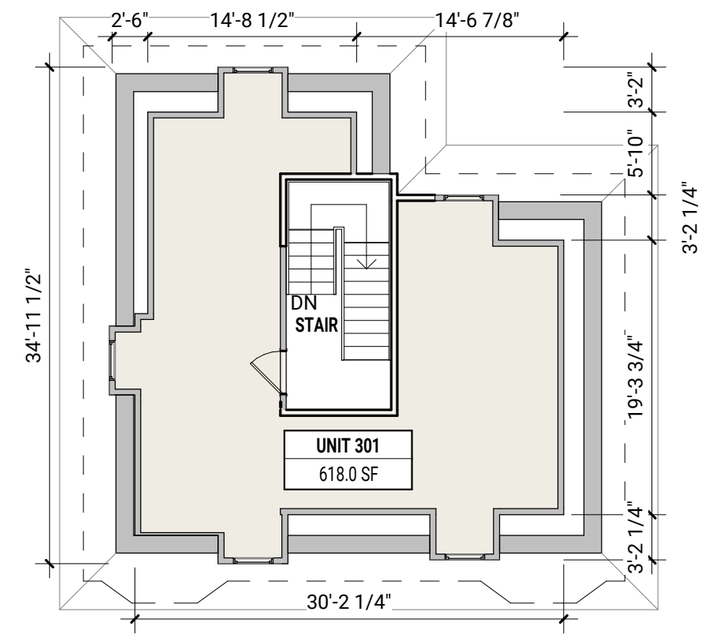
1ST FLOOR



2ND FLOOR



3RD FLOOR



LEGEND

- RESIDENTIAL UNIT
- COMMERCIAL SPACE

UNIT SCHEDULE

NAME	AREA	COMMENTS
UNIT 101	649.5 SF	ONE BED/ONE BA
UNIT 102	609.3 SF	ONE BED/ONE BA
UNIT 201	587.4 SF	ONE BED/ONE BA
UNIT 202	532.5 SF	ONE BED/ONE BA
UNIT 301	618.0 SF	ONE BED/ONE BA

821 Mass. Ave, Arlington MA

PROPOSED PLANS

10.21.2021

MONTE FRENCH
DESIGN STUDIO
ARCHITECTURE + PLANNING

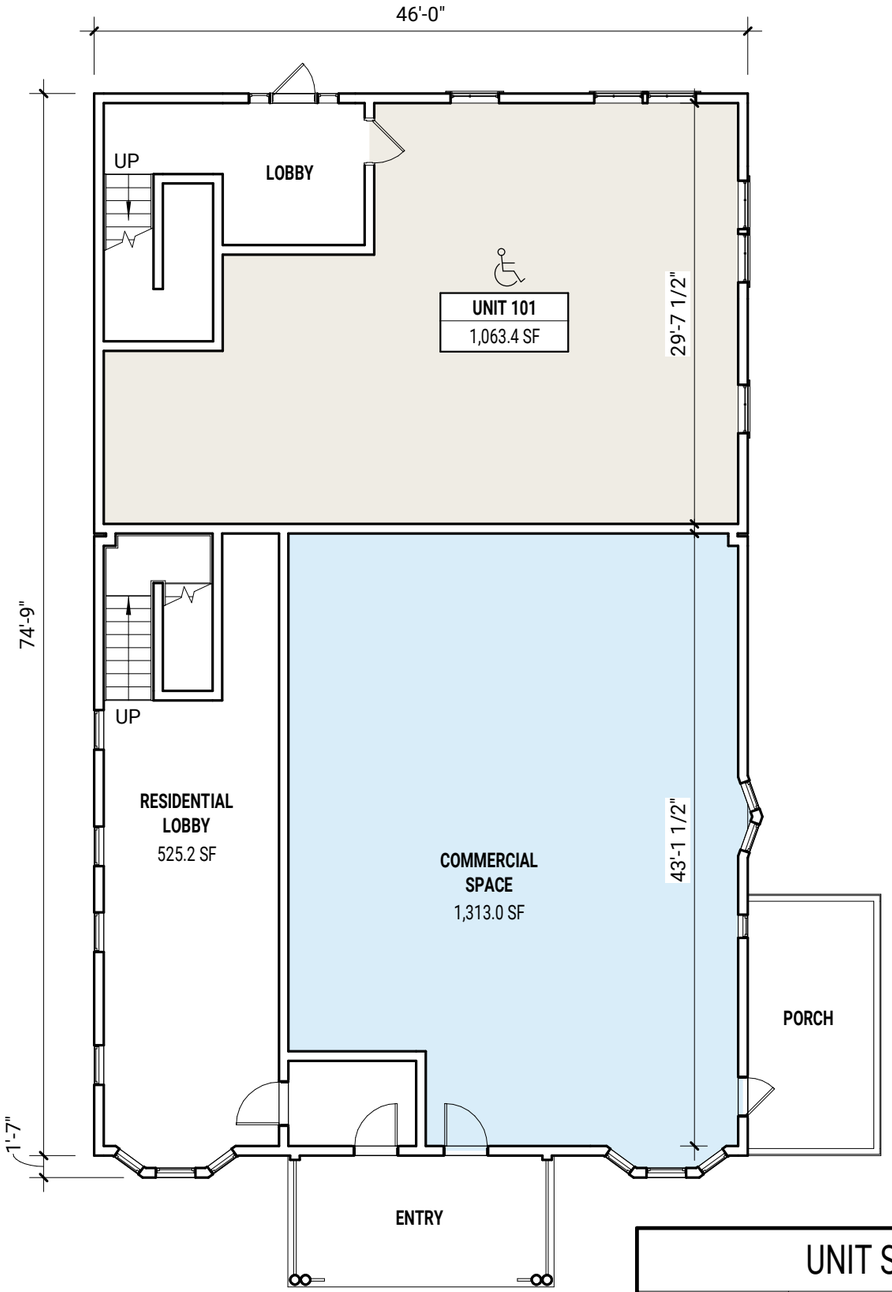
Concept 02

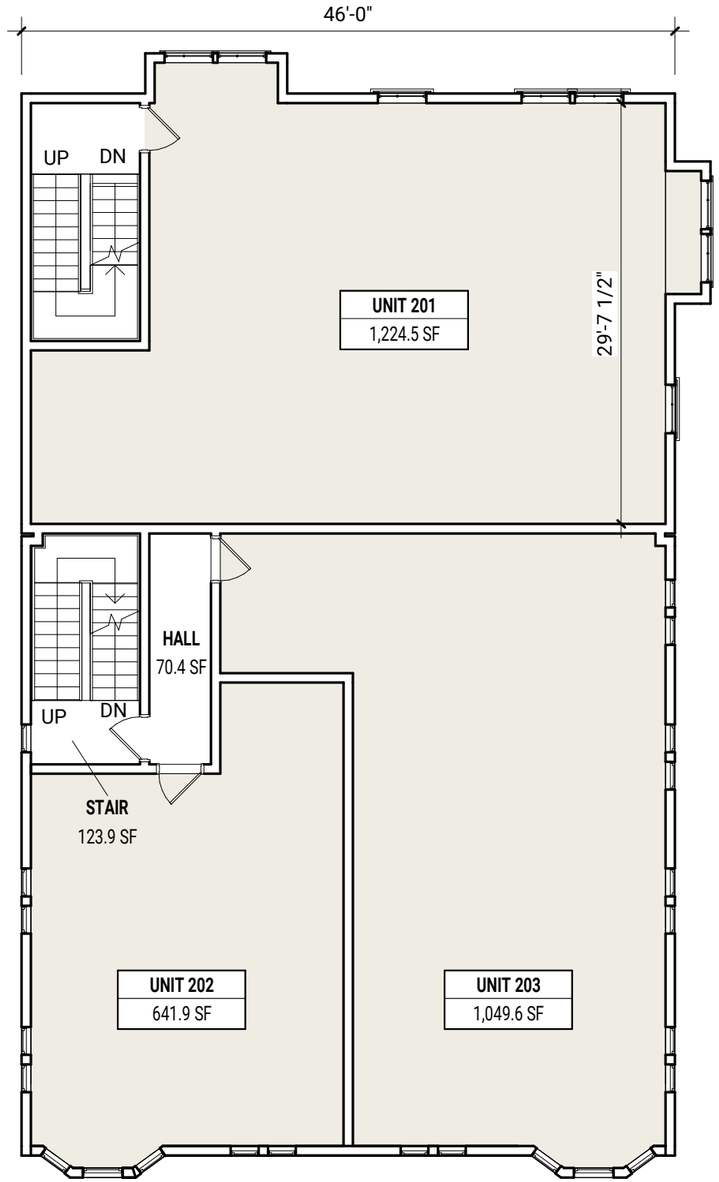
821 Mass. Ave, Arlington MA

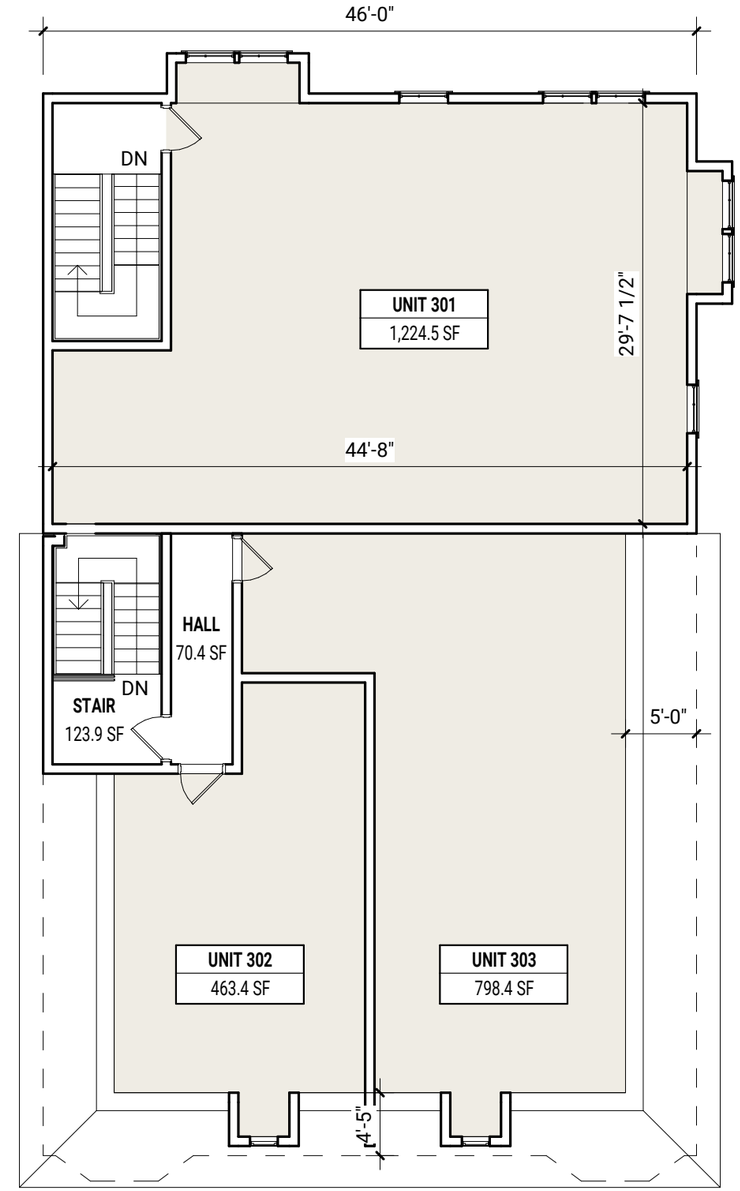
PROPOSED SITE PLAN

10.21.2021

MONTE FRENCH
DESIGN STUDIO
ARCHITECTURE + PLANNING







UNIT SCHEDULE

NAME	AREA	COMMENTS
UNIT 101	1,063.4 SF	2 BEDROOM, ACCESSIBLE
UNIT 201	1,224.5 SF	3 BEDROOM
UNIT 202	641.9 SF	1 BEDROOM
UNIT 203	1,049.6 SF	2 BEDROOM

UNIT SCHEDULE

NAME	AREA	COMMENTS
UNIT 301	1,224.5 SF	3 BEDROOM
UNIT 302	463.4 SF	STUDIO
UNIT 303	798.4 SF	1 BEDROOM +
Total Units: 7		

LEGEND

RESIDENTIAL UNIT

COMMERCIAL SPACE

821 Mass. Ave, Arlington MA

PROPOSED PLANS

MONTE FRENCH DESIGN STUDIO
ARCHITECTURE + PLANNING

From: "Jenny Raitt" <JRaitt@town.arlington.ma.us>
To: "Mary Muszynski" <MMuszynski@town.arlington.ma.us>
Date: 10/21/2021 03:04 PM
Subject: Fwd: FW: Atwood House - Design Schemes

Jennifer Raitt
Director, Department of Planning and Community Development
Town of Arlington

Arlington values equity, diversity, and inclusion. We are committed to building a community where everyone is heard, respected, and protected.

From: "Robert Annese" <law@robertannese.com>
To: "Jennifer Raitt" <jraitt@town.arlington.ma.us>, "'Kelly Lynema'" <KLynema@town.arlington.ma.us>
Cc: "Geoffrey Noyes" <gpnoyes@comcast.net>, <emily@mfds-bos.com>
Date: Thu, 21 Oct 2021 14:35:34 -0400
Subject: FW: Atwood House - Design Schemes

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

CAUTION: This email originated from outside of the Town of Arlington's email system. Do not click links or open attachments unless you recognize the REAL sender (whose email address in the From: line in "< >" brackets) and you know the content is safe.

Hi Jenny and Kelly:

Here are two alternative plans for discussion purposes for Monday evening.

Bob

Robert J. Annese, Esquire
1171 Massachusetts Avenue
Arlington, MA 02476
Telephone: 781-646-4911
Facsimile: 781-646-4910
law@robertannese.com

From: Emily Driscoll <emily@mfds-bos.com>
Sent: Thursday, October 21, 2021 1:00 PM
To: Robert Annese <law@robertannese.com>; Geoffrey Noyes <gpnoyes@comcast.net>
Cc: Monte French <monte@mfds-bos.com>
Subject: Atwood House - Design Schemes

Hi Bob & Geoff,

Attached is a package with two conceptual design schemes, my comments are as follows:

Concept 1
- Residential only
- Keeps the existing building and placement on site
- Recaptures the side porch on the ground floor as living area
- Total of 5 residential units

Concept 2
- Mixed use w/ commercial space on ground floor only
- Similar footprint to the mixed use scheme we presented previously to the ARB, but the front portion of the building is a widened version of the Atwood house footprint.
- Intent here is to keep the design elements of the Atwood house present while still allowing for a larger building footprint, increased number of residential units, and commercial space on the ground floor.
- Building has been pushed forward on the site to improve street presence and encourage pedestrian interaction
- Total of 7 residential units and 1,313 sf of commercial space

Please take a look through each scheme and let us know if you have any questions or comments.

Thanks,

Emily Driscoll
Architectural Designer III
(o) 617.606.3880
(c) 508.439.2154

MONTE FRENCH
DESIGN STUDIO
ARCHITECTURE + PLANNING
650 Columbus Ave, Suite A
Boston, MA 02118
www.mfds-bos.com

ARLINGTON REDEVELOPMENT BOARD
TOWN HALL ARLINGTON, MASSACHUSETTS 02476
TELEPHONE 781-316-3090

By Certified First Mail

Notification of Bylaw Violations at 821 Massachusetts Avenue

July 21, 2021

Geoffrey Noyes
208 Beacon Street
Marblehead, MA 01945

Dear Mr. Noyes:

Please receive this letter as notice of numerous violations of Town of Arlington Bylaws and Zoning Bylaws as a result of specific actions taken by your contractors and agents on or about June 28, 2021 through July 2, 2021 at your property known as the "Atwood House;" as well as ongoing, long-term conditions at such property, which are governed not only by Town ordinances, but a special permit issued by the Arlington Redevelopment Board.

In brief, despite a scheduled hearing on August 3m 2021 regarding a demolition permit on an historic building before the Arlington Historical Commission (AHC), your contractors and agents conducted significant demolition of the Atwood House over an approximately weeklong period in advance of the July 4th holiday weekend without any permits or notices to the Town. As a result, Director of Inspectional Services Michael Ciampa issued a stop-work order on or about July 3, 2021 to your contractor, Groom Construction. Performing such work, particularly given your pending application for a demolition permit and scheduled hearing before the AHC, as well as this Department's efforts to work collaboratively with you on options for redevelopment is particularly egregious. As the property owner of record, you are hereby noticed of the following fines *at a minimum* for violations of the following sections of the Town Bylaws due to unpermitted, premature, and improper demolition and site maintenance activities on or about the week of June 28 through July 2, 2021, including the following:

Title V, Article 7 Section C	$1,350
Title V, Article 9.1	$350
Title V, Article 17	$700
Title IX, Article 1	$80
	$2,480

Further, as the property owner of record, despite last registering your property on January 29, 2020, you have remained non-compliant with the Town's Vacant Storefront Bylaw (Title V Article 17) and are hereby noticed that absent immediate compliance, the Town will pursue up to two years of this Bylaw provision, totaling $73,000. Please also be advised that you will be fined an additional $100 per day until the building conditions are corrected to the Town's satisfaction. If you wish to avoid these fines, conditions at the Atwood House property including securing the building, properly registering it on the Vacant Storefront Registry, must improve swiftly and dramatically. Secure building conditions shall encompass the following: removal of all boarded windows and installing new windows and siding which shall be reviewed and approved by the Arlington Historical Commission and installation of a 24-hour security and monitoring system.

Moreover, the Town intends to reopen your Environmental Design Review Special Permit Docket #3348 for 833 Mass Ave due to continued non-compliance with Special Condition #5. This Special Permit is recorded at the Middlesex South Registry of Deeds Book 131350 and Page 69 on August 18, 2009. Pursuant to this Decision, as the property owner you are required to abide by all permit conditions. The Arlington Redevelopment Board requested follow-up on actions with regard to future redevelopment of the "Atwood House" as referenced in Special Condition #5 in the original Special Permit. You remain in non-compliance of the letter sent to you via certified mail on May 4, 2020. You were required to do the following and failed to follow through: you were to apply for a demolition permit within 30 days following the Board's vote or by May 27, 2020; you were to apply for an Environmental Design Review Special Permit following expiration of the demolition delay period or earlier contingent upon Arlington Historical Commission rulings per Town Bylaw Article 6 Historically or Architecturally Significant Buildings. If you had not filed for a demolition permit, you were to apply for an Environmental Design Review Special Permit with a renovation plan for the existing building within 60 days or by June 27, 2020. Please be advised that the Arlington Redevelopment Board reserves the right to revoke this Special Permit and staff may advise the Board of this option accordingly. As the owner, you have demonstrated willful and reckless negligence since this Special Permit was granted in 2009.

Finally, Inspectional Services advises this Department that commencing work prior to obtaining a permit constitutes a violation of the State Building Code,780 CMR Sections 105.1 and 109.4, each of which are punishable by $1,000 (or one year imprisonment) per violation, with each day constituting a separate violation. Inspectional Services will separately contact you and/or your contractor regarding penalties for commencing work without a permit.

Unless and until all of the above fines are paid and the violations are corrected to the satisfaction of the Arlington Historical Commission, the Town of Arlington intends to the full extent permitted by law to place a municipal lien on your property.

Please contact this office with questions regarding this notice at 781-316-3090.

Sincerely,

Jennifer Raitt
Director of Planning and Community Development

Michael Ciampa
Interim Director of Inspectional Services

cc: Robert J. Annese, Esquire
Rachel Zsembery, AIA, Chair, Arlington Redevelopment Board
JoAnn Robinson, Chair, Arlington Historical Commission
Douglas Heim, Town Counsel
Christine Bongiorno, Director of Health and Human Services
Chief Juliann Flaherty, Police Department
Chief Kevin Kelley, Fire Department

ROBERT J. ANNESE
ATTORNEY AT LAW

September 23, 2020

Via e-mail: robinsarcn.com

JoAnn Robinson, Chair
Historical Commission
Whittemore Robbins House
670R Massachusetts Ave
Arlington, Ma 02476

RE: Noyes Realty LLLP
(Atwood House) 821 Massachusetts Avenue, Arlington, Massachusetts

Dear Ms. Robinson:

My client is going to develop the real estate located at 821 Massachusetts Avenue, Arlington, i.e. the "Atwood House" and is filing a demolition application in connection with its building plans.

The building plans have not yet been finalized and will be the subject of further hearings before the Arlington Redevelopment Board.

I am sending a preliminary draft of the development plans in connection with the development of the property which will likely change as any hearings progress before the Arlington Redevelopment Board.

I am sending this material along to the Historical Commission at this time and would request that the Commission schedule if necessary a hearing regarding this matter with respect to my client's demolition application and plans.

Very truly yours,

Robert J. Annese

RJA:lm

Enclosures

cc: Mr. Geoffrey P. Noyes
Noyes Realty, LLLP

1171 MASSACHUSETTS AVENUE • ARLINGTON, MASSACHUSETTS 02476 • TELEPHONE 781-646-4911 • FAX 781-646-4910
E-MAIL ADDRESS: LAW@ROBERTANNESE.COM

ARLINGTON REDEVELOPMENT BOARD
TOWN HALL ARLINGTON, MASSACHUSETTS 02476
TELEPHONE 781-316-3090

May 4, 2020

Geoffrey Noyes
208 Beacon Street
Marblehead, MA 01945

Re: Environmental Design Review Special Permit 3348

Dear Mr. Noyes,

I am writing to follow-up from the April 27, 2020 Redevelopment Board meeting regarding the property you own located at 833 Massachusetts Avenue in Arlington, MA. This property has a Special Permit 3348 recorded at the Middlesex South Registry of Deeds Book 131350 and Page 69 on August 18, 2009. Pursuant to this Decision, as the property owner you are required to abide by all permit conditions. The Arlington Redevelopment Board requested follow-up on actions with regard to future redevelopment of the "Atwood House" as referenced in Special Condition #5 in the original Special Permit.

I am writing to inform you that on Monday, April 27, 2020, the Arlington Redevelopment Board voted unanimously (5-0) to close the hearing. In their vote, closing this hearing was conditioned on the following actions:

- The property owner shall apply for a demolition permit within 30 days following this vote or by May 27, 2020.
- The property owner shall apply for an Environmental Design Review Special Permit following expiration of the demolition delay period or earlier contingent upon Arlington Historical Commission rulings per Town Bylaw Article 6 Historically or Architecturally Significant Buildings.
- If the property owner does not file a demolition permit, the owner shall apply for an Environmental Design Review Special Permit with a renovation plan for the existing building within 60 days or by June 27, 2020.

Should you have any questions about this vote, please contact me at 781-316-3092.

Sincerely,

Jennifer Raitt
Secretary Ex-Officio

cc: Andrew Bunnell, Chair
Robert J. Annese, Esquire

DEC 18 2019

COMMONWEALTH OF MASSACHUSETTS.
MIDDLESEX S.S. _____
SOUTH DIST. REGISTRY OF DEEDS
CAMBRIDGE, MA
I HEREBY CERTIFY THE FOREGOING
IS A TRUE COPY OF A PAPER
RECORDED IN BOOK 73849
PAGE 534

REGISTER

I hereby certify this is a True Copy of the Decision of the Arlington Redevelopment Board as filed with the Office of the Town Clerk of the Town of Arlington, Massachusetts on and that 20 days have elapsed after the Decision and no Appeal has been filed ATTEST: Janice G. Weber

Date of Issue 12-9-2019 Asst. Town Clerk

TOWN CLERK'S OFFICE
ARLINGTON, MA 02174

2019 NOV 18 AM 9:34

RECEIVED

ARLINGTON REDEVELOPMENT BOARD

Arlington, Massachusetts
Middlesex, ss

DOCKET NO. 3348

DECISION
Special Permit Under
ENVIRONMENTAL DESIGN REVIEW

Applicant: CVS
One CVS Drive, Woonsocket, RI 02895
Property Address: 833 Massachusetts Avenue, Arlington, Massachusetts 02476

Date of Hearings: August 12, 2019, November 4, 2019
Date of Decision: November 4, 2019

20 Day Appeal Period Ends: December 8, 2019

Members Approved	Opposed
Eugene B. Benson	
David M. Watt	

Janice G. Weber 12-9-2019

Asst. Town Clerk's Certification Date

I hereby certify this is a True Copy of the Decision of the Arlington Redevelopment Board as filed with the Office of the Town Clerk of the Town of Arlington, Massachusetts on
and that 20 days have elapsed after the Decision and no Appeal has been filed ATTEST: Janice A. Weber Asst Town Clerk

Date of Issue 12-9-2019

TOWN CLERK'S OFFICE
ARLINGTON, MA 02174

2019 NOV 18 AM 9:33

RECEIVED

Town of Arlington, Massachusetts

Redevelopment Board

730 Massachusetts Avenue, Arlington, Massachusetts 02476

DECISION OF THE BOARD

Environmental Design Review Docket #3348
833 Massachusetts Avenue, Arlington, MA 02476
CVS

November 4, 2019

This Decision applies to the re-opening of Special Permit Docket 3348 by CVS to install new signage consistent with CVS rebranding. The CVS store is located at 833 Massachusetts Avenue within a B4 Vehicular Oriented Business District. The re-opening of the Special Permit is to allow the Board to review and approve the signage, under Section 3.4, Environmental Design Review, and section 6.2, Signs. A public hearing was held on August 12, 2019, and continued to November 4, 2019, when this decision was rendered.

Materials submitted for consideration of this application:
Application for Environmental Design Review Special Permit application dated June 27, 2019.

The following criteria have been met, per Section 3.3.3, Arlington Zoning Bylaw:

1. The retail pharmacy is allowed in the B-4 Vehicular Oriented Business District.

2. The retail pharmacy has operated in this location for many years.

3. There are no exterior alterations other than signage.

4. The retail pharmacy will not overload any public utilities: public water, drainage or sewer system or any other municipal system.

5. No special regulations are applicable to the use.

6. The use does not impair the integrity or character of the neighborhood. Although additional directional signs will be installed to assist in circulation on the site, the large wall signs will be smaller than the existing signage on the building.

7. The use will not be in excess or detrimental to the character of the neighborhood.

The following criteria have been met, per Section 3.4.4, Arlington Zoning Bylaw:

A. **EDR-1 Preservation of Landscape**
There are no changes to the site that would impact existing natural features.

B. **EDR-2 Relation of the Building to the Environment**
There are no changes to the exterior of the building other than the installation of new signage to replace the existing signage.

C. **EDR-3 Open Space**
The 2009 Decision indicated that landscaping would be installed between the front of the building and the Massachusetts Avenue sidewalk. This area is entirely sidewalk and three benches are present. The tenant and the property owner will work with the Department of Planning and Community Development to come to a reasonable solution that reflects the previous Decision. There are no other changes to open space as a result of the signage rebranding.

D. **EDR-4 Circulation**
The existing circulation does not change; however, the addition of a Do Not Enter sign will help ensure that internal circulation occurs as it is intended.

E. **EDR-5 Surface Water Drainage**
The signage rebranding will not affect surface water run-off.

F. **EDR-6 Utilities Service**
There are no changes to the utility service as a result of the signage rebranding.

G. **EDR-7 Advertising Features**
The existing CVS signage includes a slash, and reads as CVS/pharmacy. The rebranding eliminates the slash, but includes a heart shape in front of the words CVS pharmacy. The rebranding retains the typical red color associated with CVS.

The new signage includes removing the large signage above the main entrance of the building and other plaques, and replacing it with updated signage. A Do Not Enter sign will be installed. All other directional signage will be retained.

The signage on the Massachusetts Avenue frontage is currently 75.18 square feet and will be replaced with signage that measures approximately 33.08 square feet. The reason for the reduction is the size of the letters. The existing letters are approximately 36 inches and the proposed letters are 22.5 inches. Additionally, the new signage will include channel LED illumination.

The main signage facing the parking lot is currently 33.41 square feet and will be replaced with signage that measures approximately 33.08 square feet. The existing letters are

approximately 24 inches and the proposed letters are 22.5 inches. Additionally, the new signage will include channel LED illumination.

Three plaques on the property will be updated. A plaque at the main entrance will be replaced. This plaque conveys information regarding the opening hours, the store manager, and the pharmacy manager. The plaque will remain but the CVS/pharmacy will be replaced with the heart branding. The receiving entrance plaque will be replaced with a 3 square foot plaque. A directional sign will be replaced at the drive-thru pharmacy that indicates both lanes offer full service. It is approximately 4.17 square feet.

A Do Not Enter sign will be installed at the end of the main drive aisle in the parking lot. At the rear of the site, the circulation is one way in order to access the drive-thru pharmacy. The Do Not Enter sign will reinforce the circulation pattern. The sign will be installed about 3 feet above grade and is approximately 2.25 square feet.

An additional directional sign that was not accounted for previously was also acknowledged during the public hearing. The directional signage provides a visual cue on the best way to access the drive through pharmacy.

All other directional signage remains as is on the property.

The reduction in the size of the main signage, the lighting upgrade, and the addition of the Do Not Enter sign are improvements to the property.

H. <u>EDR-8 Special Features</u>

There are no changes to the building or the site that would cause any adverse impacts on light, air and water resources, or on noise and temperature levels.

I. <u>EDR-9 Safety</u>

There are no changes to the building or the site that would cause any safety or accessibility concerns.

J. <u>EDR-10 Heritage</u>

The CVS building is not located on any local or State historic property listing. The adjacent Atwood House is identified as a significant building per Title VI, Article 6 of the Town Bylaw. The signage rebranding does not impact the Atwood House and the 2009 Decision retains jurisdiction over future plans for the structure as does the Historical Commission. The Redevelopment Board requests that the property owner attend the December 16, 2019 meeting to discuss the future of the Atwood House.

K. <u>EDR-11 Microclimate</u>

The signage rebranding will not impact the microclimate.

L. EDR-12 Sustainable Building and Site Design

The signage rebranding will support sustainable building and site design through the usage of LED fixtures to illuminate the signage.

The project must adhere to the following general conditions:

1. The final plans and specifications for signage shall be subject to final approval by the Department of Planning and Community Development (DPCD).

2. Any substantial or material deviation during construction from the approved plans and specifications is subject to the written approval of the Arlington Redevelopment Board.

3. The conditions of the 2009 Special Permit decision are still in force. The Board maintains continuing jurisdiction over this permit and may, after a duly advertised public hearing, attach other conditions or modify these conditions as it deems appropriate in order to protect the public interest and welfare.

The project must adhere to the following special conditions:

1. The Applicant and the property owner will work with the Department of Planning and Community Development to come to a reasonable solution that reflects the requirement of the 2009 Decision to install landscaping between the front of the building and the Massachusetts Avenue sidewalk.

2. The Applicant and property owner appear at the December 16, 2019, Redevelopment Board hearing to discuss the ongoing compliance with the 2009 Decision, with special attention to the Atwood House.

ARLINGTON REDEVELOPMENT BOARD
TOWN HALL ARLINGTON, MASSACHUSETTS 02476
TELEPHONE 781-316-3090

August 15, 2019

Geoffrey Noyes
208 Beacon Street
Marblehead, MA 01945

Re: Environmental Design Review Special Permit 3348 - CVS

Dear Mr. Noyes,

I am writing regarding the condition of property you own located at 833 Massachusetts Avenue in Arlington, MA. This property has a Special Permit 3348 recorded at the Middlesex South Registry of Deeds Book 131350 and Page 69 on August 18, 2009. Pursuant to this Decision, as the property owner you are required to abide by all permit conditions. The Arlington Redevelopment Board is specifically concerned about Special Condition #5 with regard to your responsibility to maintain the "Atwood House". This condition requires you to make reasonable and diligent efforts to prevent any damage from the elements or otherwise, until the property is redeveloped. Currently, the property is partially boarded and partially exposed to the elements. The partial exposure is not compliant with the Decision.

Your tenant CVS Health recently requested an amendment to Special Permit 3348 in order to increase the number of signs on the property. At the opening of the public hearing on August 12th, the Board quickly indicated that they were concerned about broader property issues referenced above and subsequently continued the hearing to September 9th at 7:30 p.m. in the Town Hall, 730 Massachusetts Avenue, second floor conference room. The Board requests your attendance at this meeting in order to provide a property update regarding the conditions and your ability to meet permit requirements.

Please contact me at your earliest convenience should you wish to discuss these matters. Otherwise, your presence is expected at the hearing date and time noted above.

Sincerely,

Jennifer Raitt
Secretary Ex-Officio

cc: Andrew Bunnell, Chair
Robert J. Annese, Esquire

Town of Arlington, Massachusetts
Department of Planning & Community Development
730 Massachusetts Avenue, Arlington, Massachusetts 02476

Public Hearing Memorandum

The purpose of this memorandum is to provide the Arlington Redevelopment Board and public with technical information and a planning analysis to assist with the regulatory decision-making process.

To: Arlington Redevelopment Board

From: Jennifer Raitt, Secretary Ex Officio

Subject: Environmental Design Review, 833 Massachusetts Ave, Arlington, MA
Docket #3348

Date: August 6, 2019

I. Docket Summary

This is an application by Gary McCoy, Poyant Signs, for CVS, at 833 Massachusetts Avenue, to re-open Special Permit Docket #3348. This is in accordance with the provisions of M.G.L. Chapter 40A Section 11, and the Town of Arlington Zoning Bylaw Section 3.4 Environmental Design Review. The applicant proposes to install new signage consistent with CVS rebranding. The re-opening of the Special Permit is to allow the Board to review and approve the signage, under Section 6.2, Signs.

Materials submitted for consideration of this application:

1. Environmental Design Review Special Permit Application dated June 27, 2019.

II. Application of Special Permit Criteria (Arlington Zoning Bylaw, Section 3.3)

1. **Section 3.3.3.A.**
   The use requested is listed as a Special Permit in the use regulations for the applicable district or is so designated elsewhere in this Bylaw.

   The retail pharmacy is allowed in the B-4 Vehicular Oriented Business District. The Board can find that this condition is met.

2. **<u>Section 3.3.3.B.</u>**
   The requested use is essential or desirable to the public convenience or welfare.

   The retail pharmacy has operated in this location for many years. The Board can find that this condition is met.

3. **<u>Section 3.3.3.C.</u>**
   The requested use will not create undue traffic congestion or unduly impair pedestrian safety.

   There are no exterior alterations other than signage. The Board can find that this condition is met.

4. **<u>Section 3.3.3.D.</u>**
   The requested use will not overload any public water, drainage or sewer system or any other municipal system to such an extent that the requested use or any developed use in the immediate area or in any other area of the Town will be unduly subjected to hazards affecting health, safety, or the general welfare.

   This retail pharmacy has operated in this location for many years without overloading any public utilities. The Board can find that this condition is met.

5. **<u>Section 3.3.3.E.</u>**
   Any special regulations for the use as may be provided in the Bylaw are fulfilled.

   All such regulations are fulfilled.

6. **<u>Section 3.3.3.F.</u>**
   The requested use will not impair the integrity or character of the district or adjoining districts, nor be detrimental to the health or welfare.

   The use does not impair the integrity or character of the neighborhood. The Board can find that this condition is met.

7. **<u>Section 3.3.3.G.</u>**
   The requested use will not, by its addition to a neighborhood, cause an excess of the use that could be detrimental to the character of said neighborhood.

   The use will not be in excess or detrimental to the character of the neighborhood. The Board can find that this condition is met.

III. Environmental Design Review Standards (Arlington Zoning Bylaw, Section 3.4)

A. EDR-1 Preservation of Landscape

The landscape shall be preserved in its natural state, insofar as practicable, by minimizing tree and soil removal, and any grade changes shall be in keeping with the general appearance of neighboring developed areas.

There are no changes to the landscape as there are no proposed exterior alterations. The Board can find that this condition is met.

B. EDR-2 Relation of the Building to the Environment

Proposed development shall be related harmoniously to the terrain and to the use, scale, and architecture of the existing buildings in the vicinity that have functional or visible relationship to the proposed buildings. The Arlington Redevelopment Board may require a modification in massing so as to reduce the effect of shadows on the abutting property in an R0, R1 or R2 district or on public open space.

There are no changes to the exterior of the building other than the installation of new signage replacing the existing signage. The Board can find that this condition is met.

C. EDR-3 Open Space

All open space (landscaped and usable) shall be so designed as to add to the visual amenities of the vicinity by maximizing its visibility for persons passing by the site or overlooking it from nearby properties. The location and configuration of usable open space shall be so designed as to encourage social interaction, maximize its utility and facilitate maintenance.

There are no changes to open space. The Board can find that this condition is met.

D. EDR-4 Circulation

With respect to vehicular and pedestrian and bicycle circulation, including entrances, ramps, walkways, drives, and parking, special attention shall be given to location and number of access points to the public streets (especially in relation to existing traffic controls and mass transit facilities), width of interior drives and access points, general interior circulation, separation of pedestrian and vehicular traffic, access to community facilities, and arrangement of vehicle parking and bicycle parking areas, including bicycle parking spaces required by Section 6.1.12 that are safe and convenient and, insofar as practicable, do not detract from the use and enjoyment of proposed buildings and structures and the neighboring properties.

The existing circulation does not change; however, the addition of a Do Not Enter sign will help ensure that circulation occurs as it is intended. The Board can find that this condition is met.

E. <u>EDR-5 Surface Water Drainage</u>

Special attention shall be given to proper site surface drainage so that removal of surface waters will not adversely affect neighboring properties or the public storm drainage system. Available Best Management Practices for the site should be employed, and include site planning to minimize impervious surface and reduce clearing and re-grading. Best Management Practices may include erosion control and stormwater treatment by means of swales, filters, plantings, roof gardens, native vegetation, and leaching catch basins. Stormwater should be treated at least minimally on the development site; that which cannot be handled on site shall be removed from all roofs, canopies, paved and pooling areas and carried away in an underground drainage system. Surface water in all paved areas shall be collected in intervals so that it will not obstruct the flow of vehicular or pedestrian traffic and will not create puddles in the paved areas.

In accordance with Section 3.3.4., the Board may require from any applicant, after consultation with the Director of Public Works, security satisfactory to the Board to insure the maintenance of all stormwater facilities such as catch basins, leaching catch basins, detention basins, swales, etc. within the site. The Board may use funds provided by such security to conduct maintenance that the applicant fails to do.

The Board may adjust in its sole discretion the amount and type of financial security such that it is satisfied that the amount is sufficient to provide for any future maintenance needs.

There will be no changes to the exterior of the building or surface water run-off as a result of this proposal. The Board can find that this condition is met.

F. <u>EDR-6 Utilities Service</u>

Electric, telephone, cable TV, and other such lines of equipment shall be underground. The proposed method of sanitary sewage disposal and solid waste disposal from all buildings shall be indicated.

There will be no changes to the utility service as a result of this proposal. The Board can find that this condition is met.

G. <u>EDR-7 Advertising Features</u>

The size, location, design, color, texture, lighting and materials of all permanent signs and outdoor advertising structures or features shall not detract from the use and enjoyment of proposed buildings and structures and the surrounding properties.

The existing CVS signage includes a slash, and appears as CVS/pharmacy. The proposed rebranding eliminates the slash and includes a heart in front of the words CVS pharmacy. The heart rebranding retains the typical red color associated with CVS.

The scope of work for the new signage includes removing the large signage above the main entrance of the building and other plaques, and replacing it with updated signage. A new Do Not Enter sign will be installed. All other directional signage will be retained.

The signage on the Massachusetts Avenue frontage is currently 75.18 square feet and will be replaced with signage that measures approximately 33.08 square feet. The reason for the reduction is the size of the letters. The existing letters are approximately 36 inches and the proposed letters are 22.5 inches. Additionally, the new signage will include channel LED illumination.

The main signage facing the parking lot is currently 33.41 square feet and will be replaced with signage that measures approximately 33.08 square feet. The existing letters are approximately 24 inches and the proposed letters are 22.5 inches. Additionally, the new signage will include channel LED illumination.

Three plaques on the property will be updated. A plaque at the main entrance will be replaced. This plaque conveys information regarding the opening hours, the store manager, and the pharmacy manager. The plaque will remain but the CVS/pharmacy will be replaced with the heart branding. The receiving entrance plaque will be replaced with a 3 square foot plaque. A directional sign will be replaced at the drive-thru pharmacy that indicates both lanes offer full service. It is approximately 4.17 square feet.

A new Do Not Enter sign will be installed at the end of the main drive aisle in the parking lot. At the rear of the site, the circulation is one way in order to access the drive-thru pharmacy. The Do Not Enter sign will reinforce the circulation pattern. The sign will be installed about 3 feet above grade and is approximately 2.25 square feet. All other directional signage remains as is on the property.

The reduction in the size of the main signage, the lighting upgrade, and the addition of the Do Not Enter sign are improvements to the property. The Board can find that this condition is met.

H. <u>EDR-8 Special Features</u>

Exposed storage areas, exposed machinery installations, service areas, truck loading areas, utility buildings and structures, and similar accessory areas and structures shall be subject to such setbacks, screen plantings or other screening methods as shall reasonably be required to prevent their being incongruous with the existing or contemplated environment and the surrounding properties.

No changes are proposed. The Board can find that this condition is met.

I. <u>EDR-9 Safety</u>

With respect to personal safety, all open and enclosed spaces shall be designed to facilitate building evacuation and maximize accessibility by fire, police and other emergency personnel and equipment. Insofar as practicable, all exterior spaces and interior public and semi-public spaces shall be so designed to minimize the fear and probability of personal harm or injury by increasing the potential surveillance by neighboring residents and passersby of any accident or attempted criminal act.

No changes are proposed. The Board can find that this condition is met.

J. <u>EDR-10 Heritage</u>

With respect to Arlington's heritage, removal or disruption of historic, traditional or significant uses, structures or architectural elements shall be minimized insofar as practical whether these exist on the site or on adjacent properties.

The CVS building is not located on any local or State historic property listing. However, the adjacent Atwood House is identified as a significant building per Title VI, Article 6 of the Town Bylaw. The signage rebranding does not impact the Atwood House and the Special Permit Decision retains jurisdiction over future plans for the structure as does the Historical Commission. The Board can find that this condition is met.

K. <u>EDR-11 Microclimate</u>

With respect to the localized climatic characteristics of a given area, any development which proposes new structures, new hard surface, ground coverage or the installation of machinery which emits heat, vapor or fumes shall endeavor to minimize insofar as practicable, any adverse impacts on light, air and water resources or on noise and temperature levels of the immediate environment.

No changes are proposed. The Board can find that this condition is met.

L. <u>EDR-12 Sustainable Building and Site Design</u>

Projects are encouraged to incorporate best practices related to sustainable sites, water efficiency, energy and atmosphere, materials and resources, and indoor environmental quality. Applicants must submit a current Green Building Council Leadership in Energy and Environmental Design (LEED) checklist, appropriate to the type of development, annotated with narrative description that indicates how the LEED performance objectives will be incorporated into the project.

No changes are proposed. The Board can find that this condition is met.

IV. <u>Conditions</u>

1. The final plans and specifications for signage shall be subject to final approval by the Department of Planning and Community Development (DPCD).

2. Any substantial or material deviation during construction from the approved plans and specifications is subject to the written approval of the Arlington Redevelopment Board.

3. The conditions of the original Special Permit decision are still in force. The Board maintains continuing jurisdiction over this permit and may, after a duly advertised public hearing, attach other conditions or modify these conditions as it deems appropriate in order to protect the public interest and welfare.

June 27, 2019

Town of Arlington
Planning & Community Development
730 Mass Ave. Annex
Arlington, MA 02476

Re: 833 Mass Ave. CVS Pharmacy Signage Rebrand

Dear Town of Arlington,

Please find enclosed special sign permit application for Environmental Design Review and supporting documents as needed.

2 copies of:

- Application & Plans
- Owner Approval Letter from CVS
- COI & Worker's Comp.
- Construction Supervisor License
- Original Sign Permit Application
- $500 Permit Fee (Check)
- Dimensional & Parking Info Form
- Site Plan
- Drawing of Existing Conditions and Drawing of Proposed
- Photographs
- Impact Statement

An electronic copy of these materials have been sent to Erin Zwirko.

If you have any questions, please call me at 508-328-1457.

Sincerely,

Gary McCoy
Account Executive
125 Samuel Barnet Boulevard
New Bedford, MA 02745
508.328.1457

TOWN OF ARLINGTON
REDEVELOPMENT BOARD
Application for Special Permit In Accordance with Environmental Design Review Procedures (Section 3.4 of the Zoning Bylaw)

Docket No. _____

1. Property Address 833 Massachusetts Ave Arlington, MA 02476
Name of Record Owner(s) CVS Pharmacy Phone 401-770-3047
Address of Owner One CVS Drive, MC 1190 , Woonsocket, RI 02895
Street City, State, Zip

2. Name of Applicant(s) (if different than above) Poyant Signs
Address 125 Samuel Barnet Blvd. New Bedford MA 02745 Phone 508-328-1457
Status Relative to Property (occupant, purchaser, etc.) Sign Contractor

3. Location of Property 52-1-1
Assessor's Block Plan, Block, Lot No.

4. Deed recorded in the Registry of deeds, Book _____, Page _____;
-or- registered in Land Registration Office, Cert. No. _____, in Book _____, Page _____.

5. Present Use of Property (include # of dwelling units, if any) Pharmacy/Commercial

6. Proposed Use of Property (include # of dwelling units, if any) Pharmacy/Commercial (Like for Like)

7. Permit applied for in accordance with the following Zoning Bylaw section(s)

section(s)	title(s)
3.3	Special Permits
3.4	Environmental Design Review
6.2	Signs

8. Please attach a statement that describes your project and provide any additional information that may aid the ARB in understanding the permits you request. Include any reasons that you feel you should be granted the requested permission.

see attached statement

(In the statement below, strike out the words that do not apply)

The applicant states that CVS Pharmacy is the ~~owner -or- occupant -or-~~ purchaser under agreement of the property in Arlington located at 833 Massachusetts Ave which is the subject of this application; and that unfavorable action -or- no unfavorable action has been taken by the Zoning Board of Appeals on a similar application regarding this property within the last two years. The applicant expressly agrees to comply with any and all conditions and qualifications imposed upon this permission, either by the Zoning Bylaw or by the Redevelopment Board, should the permit be granted.

_____ _____
Signature of Applicant(s)

125 Samuel Barnet Blvd. New Bedford MA 02745 508-328-1457
Address Phone

TOWN OF ARLINGTON
Dimensional and Parking Information
for Application to
The Arlington Redevelopment Board

Docket No. _____

Property Location 833 Massachusetts Ave

Zoning District B4

Owner: CVS Pharmacy

Address: One CVS Drive, MC 1190 Woonsocket, RI 02895

Present Use/Occupancy: No. of Dwelling Units:
Pharmacy

Uses and their gross square feet:

Proposed Use/Occupancy: No. of Dwelling Units:
Pharmacy

Uses and their gross square feet:

	Present Conditions	Proposed Conditions	Min. or Max. Required by Zoning for Proposed Use
Lot Size	79715 SF	79715 SF	min.
Frontage			min.
Floor Area Ratio			max.
Lot Coverage (%), where applicable			max.
Lot Area per Dwelling Unit (square feet)	NA	NA	min. NA
Front Yard Depth (feet)			min.
Side Yard Width (feet) right side			min.
left side			min.
Rear Yard Depth (feet)			min.
Height			min.
Stories	1	1	stories
Feet			feet
Open Space (% of G.F.A.)			min.
Landscaped (square feet)			(s.f.)
Usable (square feet)			(s.f.)
Parking Spaces (No.)			min.
Parking Area Setbacks (feet), where applicable			min.
Loading Spaces (No.)			min.
Type of Construction	Signage		
Distance to Nearest Building			min.

June 27, 2019

Town of Arlington
Planning & Community Development
730 Mass Ave. Annex
Arlington, MA 02476

Re: Impact Statement

Dear Town of Arlington,

Poyant has been contracted with CVS pharmacy to rebrand their existing signage at their location at 833 Massachusetts Avenue, Arlington MA. Due to the scope of work, we feel that there will be no negative impact as it relates to the Arlington Environmental Design Review Standards. The existing signage that is currently at this location include lighting systems that can be more hazardous to the environment due to the amount of energy used. We will be replacing this existing lighting system with new, energy-efficient LEDs. The following is the scope of work for this proposed signage project:

- Remove existing (2) 24" Main ID Channel Lettersets and plaques.
- Replace with (2) 22" Channel Letters (LED Illuminated) and new regulatory plaques.
- Install Do Not Enter sign.
- All directional signage will remain as is.

Please review the attached signage plans for more details (install locations, etc.).

If you have any questions, please call me at 508-328-1457.

Sincerely,

Gary McCoy
Account Executive
125 Samuel Barnet Boulevard
New Bedford, MA 02745
508.328.1457

One CVS Drive
Woonsocket, RI 02895

June 19, 2019

To: Building Inspector

Re: CVS pharmacy contracted sign vendor

To Whom It May Concern:

CVS pharmacy has awarded Poyant Signs Inc. as the exterior sign vendor throughout New England and New York. Poyant Signs Inc. has a term contract in place with CVS pharmacy.

Please accept this letter to allow Poyant Signs Inc. & their respective agents to act as our agent, pull permits and install signs as per code.

Sincerely,

Karen Rezuke

Karen Rezuke

Properties Programs and Strategy Administration, Sr. Consultant Exterior Branding

CVS pharmacy

2019 THH BIR Upgrade

Existing Location Overview

SITE SIGNAGE SUMMARY

EXISTING	PROPOSED
(1) 24" Main ID Channel Letterset	(1) IL-22-CL-L LED Heart
(2) 24" Main ID Channel Letterset	(2) IL-22-CL-L LED Heart
(3) Drive-Thru Information Panel	(3) Full Service Regulatory Plaque
(4) Pharmacy Pick-Up/Drop-off Plaque	(4) Remove and Dispose
(5) Missing Do Not Enter Sign	(5) Do Not Enter Sign
(6) Receiving Entrance Plaque	(6) Receiving Entrance Plaque
(7) Hours Plaque	(7) Hours Plaque
(8) Directional Signs (Qty:4)	(8) Leave As Is

Proposed Signage Overview

Loc#: 00278

833 Massachusetts Ave.
Arlington, MA 02476

Poyant
Building Your Brand

Drawing prepared by:

Poyant
Building Your Brand

SITE PLAN

Location:
833 Massachusetts Ave.
Arlington, MA 02476

Proj #:
16189

Loc#:
278

File Path:
ACCOUNTS\C\CVS pharmacy\Locations 2016\3018_MC\3018_0000_City_ST

Drawing prepared for:

CVS pharmacy

Rev#:	Req#:	Date:	Req. By:	Drawn By:	Revision Description:
Original	000000	4/16/19	GM	CC	
Rev 1	000000	6/13/19	GM	CC	Notes
Rev 2	000000	00/00/00	XXX	XXX	
Rev 3	000000	00/00/00	XXX	XXX	
Rev 4	000000	00/00/00	XXX	XXX	
Rev 5	000000	00/00/00	XXX	XXX	
Rev 6	000000	00/00/00	XXX	XXX	

Rev#:	Req#:	Date:	Req. By:	Drawn By:
Rev 7	000000	00/00/00	XXX	XXX
Rev 8	000000	00/00/00	XXX	XXX
Rev 9	000000	00/00/00	XXX	XXX
Rev 10	000000	00/00/00	XXX	XXX

NOTES:

Drawing prepared by:

Poyant Building Your Brand

SITE PLAN

Location:
833 Massachusetts Ave.
Arlington, MA 02476

Proj #:
16189

Loc#:
278

File Path:
ACCOUNTS\C\CVS pharmacy\Locations 2016\3018_MC\3018_0000_City_ST

Drawing prepared for:

CVS pharmacy

Rev#:	Req#:	Date:	Req. By:	Drawn By:	Revision Description:
Original	000000	4/16/19	GM	CC	
Rev 1	000000	6/13/19	GM	CC	Notes
Rev 2	000000	00/00/00	XXX	XXX	
Rev 3	000000	00/00/00	XXX	XXX	
Rev 4	000000	00/00/00	XXX	XXX	
Rev 5	000000	00/00/00	XXX	XXX	
Rev 6	000000	00/00/00	XXX	XXX	

Rev#:	Req#:	Date:	Req. By:	Drawn By:
Rev 7	000000	00/00/00	XXX	XXX
Rev 8	000000	00/00/00	XXX	XXX
Rev 9	000000	00/00/00	XXX	XXX
Rev 10	000000	00/00/00	XXX	XXX

NOTES:

Drawing prepared by:

Poyant Building Your Brand

SITE PLAN

Location:
833 Massachusetts Ave.
Arlington, MA 02476

Proj #:
16189

Loc#:
278

File Path:
ACCOUNTS\C\CVS pharmacy\Locations 2016\3018_MC\3018_0000_City_ST

Drawing prepared for:

CVS pharmacy

Rev#:	Req#:	Date:	Req. By:	Drawn By:	Revision Description:
Original	000000	4/16/19	GM	CC	
Rev 1	000000	6/13/19	GM	CC	Notes
Rev 2	000000	00/00/00	XXX	XXX	
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Rev 4	000000	00/00/00	XXX	XXX	
Rev 5	000000	00/00/00	XXX	XXX	
Rev 6	000000	00/00/00	XXX	XXX	

Rev#:	Req#:	Date:	Req. By:	Drawn By:
Rev 7	000000	00/00/00	XXX	XXX
Rev 8	000000	00/00/00	XXX	XXX
Rev 9	000000	00/00/00	XXX	XXX
Rev 10	000000	00/00/00	XXX	XXX

NOTES:

Drawing prepared by:

Poyant Building Your Brand

SITE PLAN

Location:
833 Massachusetts Ave.
Arlington, MA 02476

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Drawing prepared for:

CVS pharmacy

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Rev 4	000000	00/00/00	XXX	XXX	
Rev 5	000000	00/00/00	XXX	XXX	
Rev 6	000000	00/00/00	XXX	XXX	

Rev#:	Req#:	Date:	Req. By:	Drawn By:
Rev 7	000000	00/00/00	XXX	XXX
Rev 8	000000	00/00/00	XXX	XXX
Rev 9	000000	00/00/00	XXX	XXX
Rev 10	000000	00/00/00	XXX	XXX

NOTES:

Drawing prepared by:

Poyant
Building Your Brand

SITE PLAN

Location:
833 Massachusetts Ave.
Arlington, MA 02476

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Drawing prepared for:

CVS pharmacy

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Rev 5	000000	00/00/00	XXX	XXX	
Rev 6	000000	00/00/00	XXX	XXX	

Rev#:	Req#:	Date:	Req. By:	Drawn By:
Rev 7	000000	00/00/00	XXX	XXX
Rev 8	000000	00/00/00	XXX	XXX
Rev 9	000000	00/00/00	XXX	XXX
Rev 10	000000	00/00/00	XXX	XXX

NOTES:

Drawing prepared by:

Poyant Building Your Brand

OVERVIEW PHOTOS

Location:
833 Massachusetts Ave.
Arlington, MA 02476

Proj #:
16189

Loc#:
278

File Path:
ACCOUNTS\C\CVS pharmacy\Locations 2016\3018_MC\3018_0000_City_ST

Drawing prepared for:

CVS pharmacy

Rev#:	Req#:	Date:	Req. By:	Drawn By:	Revision Description:					
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Rev 1	000000	6/13/19	GM	CC	Notes	Rev #:	Req#:	Date:	Req. By:	Drawn By:
Rev 2	000000	00/00/00	XXX	XXX		Rev 7	000000	00/00/00	XXX	XXX
Rev 3	000000	00/00/00	XXX	XXX		Rev 8	000000	00/00/00	XXX	XXX
Rev 4	000000	00/00/00	XXX	XXX		Rev 9	000000	00/00/00	XXX	XXX
Rev 5	000000	00/00/00	XXX	XXX		Rev 10	000000	00/00/00	XXX	XXX
Rev 6	000000	00/00/00	XXX	XXX						

7

8

8

NOTES:

Drawing prepared by:

Poyant
Building Your Brand

OVERVIEW PHOTOS

Location:
833 Massachusetts Ave.
Arlington, MA 02476

Proj #:
16189

Loc#:
278

File Path:
ACCOUNTS\C\CVS pharmacy\Locations 2016\3018_MC\3018_0000_City_ST

Drawing prepared for:

CVS pharmacy

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Rev 2	000000	00/00/00	XXX	XXX	
Rev 3	000000	00/00/00	XXX	XXX	
Rev 4	000000	00/00/00	XXX	XXX	
Rev 5	000000	00/00/00	XXX	XXX	
Rev 6	000000	00/00/00	XXX	XXX	

Notes	Rev#:	Req#:	Date:	Req. By:	Drawn By:
	Rev 7	000000	00/00/00	XXX	XXX
	Rev 8	000000	00/00/00	XXX	XXX
	Rev 9	000000	00/00/00	XXX	XXX
	Rev 10	000000	00/00/00	XXX	XXX

NOTES: Approach photos

Drawing prepared by:

Poyant
Building Your Brand

OVERVIEW PHOTOS

Location:
833 Massachusetts Ave.
Arlington, MA 02476

Proj #:
16189

Loc#:
278

File Path:
ACCOUNTS\C\CVS pharmacy\Locations 2016\3018_MC\3018_0000_City_ST

Drawing prepared for:
CVS pharmacy

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Rev 5	000000	00/00/00	XXX	XXX	
Rev 6	000000	00/00/00	XXX	XXX	

Rev#:	Req#:	Date:	Req. By:	Drawn By:
Rev 7	000000	00/00/00	XXX	XXX
Rev 8	000000	00/00/00	XXX	XXX
Rev 9	000000	00/00/00	XXX	XXX
Rev 10	000000	00/00/00	XXX	XXX

Existing Signage - West Elevation

Existing Sign #1 (33.41 SQ.FT.)

24" Main ID Channel Letterset	S/F ☒	D/F ☐
Illuminated	Yes ☒	No ☐

Proposed Signage - West Elevation

Proposed Signage

Not To Scale Unless Noted

CVS pharmacy

IL-33-CL-L (33.08 SqFt)

1

Drawing prepared by:

Poyant Building Your Brand

RECOMMENDATION

Location: 833 Massachusetts Ave. Arlington, MA 02476

Proj #: 16189

Loc#: 278

File Path: ACCOUNTS\C\CVS pharmacy\Locations 2016\3018_MC\3018_0000_City_ST

Drawing prepared for: CVS pharmacy

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Original	000000	4/16/19	GM	CC	
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Rev 2	000000	00/00/00	XXX	XXX	
Rev 3	000000	00/00/00	XXX	XXX	
Rev 4	000000	00/00/00	XXX	XXX	
Rev 5	000000	00/00/00	XXX	XXX	
Rev 6	000000	00/00/00	XXX	XXX	

Rev#:	Req#:	Date:	Req. By:	Drawn By:
Rev 7	000000	00/00/00	XXX	XXX
Rev 8	000000	00/00/00	XXX	XXX
Rev 9	000000	00/00/00	XXX	XXX
Rev 10	000000	00/00/00	XXX	XXX

Existing Signage - South Elevation

Existing Sign #2 (75.18 SQ.FT.)

36" Main ID Channel Letters	S/F [X]	D/F [ ]
Illuminated	Yes [X]	No [ ]

Existing Sign #7 (2 SQ.FT.)

Hours Plaque	S/F [X]	D/F [ ]
Illuminated	Yes [ ]	No [X]

Proposed Signage - South Elevation

Not To Scale Unless Noted

Proposed Signage

CVS pharmacy

IL-22-CL-L LED Heart (33.08 SqFt)

2

Reface Header Only with new Vinyl Graphics

7

Drawing prepared by:

Poyant Building Your Brand

RECOMMENDATION

Location: 833 Massachusetts Ave. Arlington, MA 02476

Proj #: 16189

Loc#: 278

File Path: ACCOUNTS\C\CVS pharmacy\Locations 2016\3018_MC\3018_0000_City_ST

Drawing prepared for: CVS pharmacy

Rev#:	Req#:	Date:	Req. By:	Drawn By:	Revision Description:
Original	000000	4/16/19	GM	CC	
Rev 1	000000	6/13/19	GM	CC	
Rev 2	000000	00/00/00	XXX	XXX	
Rev 3	000000	00/00/00	XXX	XXX	
Rev 4	000000	00/00/00	XXX	XXX	
Rev 5	000000	00/00/00	XXX	XXX	
Rev 6	000000	00/00/00	XXX	XXX	

Notes	Rev#:	Req#:	Date:	Req. By:	Drawn By:
	Rev 7	000000	00/00/00	XXX	XXX
	Rev 8	000000	00/00/00	XXX	XXX
	Rev 9	000000	00/00/00	XXX	XXX
	Rev 10	000000	00/00/00	XXX	XXX

Existing Signage - North Elevation

Existing Sign #3 (12 SQ.FT.)

Drive-Thru Information Panel	S/F [X]	D/F [ ]
Illuminated	Yes [ ]	No [X]

Existing Sign #4 (1 SQ.FT.)

Pharmacy Pick-Up/Drop-Off Plaque	S/F [X]	D/F [ ]
Illuminated	Yes [ ]	No [X]

Proposed Signage - North Elevation

Not To Scale Unless Noted

Proposed Signage

full service both lanes

(4.17 SqFt)

3

Drawing prepared by:

Poyant Building Your Brand

RECOMMENDATION

Location: 833 Massachusetts Ave. Arlington, MA 02476

Proj #: 16189

Loc#: 278

File Path: ACCOUNTS\C\CVS pharmacy\Locations 2016\3018_MC\3018_0000_City_ST

Drawing prepared for: CVS pharmacy

Rev#:	Req#:	Date:	Req. By:	Drawn By:	Revision Description:
Original	000000	4/16/19	GM	CC	
Rev 1	000000	6/13/19	GM	CC	Notes
Rev 2	000000	00/00/00	XXX	XXX	
Rev 3	000000	00/00/00	XXX	XXX	
Rev 4	000000	00/00/00	XXX	XXX	
Rev 5	000000	00/00/00	XXX	XXX	
Rev 6	000000	00/00/00	XXX	XXX	

Rev#:	Req#:	Date:	Req. By:	Drawn By:
Rev 7	000000	00/00/00	XXX	XXX
Rev 8	000000	00/00/00	XXX	XXX
Rev 9	000000	00/00/00	XXX	XXX
Rev 10	000000	00/00/00	XXX	XXX

Existing Signage - North Elevation

Existing Sign #6 (3 SQ.FT.)
Receiving Entrance Plaque
Illuminated

S/F [X] D/F [ ]
Yes [ ] No [X]

Proposed Signage - North Elevation

Proposed Signage

receiving entrance
cvs pharmacy

(3 SqFt)

6

Not To Scale Unless Noted

Drawing prepared by:

Poyant
Building Your Brand

RECOMMENDATION

Location:
833 Massachusetts Ave.
Arlington, MA 02476

Proj #:
16189

Loc#:
278

File Path:
ACCOUNTS\C\CVS pharmacy\Locations 2016\3018_MC\3018_0000_City_ST

Drawing prepared for:

cvs pharmacy

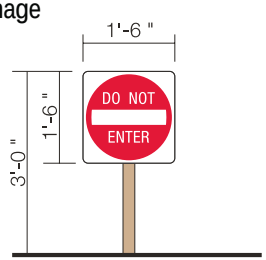
Rev#:	Req#:	Date:	Req. By:	Drawn By:	Revision Description:
Original	000000	4/16/19	GM	CC	
Rev 1	000000	6/13/19	GM	CC	
Rev 2	000000	00/00/00	XXX	XXX	
Rev 3	000000	00/00/00	XXX	XXX	
Rev 4	000000	00/00/00	XXX	XXX	
Rev 5	000000	00/00/00	XXX	XXX	
Rev 6	000000	00/00/00	XXX	XXX	

Notes	Rev#:	Req#:	Date:	Req. By:	Drawn By:
	Rev 7	000000	00/00/00	XXX	XXX
	Rev 8	000000	00/00/00	XXX	XXX
	Rev 9	000000	00/00/00	XXX	XXX
	Rev 10	000000	00/00/00	XXX	XXX

Existing Signage - West Elevation

Proposed Signage - West Elevation

Proposed Signage



(2.25 SqFt)

5

Not To Scale Unless Noted

Drawing prepared by:

Poyant
Building Your Brand

RECOMMENDATION

Location:
833 Massachusetts Ave.
Arlington, MA 02476

Proj #:
16189

Loc#:
278

File Path:
ACCOUNTS\C\CVS pharmacy\Locations 2016\3018_MC\3018_0000_City_ST

Drawing prepared for:

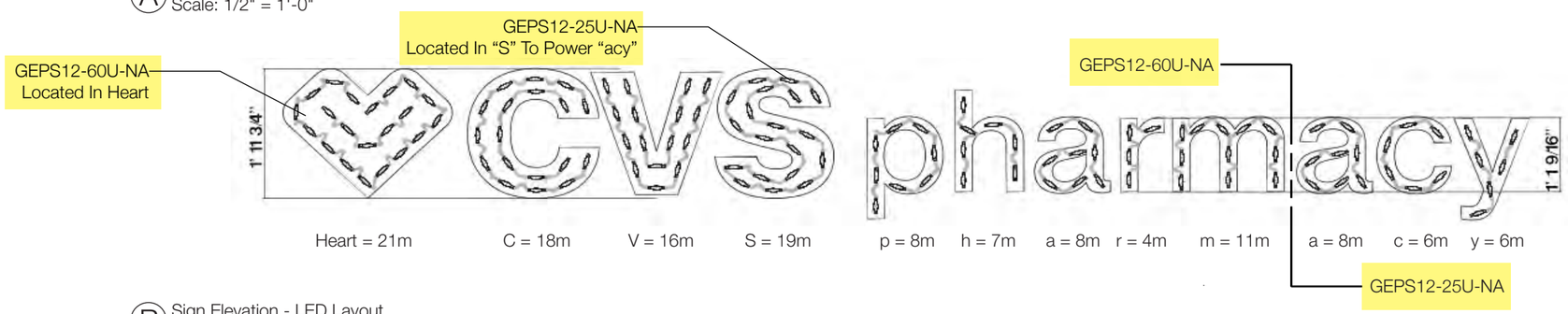
CVS pharmacy

Rev#:	Req#:	Date:	Req. By:	Drawn By:	Revision Description:
Original	000000				
Rev 1	000000	6/13/19	GM	CC	
Rev 2	000000	00/00/00	XXX	XXX	
Rev 3	000000	00/00/00	XXX	XXX	
Rev 4	000000	00/00/00	XXX	XXX	
Rev 5	000000	00/00/00	XXX	XXX	
Rev 6	000000	00/00/00	XXX	XXX	

Notes	Rev#:	Req#:	Date:	Req. By:	Drawn By:
	Rev 7	000000	00/00/00	XXX	XXX
	Rev 8	000000	00/00/00	XXX	XXX
	Rev 9	000000	00/00/00	XXX	XXX
	Rev 10	000000	00/00/00	XXX	XXX

QTY: 2 Sets

A Sign Elevation - Front View
Scale: 1/2" = 1'-0"



B Sign Elevation - LED Layout
Scale:N.T.S.

Specifications

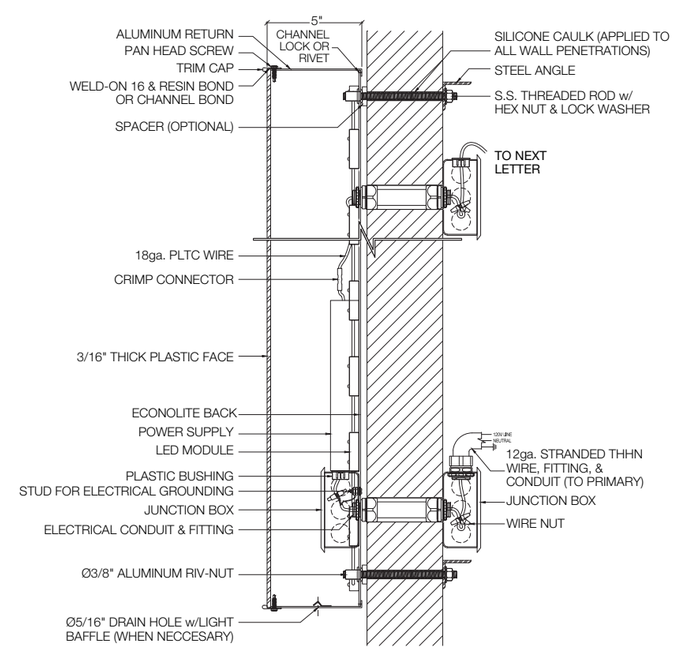
Qty:TBD 33.08 SqFt (Each)

Internally Illuminated Channel Letters (Horizontal)

- Chemcast 3/16" Red acrylic faces #2793
- 5" Deep pre-finished Hunter Red returns
- 3/4" Jewelite True Red trim cap
- Red LED illumination, GEMXRD-W1, 132 mod
- 120 Volt Power supplies, (Qty:1) GEPS12-60U-NA, (Qty:1) GEPS12-25U-NA
- 0.9 Amps, 77.88 Watts
- Mounted to existing fascia as required

Colors & Materials

- Chemcast Red Acrylic #2793
- Jewelite True Red Trim Cap
- Pre-Finished Hunter Red Returns



B Typical Section - Face Lit Letter - Flush Mounted & Self Contained
Scale: 1 1/2"=1'-0"

cvs pharmacy

One CVS Drive
Woonsocket, RI

Drawn By: L. Rogers

File Name: Main ID_ Channel Letters_Horizontal

Revisions:

This sign is intended to be installed in accordance with the requirements of Article 600 of the National Electrical Code and / or other applicable local codes. This includes proper grounding and bonding of the sign.

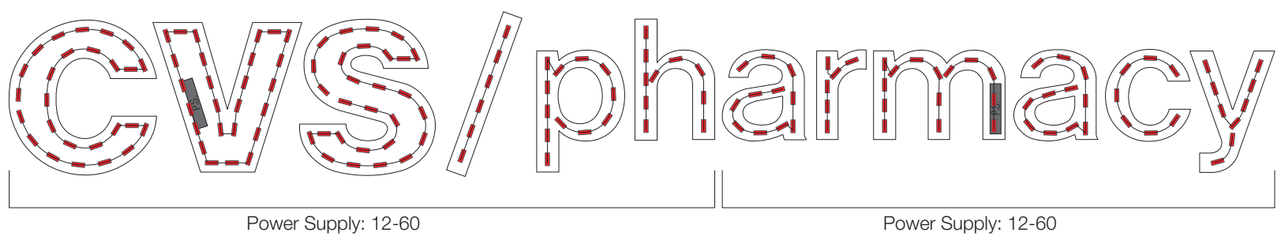
Approved By:
Approved for Construction
Martin B Higgins III
Date:

Main ID Channel Letters Horizontal - IL-22-CL-L

CHANNEL LETTERS

Existing Channel Letters
QTY: 2 Sets

A Channel Letters
Scale: 1/2”=1’-0”



B LED Layout
Scale: 1/2”=1’-0”

Specifications

Qty = 1 33.41 Sq Ft

24” CVS/pharmacy Channel Letters

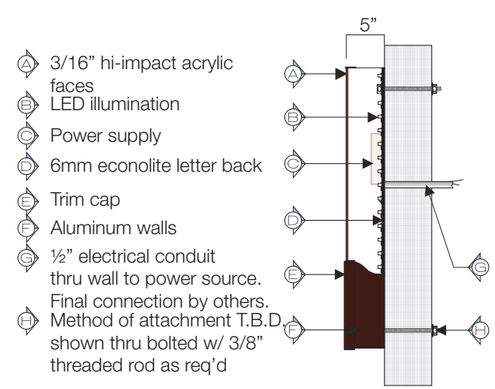
-3/16” acrylic faces - #2793 red (typical)
-5” deep .040 aluminum returns alliance pre-painted (dark bronze)
-3/4” bronze trim cap
-U.L. Listed
-(1) 20 amp circuit

-LED Specifications: Tetra MAX Red LED Strip
-LED Module: GERDMXS6
-# Of Modules#: 166
-# Of Feet: 56
-Power Supply: GEPS12-60
-Quantity: 2
-Total amps: 1.7

Colors & Materials

Red acrylic #2793

Dark Bronze trim cap & returns



LED LIT CHANNEL LETTER
MOUNTED TO WALL

Poyant
Building Your Brand

125 Samuel Barnet Boulevard
New Bedford, MA 02745
800.544.0961 | poyantsigns.com

CVS/pharmacy

Main Street
Town, State

Project: 12264
CVS/pharmacy

Sales: Gary McCoy
Date: 06.23.09
Designer: LB

Revisions:

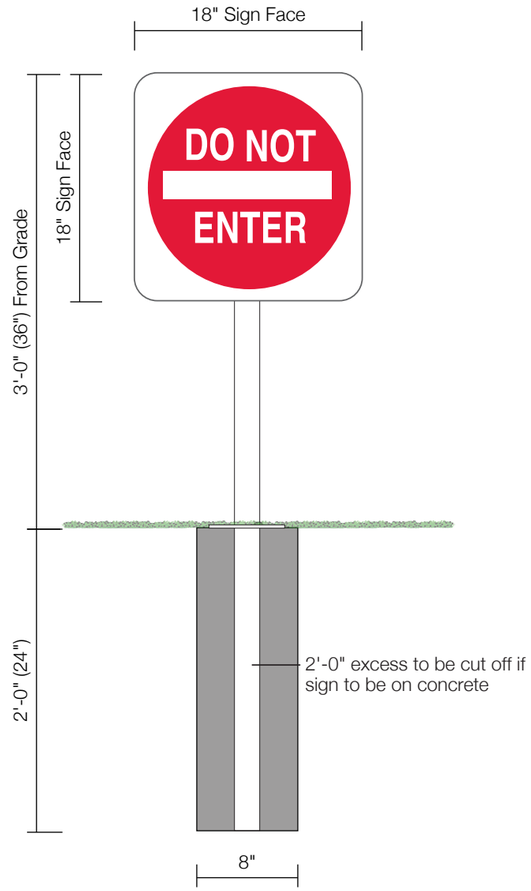
Approved By:

Date:

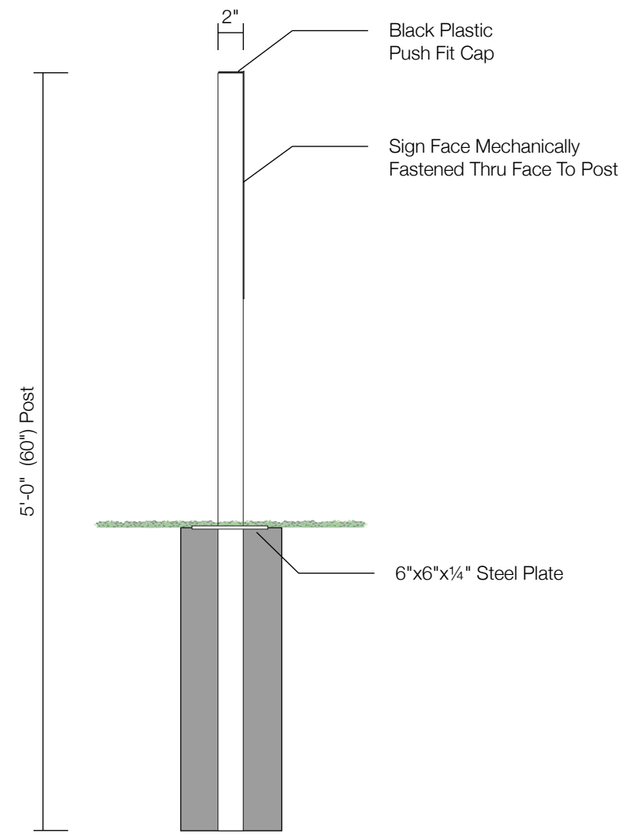
24” Channel Letters

Sign Type 1A | 1A.4

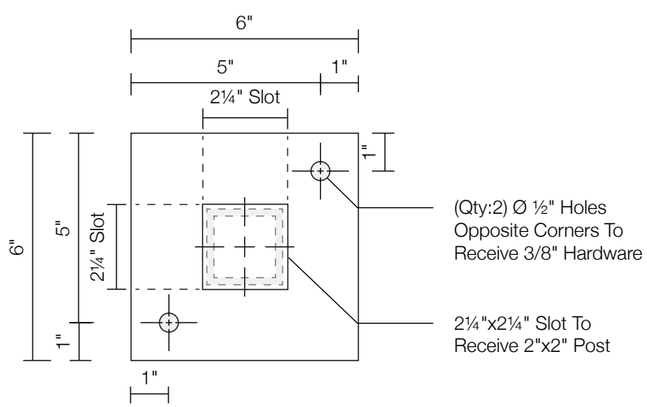
C4



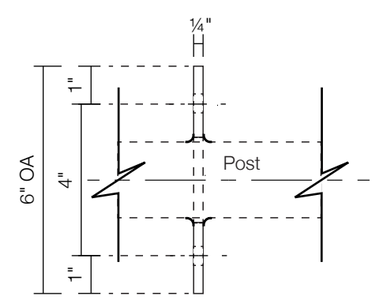
A Sign Elevation - Front View
Scale: 1"=1'-0"



B Sign Elevation - Side View
Scale: 1"=1'-0"



C Plate Detail - Front View
Scale: 3"=1'-0"



D Plate Detail - Side View
Scale: 3"=1'-0"

Specifications

Qty = 1 — 2.25 Sq Ft

Single Faced Non Illuminated Regulatory Sign

- Single sided stock regulatory sign face w/ engineer grade reflective white background & red graphics
- Sign face mechanically fastened thru face to post
- (Qty:1) 2" x 2" x 5'-0" steel post to be painted colonial white
- Black plastic push fit cap for top of post
- (Qty:1) 6" x 6" x ¼" steel plate welded 3'-0" down from top of pole painted white

* If installation is on concrete: cut pole from bottom of plate down & mount plate to concrete

* If installation is to be direct burial: bury 2'-0" below grade To be in direct burial foundation
- Foundation to be 2'-0"D x 8" Dia. with (Qty:1) Bags of pea stone per hole
  *Foundation varies per location

Colors & Materials

Paint

Colonial White; Satin Finish

CVS pharmacy

One CVS Drive
Woonsocket, RI

Drawn By: L. Rogers

Revisions:

3.23.17 LR Rev changed to pea stone fill T2

This sign is intended to be installed in accordance with the requirements of Article 600 of the National Electrical Code and / or other applicable local codes. This includes proper grounding and bonding of the sign.

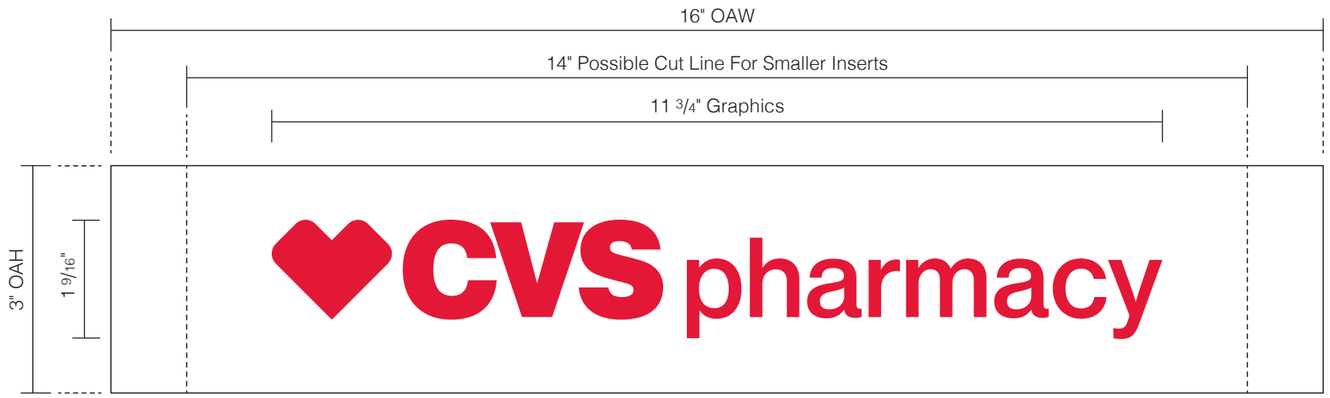
Approved By:

Date:

Do Not Enter_
Regulatory Sign

DO NOT ENTER

Page XX



(A) Sign Elevation - Front View
Scale: 6"=1'-0"

Specifications

Qty = 60 Sq Ft Varies

Replacement Single Face Header Insert For Rebrand Hours Plaques

*Remove & Dispose Of Existing Header Inserts w/ Old Logo

- .080 Styrene direct print inserts
- Red to match PMS 186c
- Some locations may need to be trimmed in length to fit
- Install new insert in top section of existing hours plaques

Colors & Materials

Direct Print

CVS pharmacy (Qty:60) 3"H x 16"W .080 Styrene Direct Printed w/ CVS Red; PMS 186C Trimmed To Finished Size

(B) Photo Comp - Existing Example
Not To Scale

(C) Photo Comp - Proposed Example
Not To Scale

Poyant
Building Your Brand

125 Samuel Barnet Boulevard
New Bedford, MA 02745
800.544.0961 | poyantsigns.com

CVS pharmacy
Corporate

Project: 14840
CVS Pharmacy

Sales: Gary McCoy
Date: 05.07.18
Designer: LR

Revisions:

5.7.18 LR T2

This sign is intended to be installed in accordance with the requirements of Article 600 of the National Electrical Code and / or other applicable local codes. This includes proper grounding and bonding of the sign.

Approved By:
Approved for Construction
Martin B Higgins III
Date:

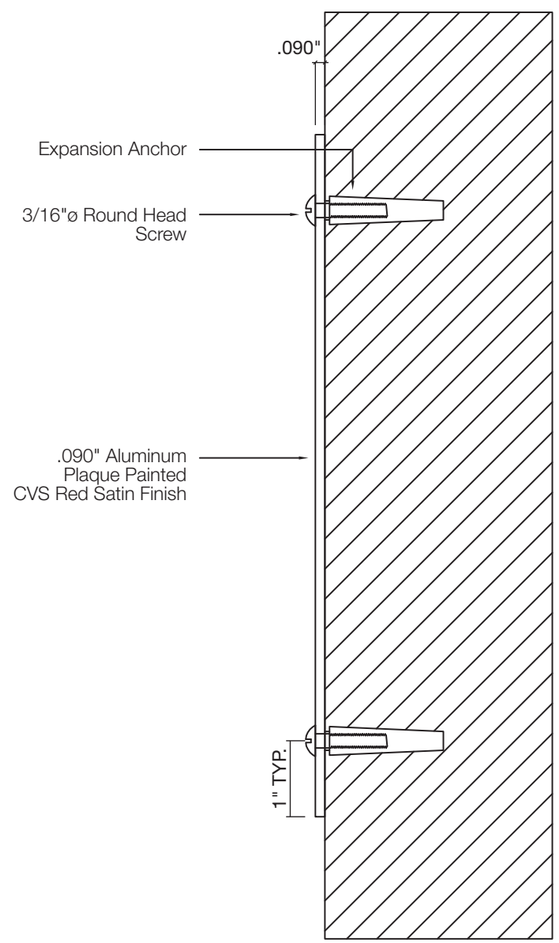
Pylon Sign

Option A

Sign Type 14840.1A | 1A.1

Receiving Entrance Plaque

A Sign Elevation - Front View
Scale: 2"=1'-0"



B Section

Specifications

S/F Non-Illuminated Door Plaque
(3.0 SQ.FT.)

Background
- 0.090" Aluminum Plaque Painted CVS Red

Graphics
- White Vinyl Graphics

Hardware
- 3/16"ø Round Head Screw
- Expansion Anchor

Colors & Materials

Paint to Match PMS #186 (Satin Finish)

White Opaque Vinyl

cvs pharmacy

One CVS Drive
Woonsocket, RI

Drawn By: N. Pazdan

File Name: Receiving Entrance Plaque

Revisions:

This sign is intended to be installed in accordance with the requirements of Article 600 of the National Electrical Code and / or other applicable local codes. This includes proper grounding and bonding of the sign.

Approved By:

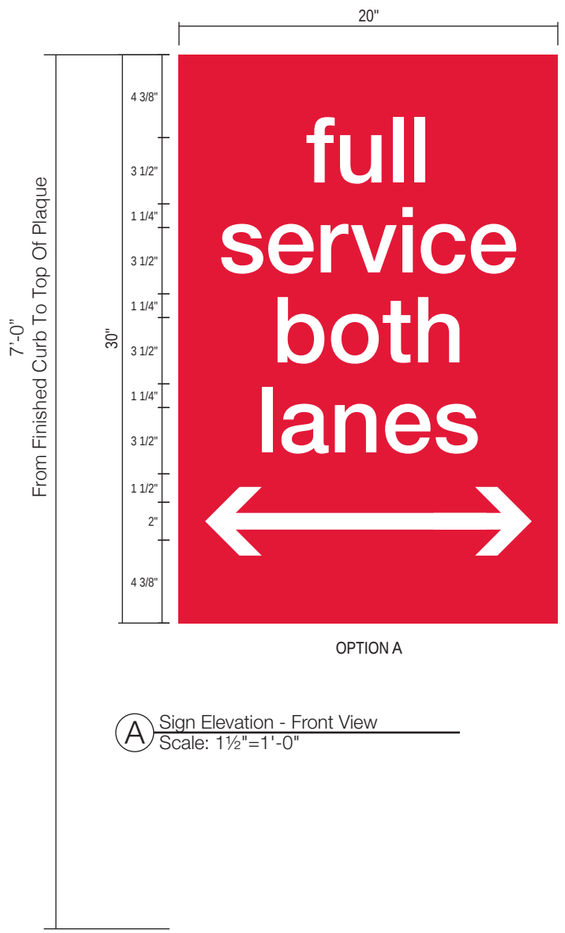
Date:

Receiving Entrance Plaque

RECEIVING ENTRANCE PLAQUE

Page 56

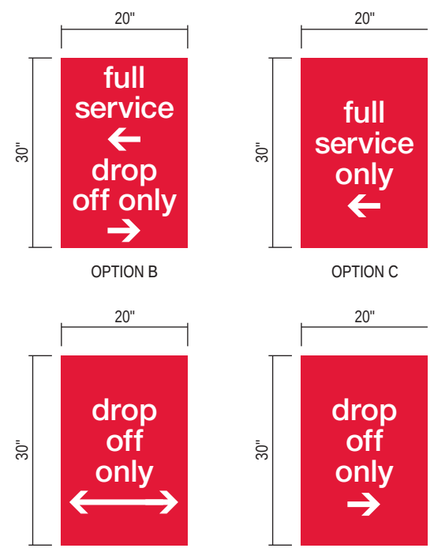
Drive-Thru Regulatory Plaques



OPTION A

A Sign Elevation - Front View
Scale: 1½"=1'-0"

B Sign Elevation - Side View
Scale: 1½"=1'-0"



C Sign Elevation - Option B-E
Scale: ¾"=1'-0"

Specifications

S/F Non-Illuminated Full Service Plaque
(4.17 SQ.FT.)

Background
- 1/8" Aluminum Plaque Painted CVS Cardinal Red

Graphics
- White Reflective Vinyl Graphics

Install
- Mount to drive-thru canopy columns as required in field

Colors & Materials

CVS Cardinal Red (Satin Finish)

White Reflective; 3M 680-10

Poyant
Building Your Brand

125 Samuel Barnet Boulevard
New Bedford, MA 02745
800.544.0961 | poyantsigns.com

CVS pharmacy

Corporate

Project: 14866
Fairhaven Commons

Sales: Gary McCoy
Date: 05.16.18
Designer: LR

Revisions:

5.16.18 LR T2

This sign is intended to be installed in accordance with the requirements of Article 600 of the National Electrical Code and / or other applicable local codes. This includes proper grounding and bonding of the sign.

Approved By:
Approved for Construction
Martin B Higgins III
Date:

Drive-Thru Regulatory Plaque
Standard

Option A

Sign Type 14866.1A | 1A.1


MoneyGram
03.26.2019


03.26.2019


CVS/pharmacy
RECEIVING
ENTRANCE
03.26.2019


CLEARANCE
NO IDLING
4KYM20
03.26.2019


CVS/pharmacy
CVS/pharmacy
03.26.2019


CVS/pharmacy
03.26.2019

NOTES

1. HANDICAP ACCESSIBLE PARKING SPACES SHALL CONFORM TO THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT (A.D.A.) OF JANUARY, 1992 AND ANY AMENDMENTS THERETO AND THE COMMONWEALTH OF MASSACHUSETTS ARCHITECTURAL ACCESS BOARD, 521 CMR, WHICHEVER IS MORE STRINGENT.

2. ACCESSIBLE PARKING SPACES DESIGNATED WITH A "V" SHALL BE SIGNED AS "VAN ACCESSIBLE" PER A.D.A. 4.1.2.5B.

3. ALL NEW CURBING INSTALLED ON MASSACHUSETTS AVENUE SHALL BE VERTICAL GRANITE CURB (VGC), TYPE VA3 PER MHD SPECIFICATION M 9.04.1

4. ALL LIMITS OF PAVEMENT SHALL BE CURBED UNLESS NOTED OR DETAILED OTHERWISE.

5. ALL STANDARD PARKING SPACES SHALL BE 9 FEET IN WIDTH BY 18 FEET IN LENGTH, WITH 24 FOOT AISLES UNLESS NOTED OR DETAILED OTHERWISE.

6. ALL PAVEMENT STRIPING SHALL BE PAINTED WITH 2 COATS OF PAINT.

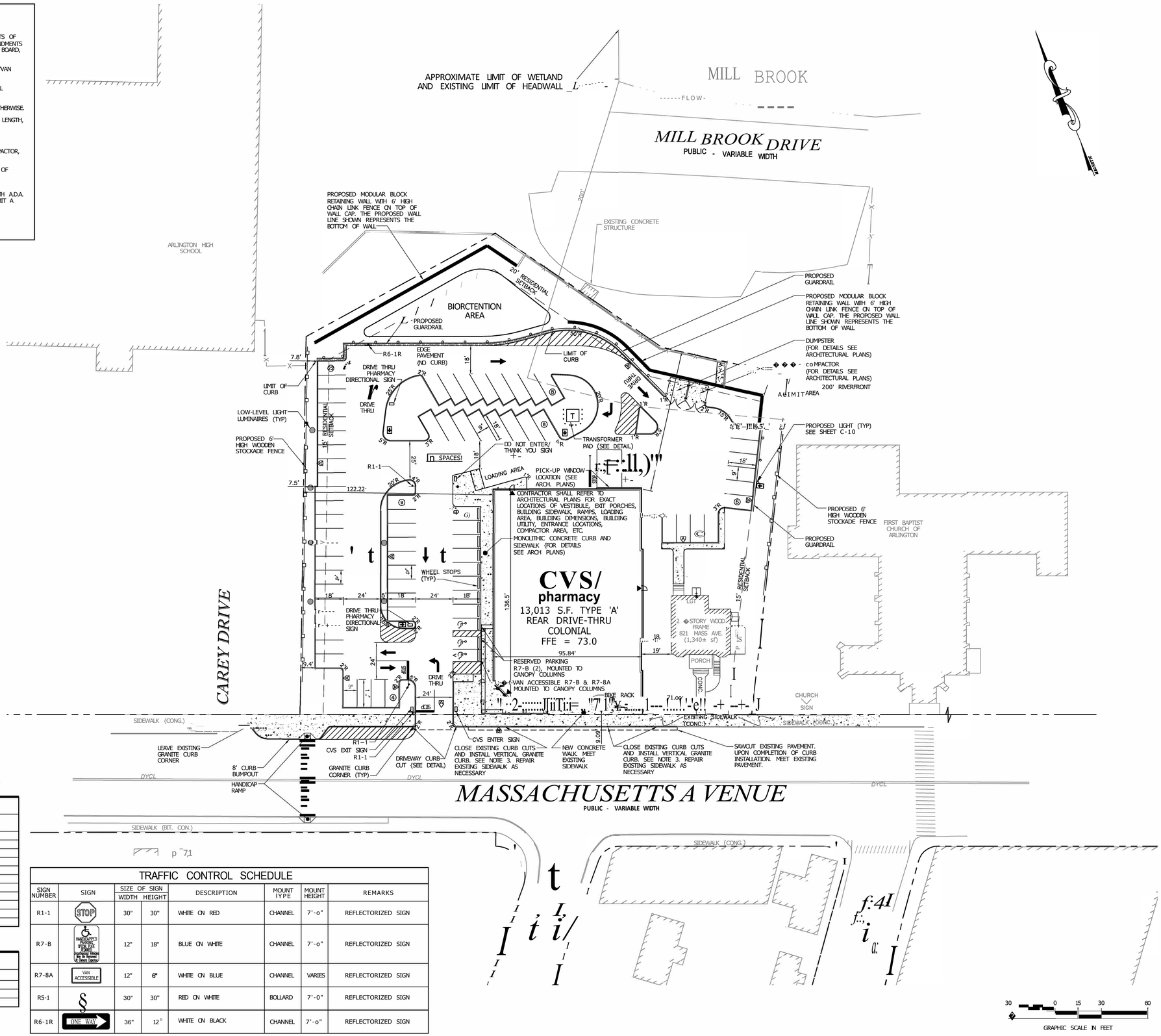
7. CONTRACTOR SHALL REFER TO BUILDING PLANS FOR CONSTRUCTION OF COMPACTOR, DUMPSTER AND DRIVE THRU AREAS.

8. CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATIONS OF VESTIBULE, EXIT PORCHES, BUILDING SIDEWALK, RAMPS, LOADING AREA, BUILDING DIMENSIONS, BUILDING UTILITIES, ENTRANCE LOCATIONS, COMPACTOR AREA, ETC.

9. ALL SIDEWALK RAMPS SHALL BE CONSTRUCTED OF CONCRETE AND COMPLY WITH A.D.A. REQUIREMENTS. PRIOR TO INSTALLING HANDICAP RAMPS, CONTRACTOR SHALL SUBMIT A DETAILED DESIGN OF EACH HANDICAP RAMP FOR THE ENGINEER'S REVIEW.

PLAN LEGEND

EXISTING	PROPOSED	DESCRIPTION
		CURBED LANDSCAPED ISLAND
	C	STANDARD PARKING STALL
	Ef	HANDICAP PARKING STALL
	(§)	PARKING COUNT
		HANDICAP SYMBOL
		MOUNTED TRAFFIC SIGN
	LCh	WHEELCHAIR RAMP
		WHEELCHAIR RAMP
	(R)	SIGNALIZED INTERSECTION
	W	EXTRUDED CONG. CURB TYPE
	W	VERTICAL GRANITE CURB TYPE
		BOLLARD
		STOP LINE
	SWCHL	SOLID WHITE CHANNELIZING LINE
	SYCHL	SOLID YELLOW CHANNELIZING LINE
	BYCL	BROKEN YELLOW CENTERLINE
	DYCL	DOUBLE YELLOW CENTERLINE
	SYCL	SOLID YELLOW CENTERLINE
	SYEL	SOLID YELLOW EDGE LINE
	SWEL	SOLID WHITE EDGE LINE
	SWLL	SOLID WHITE LANE LINE
	[T]	TRANSFORMER PAD
		CONCRETE PAVEMENT
		CHAIN LINK FENCE
		CONCRETE WHEEL STOP
		GUARDRAIL
		VERTICAL GRANITE CURB
		WOODEN STOCKADE FENCE



ZONING DISTRICT: VEHICULAR ORIENTED BUSINESS DISTRICT (B4)
RETAIL: SPECIAL PERMIT FROM ARLINGTON REDEVELOPMENT BOARD (ARB) FOR GENERAL RETAIL USE.

CVS Zoning Summary Chart

Zoning Regulation Requirements	Required	Proposed
MINIMUM LOT SIZE	NONE	1.83 AC
FRONT SETBACK	NONE	0 FT
SIDE SETBACK	NONE	71 FT
REAR SETBACK	20 FT	92 FT
MAX. BUILDING HEIGHT	35 FT	32 FT
MIN. LOT FRONTAGE	50 FT	291.5 FT
F.A.R	1.0	0.18
MIN LANDSCAPE AREA	10%	32.5%
MIN INTERIOR LANDSCAPE	8%	8.6%
RESIDENTIAL BUFFER	15 FT	7.5 FT
BUILDING SEPARATION	13 FT	18 FT

Parking Summary Chart

Description	Required	Proposed
STANDARD SPACES (9'X18')	42	69
NO. OF ACCESSIBLE SPACES**	2	3
TOTAL SPACES*	44	72

*1 SPACE PER 300 SF OF GROSS FLOOR AREA
**ADA REQUIREMENT - MINIMUM NUMBER OF ACCESSIBLE SPACES FOR A PARKING LOT TOTALING 50 TO 75 SPACES - 3 TOTAL HANDICAP SPACES
ZONING DISTRICT: VEHICULAR ORIENTED BUSINESS DISTRICT (B4)

TRAFFIC CONTROL SCHEDULE

SIGN NUMBER	SIGN	SIZE OF SIGN WIDTH	SIZE OF SIGN HEIGHT	DESCRIPTION	MOUNT TYPE	MOUNT HEIGHT	REMARKS
R1-1	STOP	30"	30"	WHITE ON RED	CHANNEL	7'-0"	REFLECTORIZED SIGN
R7-B	HANDICAPPED PARKING SPACE FINE $100 PARKING VIOLATION MUST BE REMOVED VEHICLE MAY BE TOWED AT OWNER'S EXPENSE	12"	18"	BLUE ON WHITE	CHANNEL	7'-0"	REFLECTORIZED SIGN
R7-8A	VAN ACCESSIBLE	12"	6"	WHITE ON BLUE	CHANNEL	VARIES	REFLECTORIZED SIGN
R5-1	§	30"	30"	RED ON WHITE	BOLLARD	7'-0"	REFLECTORIZED SIGN
R6-1R	ONE WAY	36"	12"	WHITE ON BLACK	CHANNEL	7'-0"	REFLECTORIZED SIGN

User = Alex Comm
Date = Tuesday, March 24, 2009 12:10:31 PM
Filename = G:\CVS\Projects\MA\Arlington\837 Mass Ave\Main\07150_C-2 SITE PLAN.dwg

CIVIL ENGINEER:
Rf O'CONNELL & ASSOCIATES, INC
Civil Engineers & Land Planners
80 MONTVALE AVE., SUITE 201
STONEHAM, MA 02180
781-279-0180
FAX: 781-279-0173

CONSULTANT:

SEAL:

CVS/ pharmacy
13,013 TYPE A PROTOTYPE
STORE NUMBER= 75672
837-821 MASSACHUSETTS AVENUE
ARLINGTON, MA
DEALTYPE=EW

DEVELOPER:
GBC
GERSHMAN BROWN CROWLEY INC.
14 BREAKNECK HILL ROAD
LINCOLN, RI 02865

REVISIONS:
ISSUED TO ARLINGTON REVIEW BOARD
REVISED PER ARB COMMENTS 03/23/2009

DRAWING BY: AKC/KPM
DATE: 03/02/2009
JOB NUMBER: 07150

TITLE:
PARKING & TRAFFIC CONTROL PLAN

SHEET NUMBER:
C-2

COMMENTS:

ARLINGTON REDEVELOPMENT BOARD
TOWN HALL ARLINGTON, MASSACHUSETTS 02476
TELEPHONE 781-316-3090

November 18, 2019

Geoffrey Noyes
208 Beacon Street
Marblehead, MA 01945

Re: Environmental Design Review Special Permit 3348

Dear Mr. Noyes,

I am writing to follow-up from the October 7, 2019 Redevelopment Board meeting regarding the property you own located at 833 Massachusetts Avenue in Arlington, MA. This property has a Special Permit 3348 recorded at the Middlesex South Registry of Deeds Book 131350 and Page 69 on August 18, 2009. Pursuant to this Decision, as the property owner you are required to abide by all permit conditions. The Arlington Redevelopment Board requests follow-up on actions with regard to future redevelopment of the "Atwood House" which was discussed with the Board on October 7th and is referenced in about Special Condition #5 in the original Special Permit.

The Board requests your attendance at their public hearing on December 16th at 8:30 p.m. in the Town Hall Auditorium at 730 Massachusetts Avenue. The Board requests your attendance at this meeting in order to provide a property update regarding this special condition and your ability to meet the permit requirement.

Please contact me at your earliest convenience should you wish to discuss these matters. Otherwise, your presence is expected at the hearing date, time, and location noted above.

Sincerely,

Jennifer Raitt
Secretary Ex-Officio

cc: Andrew Bunnell, Chair
Robert J. Annese, Esquire

Town of Arlington, Massachusetts

Meeting Minutes

Summary:

8:30 p.m. *Board will review and approve meeting minutes*

ATTACHMENTS:

	Type	File Name	Description
🗋	Reference Material	09272021_DRAFT_Minutes_Redevelepment_Board.pdf	Draft ARB Meeting Minutes 9/27/2021

Arlington Redevelopment Board
Monday September 27, 2021, 7:30 PM
Meeting Conducted Remotely via Zoom
Meeting Minutes

This meeting was recorded by ACMi.
PRESENT: Rachel Zsembery (Chair), Eugene Benson, Kin Lau, Melisa Tintocalis, Steve Revilak
STAFF: Jennifer Raitt, Director of Planning and Community Development and Kelly Lynema, Senior Planner

The Chair called the meeting to order and notified all attending that the meeting is being recorded by ACMi.

The Chair explained that this meeting is being held remotely in accordance with the Governor's March 12, 2020 order suspending certain provisions of the Open Meeting Law G.L. c. 30A, Section 20. This order from Governor Baker allows for meetings to be held remotely during this time to avoid public gatherings.

The Chair introduced the first agenda item, Public Hearings. The first hearing of the evening is a Continued Public Hearing for Docket #3665, 645 Massachusetts Avenue. Ms. Raitt notified the Board that the applicant submitted a request to continue this hearing to October 25, 2021.
Mr. Benson moved to continue the public hearing for Docket #3665 to the October 25, 2021 meeting. Mr. Lau seconded, approved 5-0.

The Chair introduced the next Public Hearing, Docket #3673, 455-457 Massachusetts Avenue. Ms. Raitt introduced the Project Manager, John Murphy, and the property owner, Cynthia Pasciuto. Robert Annese, Counsel for the applicant, introduced Peter Slowik, the Architect, Aaron Mackie, and the Civil Engineer, who Mr. Annese said will be presenting this evening. Mr. Annese gave an overview of the mixed-use building project. Mr. Murphy said that the development will be built with the existing foundation remaining. Mr. Murphy said that the former Papa Gino's space would be reconfigured to allow for parking space. Mr. Slowik said that the plan is to enhance existing historical aspects and commercial storefronts with this development. Mr. Slowik said that there are plans for indoor long-term bicycle parking and short-term bicycle parking outside the buildings. Mr. Mackie gave an overview of the plan for parking under the podium supporting the residential space. Mr. Annese said that the applicant is seeking relief from the Board for landscaped open space, usable open space, setbacks, drive aisle, lighting, and a few other issues. Mr. Annese said that this development comports with the Master Plan.

Ms. Raitt said that additional information regarding lighting, signage, existing tenant updates, and site circulation would be helpful. Ms. Raitt suggested speaking with the Historical Commission earlier in the process to start a review.

The Chair asked about the existing tenant structure and Mr. Murphy said all current tenants would be staying and the existing commercial space is not changing. Mr. Murphy said that there would be no change to the existing signage. The Chair said that she would like the Board to discuss the signage since significant changes to the building are proposed. The Chair asked about circulation on site. Mr. Murphy said that there is no loading dock in the area and that a parklet is located in the parking area. The Chair asked for an overview of the lighting plan. Mr. Murphy said that there are plans for lighting under the garage, but no additional lighting is planned for the rear of the building.

Mr. Lau said he supports this mixed-use project. Mr. Lau asked if the parking area will be enclosed and asked for a sketch of the newly created alleyway to make sure the area feels like it belongs in the center of town. Mr. Lau said that he would like to review the materials to be used. Mr. Benson said he agrees with Mr. Lau.

Mr. Benson asked if there would be a new roof and if that roof could be made solar ready and electric vehicle parking. Mr. Murphy said that the roof is planned to be solar ready and one electric vehicle parking space is included. Mr. Mackie reviewed the landscaping plans for the landscaped open space area. Mr. Benson asked if trees could be included with the landscaping plans. Mr. Benson asked about if there is a Transportation Demand Management Plan and Mr. Annese said that there will be bike share, preferred parking for carpooling tenants, and covered bike parking. Ms. Raitt said that bike parking cannot be counted towards the Transportation Demand Management Plan as it is part of parking reduction relief request.

Ms. Tintocalis asked about access outdoors from within the units. Ms. Tintocalis asked if the area over Leader Bank will be a balcony. Mr. Murphy said that it is not designed to be a common area but a small private balcony for the two units with access to the balcony. Ms. Tintocalis asked about consideration for a common area or roof deck, Mr. Murphy said with the solar array on the roof there will not allow room for a common space.

Mr. Revilak asked about the heating and cooling for the building and to what extent natural gas will be used. Mr. Slowik said that there will be electric heating and cooling in the new units, there will be no change to the tenants' existing heating sources. Mr. Mackie reviewed the stormwater management plan as requested by Mr. Revilak. The Chair asked about the façade restoration for the former Papa Gino's space. Mr. Murphy said that the façade for that space will be redesigned to fit to style of the neighboring businesses. Mr. Murphy said that on the Medford Street side the brick work will be repaired and old hardware will be removed. The Chair said that she would like to address maintenance and signage during the Board's general discussion. The Chair asked for more information regarding the unloading and delivery plan at the next meeting. Mr. Lau asked about handicapped parking and asked if there is van clearance in the garage, Mr. Slowik said that the parking meets requirements. Ms. Raitt said that clarification on the plan is necessary regarding the handicapped parking space.

The Chair opened the floor to public comment.
Susan Stamps Grafton Street said that in Broadway Plaza twelve trees will be coming down as part of the walkway renovation, which will be a great loss in this part of town. Ms. Stamps said that this area will need more greenery and trees. Ms. Stamps asked if the designers have considered using green/living walls to add interest to the building and help the heat island effect for that building and clean the air. Ms. Stamps said that the small amount of greenery planned is not enough for that area. Ms. Stamps suggested working with the Town to have some planters with flowers and greenery placed in the area.

Greg Mazmanian the Operational Director for the LLC that owns 1, 3, and 5 Medford Street said that he congratulates the developer on this plan. Mr. Mazmanian suggested requesting the landlord install one awning for all of the businesses instead of the current hodge-podge of awnings and structures.

Don Seltzer of Irving Street said that this project is in the spirt of what was passed for mixed-use development, and this is very close to the vision to what Town meeting members had when they voted for the mixed-use bylaw. Mr. Seltzer said that he is concerned about the plan for landscaping plan and the lack of usable open space. Mr. Seltzer said the addition of residential space triggers the need for open space. Mr. Seltzer said that is doable to make some of the balcony and roof space is usable open space.

With no other members of the public in queue to speak, the Chair closed the floor for public comment. Mr. Lau said he thinks it is a great project he wished that the layout for the units could be reviewed. Mr. Benson asked about the location for the outside bicycle parking being at the rear of the building. He asked if it was possible for the bike parking to be moved to Mass. Ave. or Medford Street. Mr. Murphy said that he thought it would be nice to incorporate the bike parking into the parklet. Ms. Raitt said that the bicycle parking with the parklet would be the best and safest solution. The Chair asked if the

parklet details could be prepared for the next hearing if that could be included with the special conditions. Ms. Raitt said that the staff has to hear back from the applicant to see what the applicant is willing to provide. Mr. Murphy clarified that the applicant does not see the parklet as part of the project but the applicant is volunteering to support the parklet and the possibility of the parklet becoming permanent. Mr. Benson said that the plans should specify that parking is only for tenants and not for commercial parking. Ms. Tintocalis said that she is concerned that the plantings meet the bylaw requirements but are not helping with storm runoff in any way. Ms. Tintocalis said she would like to see what the optimal plan for a parklet would look like. Mr. Revilak said the lack of usable open space is representative of other B3 parcels in town and will run into this issue again. Mr. Revilak said that he feels that the applicant will need relief regarding the usable open space for this project. The Chair asked about the roofing type to ensure it will help mitigate the heat island effect.

The Chair listed the items that the Board would like the applicant to address in a future hearing:

- Revisit layout of A5, A12, A11 / optimize residential unit layout
- Revisit location of windows on alley units (offset windows)
- Look at adding in another chase for restaurant duct / add a black iron chase on the Mass Ave side; provide required number of chases on Medford St side
- Provide information and/or samples on material selection
- Indicate that roof will be solar ready and provide solar field potential on plans
- Identify EV charging on parking plan (applicant stated it will be in one of the two surface spaces)
- Provide specific details on HP space (van or not) on parking plan
- Explore potential for introducing trees onto property line, whether in addition to or in lieu of arborvitae
- Provide a Transportation Demand Management plan using three of eight suggested methods in the Zoning Bylaw
- Provide a rendering of the back of building, showing access, lighting, circulation in alleyway
- Provide additional elevations of the garage
- Share details on commercial loading and delivery
- Share details on dumpster enclosure
- Indicate height of mechanical units relative to parapet
- On the site plan, provide a larger context showing the buildings across Medford Street and Park Terrace, the structure next door, boundaries of church
- Confirm parking is designated for residents; provide intended signage
- Provide a plan or guidelines for future tenant improvements to facade, treatment of the sign band, awnings, and signage. This should include identification of tenants with nonconforming signage, landlord standards for replacement signs, addressing abandoned lighting and hardware, etc. Specific problems cited for Leader Bank, abandoned lighting above awnings on Medford St, old or out of date signage.
- Correct the note on the site plan suggesting that Park Terrace is a private way

Mr. Murphy expressed concern about being able to provide an awning plan since the individual businesses paid for their awnings and it is unique branding. The Chair explained the Board is concerned about the businesses like Leader Bank that are over signed and the abandoned lighting hardware over some of the awnings on Medford Street. The Chair said that some tenants have not updated their signs in a significant amount of time and the look is detrimental to the overall aesthetic of the building. Mr. Murphy asked if the Leader Bank signage was approved and Ms. Raitt that the signage was previously approved and offered to work with the applicant regarding this issue. Ms. Tintocalis suggested that the applicant could incentivize the tenants to clean and update their facades.

Mr. Benson moved to continue the hearing for Docket #3673 455-475 Mass Ave to Monday, October 25, 2021, Mr. Lau seconded, approved 5-0.

The Chair introduced the Continued Public Hearing, Docket #3348, 833 Massachusetts Avenue and explained that the Board requested a monthly update for this property. Ms. Raitt asked for an update regarding the meeting with the Historical Commission. Mr. Annese introduced the property owner, Greg Noyes, and the project Architect Monty French. Mr. Annese said he would give an update regarding building surveillance. Mr. Annese said that he expects to be given more information by the Historical Commission regarding the type of siding to use at the next scheduled hearing on October 5, 2021. Mr. Annese said the plan is still to restore the building then pursue a demolition permit. Mr. French said an issue with adding to the rear of the house is that it would bring the residents right into the parking lot/CVS drive through. Mr. French said if the house was demolished the new building could be brought forward to allow for more appropriately sized building in order to make the property financially viable.

Mr. Benson reviewed part of the Special Permit which states that 10 parking spaces behind the Atwood House are reserved for its use which precludes expansion into that lot. Mr. Lau said that he feels that this is such a waste to restore the façade for $150,000.00. Mr. Lau said he would prefer to donate that amount to a housing fund. The Chair said that she is concerned that if the immediate demolition is allowed there is not guarantee that this owner will then actually develop the property. Mr. Annese said that he does not feel Mr. Noyes will be lax this time around.

Ms. Raitt said she appreciates Mr. Lau's creative suggestion and said it would be best to have Town Counsel come to talk with the Board to discuss the Board's options. Ms. Raitt said that the applicant can rebuild on the parcel with a review, approval, and amendment of the permit by the Board. Mr. Benson said he would like to have a conversation with Ms. Raitt to discuss what the Board's options are including potentially amending the Special Permit. Ms. Raitt said if there is a recommendation that emerges then Ms. Raitt will update Mr. Annese. Ms. Raitt is arranging to meet with Mr. Benson and Ms. Tintocalis to discuss the Board's options. The Chair said that she would like to keep the Special Permit open for until next month to keep the Board's options open.
Mr. Benson moved to continue Docket #3348 to October 25, 2021, Mr. Lau seconded, approved 5-0.

The Chair introduced the second agenda item, ARB Committee Appointments for ARB Members and Designees. Ms. Raitt reviewed the Board's appointments including Mr. Revilak, the Board's gubernatorial appointee, to the Housing Plan Implementation Committee.
Mr. Lau moved to approve the Board's list of appointments as amended, Mr. Benson seconded, approved 5-0.

The Chair introduced the third agenda item, Draft FY22 ARB Updated Goals. The Chair explained that the goals are as reviewed during the Board's September 11, 2021 goal setting meeting. Ms. Raitt said that she made amendments to deadline dates related to a fall Town Meeting after the goal setting meeting.
Mr. Lau moved to approve the Board's FY22 updated goals as amended, Ms. Tintocalis seconded, approved 5-0.

The Chair introduced the third agenda item, Zoning Warrant Article filing process/coordination with Redevelopment Board. The Chair explained that members of the public considering submitting an article for Town Meeting to contact the Department for assistance. Ms. Raitt said that the Board would have to decide when they would like to start discussions with petitioners that are looking for the Board's feedback. Mr. Benson said that he thinks October to mid-November would be the best time for petitioners to initially reach out to the Board to request consolations.

The Chair introduced the fourth agenda item, Meeting Minutes (8/30/21, 9/11/21).

Mr. Benson moved to approve meeting minutes for August 30, 2021 as amended, Mr. Lau seconded, approved 4-0 (Mr. Revilak abstained as he was not present).
Mr. Benson moved to approve meeting minutes for September 11, 2021 as amended, Mr. Lau seconded, approved 5-0.

The Chair opened the floor to the public for the Open Forum portion of the meeting.
Don Seltzer said he would like to discuss the Atwood House parking stipulations included with the Special Permit from 2009 that Mr. Benson reviewed earlier in the meeting. Mr. Seltzer suggested keeping the house where it is, restore the façade and sides, and expand the back of the building to increase the building size.
With no other members of the public in queue the Chair closed the open forum portion of the meeting.

Mr. Lau moved to adjourn, Ms. Tintocalis seconded, approved 5-0.
Meeting adjourned.

Town of Arlington, Massachusetts

Correspondence Received

Summary:
Correspondence received from:
S. Mraz 9/8/2021
S. Tuttle 9/12/2021
M. Guyton 9/14/2021
D. Seltzer 9/21/2021
J. Spiller 9/23/2021
D. Seltzer 10/23/21
T. Kahmann 10/25/2021

ATTACHMENTS:

	Type	File Name	Description
	Reference Material	Correspondence_received_from_S._Mraz__09-08-2021.pdf	S. Mraz 09082021
	Reference Material	Correspondence_received_from__S._Tuttle_09-12-2021.pdf	S. Tuttle 09122021
	Reference Material	Correspondence_received_from_M._Guyton_09-14-2021.pdf	M. Guyton 09142021
	Reference Material	Correspondence_received_from_D._Seltzer_09-21-2021.pdf	D. Seltzer 09212021
	Reference Material	Correspondence_received_from_J._Spiller_9-23-2021.pdf	J. Spiller 09232021
	Reference Material	Corresondence_received_from_T._Kahmann_10-25-2021.pdf	T. Kahmann 10252021
	Reference Material	Correspondence_received_from_D._Seltzer_10-23-2021.pdf	D. Seltzer 10232021

---------- Forwarded message ---------
From: **Sarah Mraz** <sarahmraz@gmail.com>
Date: Wed, Sep 8, 2021 at 5:30 PM
Subject: Chase Bank proposal objection
To: <acarter@town.arlington.ma.us>

Dear Ali,
I am one of many residents deeply disappointed to learn that the beautiful spot formerly occupied by Not Your Average Joe's in Arlington Center is soon to become yet another bank. A big corporate bank- Chase. Bank of America is already in the same building. Furthermore a quick google search tells me there are already 29 physical banking locations (branches and/or ATM) in Arlington for a population of less than 50k in 2021, an online banking age.

Can't anything be done by town officials and the property owner to work together to select/attract a tenant that will contribute to the vibrancy of our community, especially in that key area?
The RDB hearing for this proposal is on 9/27 and I understand they will have very little power to oppose it.
I look forward to hearing from you on what to do next to advocate this tenant not move forward and other solutions be explored that will benefit the community and still provide the owner with rental income.
I understand Lexington changed its zoning laws in 2016 to limit banks and the like in their downtown areas. Is that being considered in Arlington?

Thank you,
Sarah Mraz
Chandler Street

From: Sarah Tuttle <s_g_tuttle@yahoo.com>
To: "jraitt@town.arlington.ma.us" <jraitt@town.arlington.ma.us>
Date: Sun, 12 Sep 2021 00:17:51 +0000 (UTC)
Subject: Comment on Proposed Chase Branch in Arlington Center

To Whom It May Concern on the Redevelopment Board,

I am writing as a resident of Arlington regarding the proposed branch of Chase Bank where Not Your Average Joe's used to be on MA Ave in Arlington Center. I sincerely hope that the location does not become a bank of any sort. To be frank-- in Arlington we have so many banks. I am very, very tired of seeing so many store fronts taken up with financial institutions. The location of the property is one of the best in Arlington-- right across from the library, in the heart of the center, with a huge space. It should be used for something that the community can engage with-- not yet another bank. There is already a large Citizen's Bank on one side of the block, and a large Bank of America property on the other side. Yet another bank in such a prime real estate location would be a waste of an opportunity to invite a business with real value to residents. We need restaurants, shops, and other businesses that invite folks to linger in the center. Businesses that the community wants to patronize. Locations that are fun to browse, or comfortable to gather. We don't need yet another reason to drive by our most accessible, valuable commercial real estate.

Thank you for taking these comments into consideration.

Sincerely,
Sarah Tuttle

Resident-- Grove St. Arlington

From: "Matthew C. Guyton" <mcguyton@mit.edu>
Date: September 14, 2021 at 1:51:43 PM EDT
To: JRaitt@town.arlington.ma.us, ebenson@town.arlington.ma.us, KLau@town.arlington.ma.us, mtintocalis@town.arlington.ma.us, rzsembery@town.arlington.ma.us
Cc: srevilak@town.arlington.ma.us
Subject: Chase Bank - please deny

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

CAUTION: This email originated from outside of the Town of Arlington's email system. Do not click links or open attachments unless you recognize the REAL sender (whose email address in the From: line in " < >" brackets) and you know the content is safe.

Dear Redevelopment Board,

I am writing to ask you to deny the special permit for Chase Bank to replace Not Your Average Joe's.

As a frequent pedestrian in Arlington Center (especially around mealtimes), Not Your Average Joe's was what I consider an "anchor restaurant". You could walk out to the food destination of Arlington Center with the intention of patronizing one of the smaller restaurants, and if those restaurants were too busy, your backup plan could be to eat at Not Your Average Joe's, and vice versa. In this manner, the large restaurant helped drive business for the smaller restaurants. For this reason, I expect that if a restaurant does not soon replace NYAJ's, then Pasha and Thai Moon will soon go out of business. I realize that denying Chase's special permit won't bring NYAJ's back, but it will give up the opportunity for a new restaurant to open in this location before giving it a chance (and before the pandemic ends).

Frequently on our walks around the Center, my wife and I would complain about the prevalence of banks and how they ruin the walkable character of the town. The corner of Mass Ave and Pleasant has a plethora of financial institutions (listed going counterclockwise): Cambridge Bank, Coldwell Banker (not a bank, but sounds like one), Chase Bank requesting to go where NYAJ used to be, and Bank of America - leaving no good reason for pedestrians to walk to that part of the Center. Walking West up Mass Ave, the Citizen's Bank drive-thru exit creates a good place to get hit by a car, which makes me cautious when out with my young son. Walking further west up Mass Ave, the cluster of Brookline Bank, Leader Bank's lending center, and TD Bank creates another dead zone for pedestrians (made even worse by Lender Bank's large empty parking lot and crumbling brick planters that have sat unfixed for months). Keeping the Center a walkable area for pedestrians is important for businesses, because all customers are pedestrians. Banks violate this goal and are a poor use of space in the center of town.

Seeing a physical bank is like seeing a piece of history (and not in the good historical way that we strive for in Arlington). Does anyone still use physical bank locations? I have bought a house, refinanced it many times, and bought a car, all without ever visiting a physical bank location. I can't remember the last time I've physically been to a bank, and the last time I went, I was just there for a lollipop. I don't see how these locations are profitable for the banks, except that they serve as expensive billboards which would otherwise not be allowed in Arlington.

Banks are ghost towns in the evening and make the area less walkable. I believe you should be trying to drive most of the banks out of the Center, not letting more banks in. If you let Chase bank replace NYAJ's, then you have essentially given up on having a vibrant welcoming town center in Arlington. (For the record, I'm not opposed to having Chase bank or any other bank in town - I just don't want to see them located at the main corner of Arlington Center).

-Matt Guyton

22 Irving St

From: Don Seltzer <timoneer@gmail.com>
To: Rachel Zsembery <rzsembery@town.arlington.ma.us>, KLau@town.arlington.ma.us, Eugene Benson <EBenson@town.arlington.ma.us>, MTintocalis@town.arlington.ma.us, Steve Revilak <steve@srevilak.net>, Jenny Raitt <jraitt@town.arlington.ma.us>
Date: Tue, 21 Sep 2021 11:08:47 -0400
Subject: Comments for Docket 3673 - 455 Mass Ave

Critique of Proposal for Docket 3673 - 455 Mass Ave

It is encouraging to see a mixed use proposal that attempts to retain a significant ground floor commercial component, particularly in the very heart of Arlington's largest business district. There are some problems however with this initial proposal, but they appear to be solvable with reasonable modifications to the plans.

I offer the Board these comments for your consideration.

Existing Building

The existing building is on a lot of 18,929 sf at the very heart of Arlington's largest business district. The first floor has eleven businesses in an area of 13,483 sf, all of it commercial. The second floor has 3,900 sf of office space.

The largest ground floor space is 3,269 sf for an eat-in restaurant (formerly Papa Gino's). The other large business is the Leader Bank branch office. There are four small eateries, primarily for takeout service. There are also three retail spaces and two salon businesses.

There are loading docks and driveway access to the rear of these businesses.

The second floor has multiple office spaces occupied by the owner of the property, as well as several tenants. It does not appear that any of these office businesses are of the type that would create foot traffic in the Center.

Proposed Mixed Use Building

The submitted plans show ten commercial spaces on the ground floor, plus a large lobby and bike storage area for the residential use. This is a 28% reduction in commercial floor area, from 13,483 sf to 9,711 sf. Much of it comes at the expense of the large eat-in restaurant space, which is being cut in half. Some of it is attributable to the residential lobby which replaces one of the

existing retail spaces.

An option worth considering is shifting the residential lobby and entrance from the Mass Ave side to one of the Medford St storefronts, freeing up the Mass Ave space for commercial use.

The second floor replaces the existing office areas with 12 one bedroom apartments and a single studio. Two of the apartments are designated as affordable. The second floor is approximately 12,330 sf. There is an additional 1,128 sf of residential lobby and bike area on the first floor, for a total of 13,458 sf of residential gross floor area.

Calculation of Floor Area
The application package is vague about how certain floor areas are calculated and provides some inconsistent figures. Most problematic is the second floor area. It can be inferred from some of the figures provided for open space that 10,211 sf is being referenced. This number is apparently obtained by simply summing up the interior floor spaces of the thirteen apartments, ignoring all hallways, stairwells, etc.

This is the third consecutive project in which the applicant has made the same miscalculation. Gross Floor Area is not the interior dimensions of rooms, but is instead defined in the ZBL as

Gross Floor Area: The sum of the horizontal areas of all stories of a building or buildings on a lot, measured from the exterior faces of exterior walls...

The actual second floor area is about 12,330 sf, to which must be added the 1,128 sf of the first floor lobby to obtain total residential gross floor area.
Without a detailed accounting of all of the floor areas it is difficult to determine exactly what the proposed FAR is for this project. It may be as high as 1.67.

Landscaping
Most of the proposed landscaped open space is to be located in the two alley ways flanking the parking garage. They are each about 11’-12’ wide and about 70’ long, running from SW to NE.

The first 15’ of these alley ways will be in total shade throughout the year.
Halfway down the alley, there will be a brief bit of afternoon sunlight each day for six months of the year, and complete shade for the other six months.
At the far end of the alley near the access driveway, the landscaped strip will receive about 36 minutes of mid afternoon sunlight per day for eight months of the year, from late February until late October.

The proposed Wintergreen Arborvitae, Shadblow Serviceberry, Ivory Halo Dogwood, and Green Mountain Boxwood do best in full sun to partial shade. They will likely fare poorly in this location.
The June Hosta, however, does thrive in full shade.
It is recommended that only hardy, shade tolerant shrubs and trees be chosen for these areas.

Usable Open Space
The application states that *"it would be the Petitioner's position that the existing building is non-conforming with respect to the usable open space requirement contained in the zoning bylaw and the proposed changes to the building do not increase the non-conformity…".*
The ZBL only requires usable open space as a percentage of the Residential floor area. The existing building has no residential component, so is currently conforming to this requirement. The proposed changes however would add more than 13,000 sf of residential use, requiring the creation of 2,600 sf of usable open space.
The applicant further claims "there is no ability to create usable open space." That is also incorrect. The ZBL 5.3.18 specifically allows this Board to approve of balcony and roof spaces not more than 10' above the lowest story used for dwelling purposes, to count toward usable open space. The current plans already include such a roof area, serving apartments A4 and A6. There are other potentially usable roof areas that can also serve this purpose as common open space areas for all of the residents.

Logistics
Businesses typically need regular truck deliveries. The businesses at this location currently have good access. The rear entrances are all accessible to vehicles driving right up to their rear doors and there are two loading docks.
This access is sharply curtailed in the submitted plans. The vehicle access is replaced by narrow alley ways impassable to any vehicles. There is no mention of the required loading zone. It is possible that this can be provided in the garage but there is no mention of it in the plans.
There is also the question of garbage disposal for the ten businesses and the thirteen apartments. The dumpster shown in the plans may not have the capacity for all of these users. There should also be a provision for the collection of recyclables.
The parking in the garage is particularly tight, with the drive aisle four feet narrower than required by the ZBL and most design guidelines. This does not impact the general public; only the apartment residents will be inconvenienced.

This is potentially one of the better proposals for mixed use since the Broadway HCA project. There are a number of problems such as those outlined above, but none of these are insurmountable, and can be corrected with some compromise and redesign.

Don Seltzer

From: Jonathans064 <jonathans064@gmail.com>
To: Jenny Raitt <JRaitt@town.arlington.ma.us>
Date: Thu, 23 Sep 2021 14:40:36 -0400
Subject: Re: 645 Massachusetts Avenue Arlington , please don't let another bank move to Arlington

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Hi ok if I email you more comments ? it actually is hard for me to do public speaking on Zoom,

1.
645 Massachusetts Avenue Arlington, wish could be another restaurant or store, not a bank, too many banks in Arlington and read application that Chase Bank wants to expand farther then the zoning requires.

2. 2-14 Medford Street Arlington, don't want a tall building to be built there, what about the small businesses that are located on that block?
Gail Ann's Coffee Shop been there a long time, There are too many tall high rises in Arlington, Arlington is a town, don't want it to be a city.
All ready is two hotels in Arlington, one in the Heights, and one next to Menotomy Grille.
Already will be a tall building across from Stop and Shop, retail and Apartments.
Jonathan Spiller

Kindness makes the world a better place!

> From: Jonathans064 <jonathans064@gmail.com>
> To: jraitt@town.arlington.ma.us
> Date: Wed, 8 Sep 2021 13:23:58 -0400
> Subject: 645 Massachusetts Avenue Arlington , please don't let another bank move to Arlington
>
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>
> Hi is too late to give feedback? please don't let another bank move in to Arlington, too ma h banks, bank on every corner, Leader Bank, Bank Of America, Citizens Bank, Cambridge Savings Bank.
> Another restaurant should move in there os bar and kitchen in space,
> where Not Your Average Joe's used to be,
>
> https://www.arlingtonma.gov/Home/Components/News/News/11402/16?cftype=News&fbclid=IwAR1H9CF7S2_gHZQujAI8OiCtbSfaG9CfPw7JjH2nQ_qPXxONXZ2ZMJC8sGw
>
> Jonathan Spiller

Kindness makes the world a better place!

From: Tom Kahmann <tomkahmann@rcn.com>
Date: October 25, 2021 at 9:26:48 AM EDT
To: JRaitt@town.arlington.ma.us, ebenson@town.arlington.ma.us, KLau@town.arlington.ma.us, srevilak@town.arlington.ma.us, mtintocalis@town.arlington.ma.us, zsembery@town.arlington.ma.us
Cc: ACarter@town.arlington.ma.us
Subject: Chase Bank Proposal for Average Joe's Location

Another Bank Storefront in Arlington Center?

Bank Storefronts crowd out the uses that people actually want, and "deaden" the very places we are trying to enliven. 645 Mass Ave is larger and more prominent than many storefronts in Arlington Center. We can't lose it to yet another bank especially when many of us bank online now--the banks all want these locations simply as advertising their brand, and to compete with each other, not because they are "essential or desirable to the public convenience or welfare."

There are already nine banks within a short distance, even walking distance, to this location, so it's hardly desirable or necessary for the public convenience :
449 Mass Ave, Leader Bank
880 Mass Ave, TD Bank
626 Mass Ave, Cambridge Savings Bank
856 Mass Ave, Brookline Savings Bank
699 Mass Ave, Citizens Bank
655 Mass Ave, Bank of America
980 Watertown Savings Bank
864 Mass Ave, Leader Mortgage
905 Mass Ave, Citizens Bank ATM
833 Santander
325 Broadway, Chase ATM
Chase also has a full service branch a 6 minute drive from Not Your Average Joes

"essential or desirable to the public convenience or welfare". I don't think so….

Please do NOT let another large Bank Storefront come to the center and displace a potential restaurant site or other essential use.

Fwd: Docket 3673 - Solar Exposure Analysis for Proposed Alley Way at 455 Mass Ave

TO RECIPIENT CC BCC

Fwd: Docket 3673 - Solar Exposure Analysis for Proposed Alley Way at 455 Mass Ave

Attach Tahoma 10pt Normal (… Styles Line Hei… Source

From: Don Seltzer <timoneer@gmail.com>
Date: October 23, 2021 at 1:18:06 PM EDT
To: Rachel Zsembery <RZsembery@town.arlington.ma.us>, KLau@town.arlington.ma.us, Eugene Benson <EBenson@town.arlington.ma.us>, mtintocalis@town.arlington.ma.us,
Cc: Jenny Raitt <JRaitt@town.arlington.ma.us>
Subject: Docket 3673 - Solar Exposure Analysis for Proposed Alley Way at 455 Mass Ave

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To: Arlington Redevelopment Board

At the previous hearing for 455 Mass Ave I commented on the lack of sufficient sunlight exposure of the two landscaped strips proposed for this s exposure analysis.

Don Seltzer

(1 File) Attached – Drag and drop, or attach a file by clicking here: Attach
455 Mass Ave Solar Exposure Analysis.pdf 6412.87k remove

Save and Close Save Send

Docket 3673 - Solar Exposure Analysis for Proposed Alley Way at 455 Mass Ave

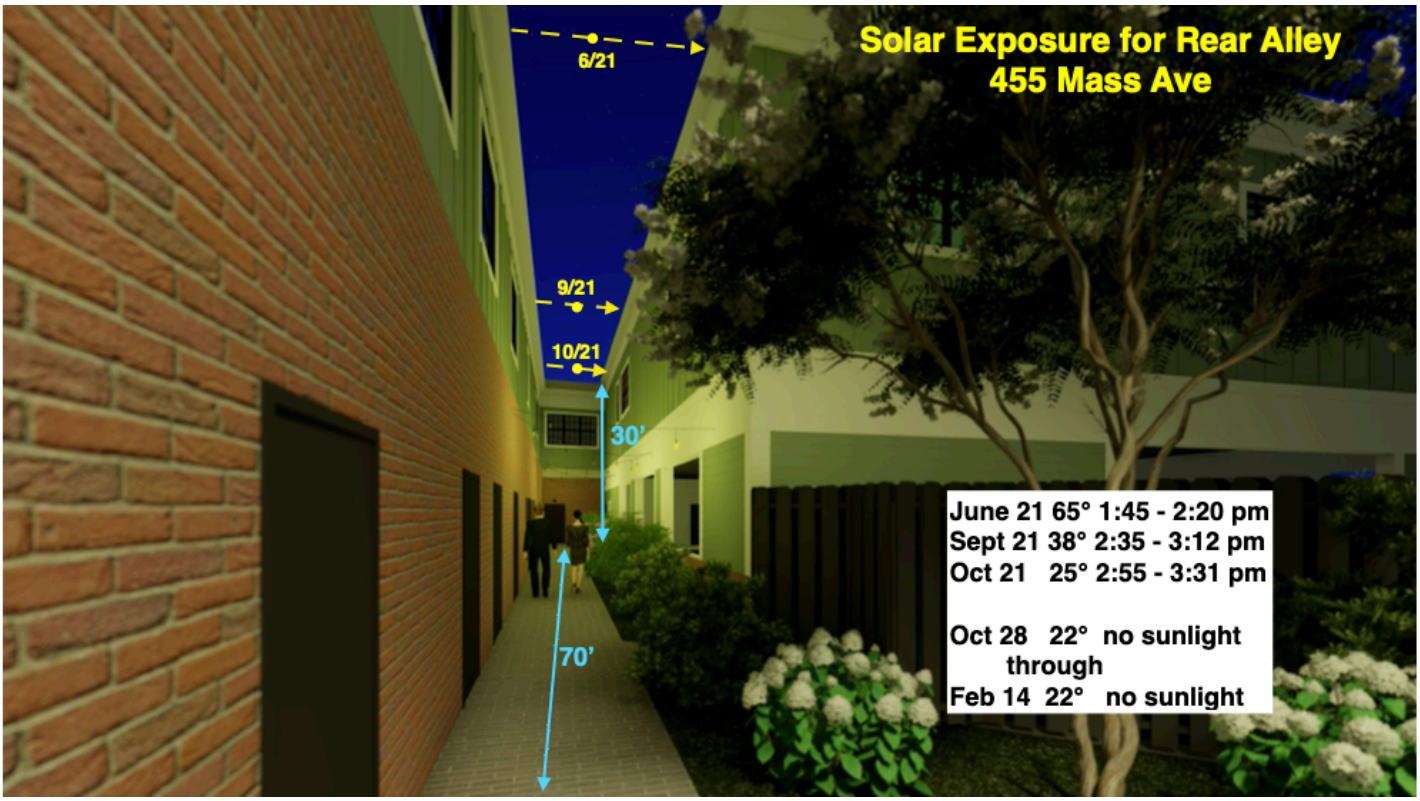
The proposed alley way is a 77’ long and 10’ wide strip oriented to the southwest, 225° compass bearing. It will be surrounded on three sides by two story buildings estimated to be 30’ high.

Half the width of this 10’ alley will be walkway and half landscaping.

The current use is as an 18’ wide parking and service/loading area for the retail businesses on the ground floor. Multiple dumpsters are also located in this area.

The narrow aperture of visible sky simplifies the analysis of solar exposure throughout the year. Near the ‘head’ of the alley, 70’ from the ‘foot’, the visible horizontal aperture of the SW sky is about 8°. Vertically, only the portion of the sky above 23° is visible at this location.

Using solar ephemeris data, it is easy to determine those months of the year when the sun’s altitude is greater than 23° above the horizon when in the SW. It is also possible to determine the length of time each day when its azimuth is in the narrow window of 221° - 229°.



For this particular alley way, the sun will pass above the roofs of the surrounding buildings for approximately 35 minutes each day, for about 8 months of the year. From late October until mid February, no sunlight will reach the head of the alley way.

Similar estimates can be made for other locations along the alley. At the midpoint, there will be a brief bit of afternoon sunlight each day for six months of the year, and complete shade for the other six months.

At the foot of the alley, closest to the building, the view of the sky in the first 14' begins at 65° above the horizon. The sun never reaches this altitude in the SW, and this portion of the alley will be in total shade throughout the year.

The proposed use of half the width of this alley as a landscaped area should be reevaluated. It is unlikely that the choice of plantings can survive on just 35 minutes of sunlight a day throughout the growing season.

It is also a challenge to maintain the other half of this alley as an accessible walkway during the winter. There does not seem to be a workable solution to clearing snow from this long narrow strip, other than to dump it onto the landscaped portion. The total lack of winter sunlight also means that the walkway will likely be dangerously icy for extended periods.

The second alley way on the other side of the garage has similar problems, but less severe because the building on the adjoining lot is neither as high, nor does it extend as far back.